

APPENDIX 12

PUBLIC CONSULTATION MATERIALS



APPENDIX 12-1
Study Contact List



Agency	Contact	Title	Notes
Provincial Ministries, Agencies and Departments			
Ministry of the Environment, Conservation and Parks (MECP)	West Central Region		Complete the project information form and send copy of notice + form by email NOTICE OF COMMENCEMENT AND COMPLETION ONLY
Ministry of the Environment, Conservation and Parks (MECP)	Joan Del Villar Cuicas	Regional Environmental Planner	
Ministry of Natural Resources and Forestry (MNR)	Al Murray	Resources Management Supervisor (Guelph District)	No Planner (Vacant) Notice of PIC #2 undeliverable
Ministry of Municipal Affairs and Housing (EA Policy)	Erick Boyd	Manager (Acting)	
Ministry of Agriculture, Food and Rural Affairs	Sarah Kielek-Caster	Rural Planner, Central West Ontario (Acting)	No longer works at Land Use Planning for Ministry
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Karla Barboza	Team Lead(A), Heritage Heritage Planning Unit Programs and Services Branch	
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Dan Minkin	Heritage Planner (Culture Services Unit)	
Ministry of Heritage, Sport, Tourism, Culture Industries (MHSTCI)	Rosi Zirger	Heritage Planner (Culture Services Unit)	
Infrastructure Ontario	Frank Dieterman		
Ministry of Transportation	Jason White	Manager	notice of PIC #2 undeliverable
Ministry of Indigenous Affairs	Lise Chabot	Manager, Ministry Partnerships Unit	
Federal Ministries, Agencies and Departments			
Fisheries and Oceans Canada			
Crown-Indigenous Relations and Northern Affairs Canada	General Contact		
Metrolinx	Katie Bright	Manager, Environmental Programs and Assessments	
Municipal Departments, Committees and Services			
City of Guelph - Accessibility	Leanne Warren	Project Specialist Accessibility	
Chamber of Commerce	Shakiba Shayani		
City of Guelph - Active Transportation	Benita van Miltenburg	Project Manager Sustainable Transportation	
City of Guelph - Building	Jeremy Laur	Chief Building Official	
City of Guelph - Asset Management	Jessica Angers	Manager Corporate Asset and Project Management	notice of PIC #2 undeliverable
City of Guelph - Communications	Mary Jo Milhomens	Manager Corporate Communications	notice of PIC #2 undeliverable
City of Guelph - Communications	Patricia Halajski	Communications Officer	notice of PIC #2 undeliverable
City of Guelph - Community Engagement	Tracy Suerich	Community Engagement Coordinator	
City of Guelph - Constructability	Ken Vanderwal	Manager Technical Services (Engineering)	
City of Guelph - Culture and Tourism	Danna Evans	General Manager Culture and Recreation	
City of Guelph - Economic Development	Christine Chapman	Manager Economic Development	
City of Guelph - Environmental Services (Stormwater)	Heather Connell	Manager Business and Technical Services	
City of Guelph - Environmental Services (Stormwater)	Colleen Gammie	Infrastructure Planning Engineer	

Agency	Contact	Title	Notes
City of Guelph - Environmental Services (Water):	Wayne Galliher	Division Manager Water Services	
City of Guelph - Environmental Services (Wastewater)	Tim Robertson	Division Manager Wastewater Services	
City of Guelph - Environmental Engineering	Prasoon Adhikari	Supervisor Environmental Engineering	
City of Guelph - Environmental Planning	Leah Lefler	Planner II Environmental Planner	
City of Guelph - Finance	Greg Clark	Manager Financial Strategy Long Term Planning	
City of Guelph - Fire Services	Steve Goode	Deputy Fire Chief Operations	
City of Guelph - Guelph Junction Railroad	Les Petroczi	General Manager Guelph Junction Railway	
City of Guelph - Guelph Public Library Information Technology	David Boyle	Manager IT Infrastructure	
City of Guelph - Legal	Jennifer Charles	Associate Solicitor	
City of Guelph - Paramedics	Leanne Swantko	Deputy Chief Guelph Wellington Paramedic Services	
City of Guelph - Parking	Jamie Zettle	Program Manager Parking	
City of Guelph - Parks/Open Space	Luke Jefferson	Manager Park and Trail Development	
City of Guelph - Parks/Street Trees	Timea Filer	Urban Forestry Field Technologist	
City of Guelph - Parks/Streetscape	Meghan Hunter	Program Manager Parks Infrastructure and Construction	
City of Guelph - Planning/Streetscape	Rory Templeton	Landscape Planner	
City of Guelph Planning – Heritage	Stephen Robinson	Senior Heritage Planner	
City of Guelph - Planning/Zoning	David DeGroot	Senior Urban Designer	
City of Guelph - Project Management Office	Ania Orłowska	Program Manager Project Management Office	
City of Guelph - Police	Darryl Goetz	Deputy Chief of Police	
City of Guelph - Police	Scott Grover	Inspector (Oversees Traffic Unit)	
City of Guelph - Operations	Terry Dooling	Manager Public Works	
City of Guelph - Realty	Terri MacCulloch	Realty Law Clerk	
City of Guelph - River Run & Sleeman Centre	Danna Evans	General Manager Culture and Recreation	
City of Guelph - Guelph Transit	Jason Simmons	Manager Transit Operations	
City of Guelph - Guelph Transit	Andrea Mikkila	Supervisor, Transit Planning and Scheduling	
City of Guelph - Engineering/Transportation	Paul Hutchinson	Supervisor Traffic Engineering	
City of Guelph - Transportation Modelling	Gwen Zhang	Transportation Planning Engineer	
Elected Officials			
Ontario Government	Mike Schreiner	MPP - Guelph	
Federal Government	Lloyd Longfield	MP - Guelph	
City of Guelph	Cam Guthrie	Mayor	
City of Guelph	Dan Gibson	Ward 1 Councillor	
City of Guelph	Bob Bell	Ward 1 Councillor	notice of PIC #2 undeliverable
City of Guelph	James Gordon	Ward 2 Councillor	
City of Guelph	Rodrigo Goller	Ward 2 Councillor	
Conservation Authority			
Grand River Conservation Authority	Ashley Rye	Resource Planner - Guelph	Email undeliverable
Grand River Conservation Authority	Katelyn Lynch	Water Resources Engineer	

Agency	Contact	Title	Notes
Grand River Conservation Authority	Tony Zammit	Watershed Ecologist	
Grand River Conservation Authority	Ben Kissner	Resource Planner	
Grand River Conservation Authority	Fred Natolochny	Supervisors of Resource Planning - Client Service Facilitator- North & South	Email undeliverable
Grand River Conservation Authority	Anindita Datta	Water Resources Engineer	Email undeliverable
Indigenous Groups			
Six Nations of the Grand River	Lonny Bomberly	Lands & Resource Director	
Six Nations of the Grand River	Peter Graham	Consultation Coordinator	
Six Nations of the Grand River	Dawn LaForme		
Six Nations of the Grand River	Chief Mark Hill	Chief	
Six Nations of the Grand River	Robbin Vanstone	Land Use Office, Lands and Research	
Haudenosaunee Confederacy Chiefs Council	Tracey Ghdi		
Haudenosaunee Confederacy Chiefs Council	Hohahes Leroy Hill		
Haudenosaunee Confederacy Chiefs Council	Haudenosaunee Development Institute	General Contact	

APPENDIX 12-2

Notices



APPENDIX 12-2-1

Notice of Commencement

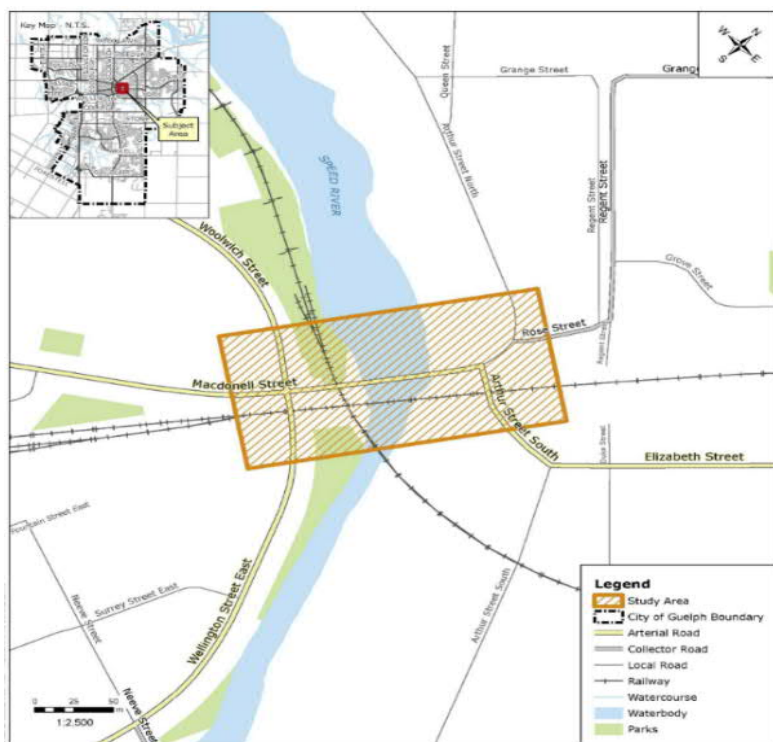


Macdonell and Allan Structures Municipal Class Environmental Assessment

The study

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many community members as Allan's Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

In response, and as part of the broader [Downtown Infrastructure Revitalization Program](#), the City of Guelph has initiated a Municipal Class EA (Class EA) for improvements and modifications to the Macdonell and Allan structures. The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street. The location and approximate extent of the study area are shown on the map.



The process

The purpose of the study is to:

- Address the structural deficiencies of the deteriorating infrastructure identified by structural inspections.
- Address the hydraulic capacity requirements of the structures.

*Information collected will be used in accordance with the Freedom of Information and Protection Privacy Act. With the exception of personal information, all comments will become part of the public record.

NOTICE OF STUDY COMMENCEMENT

- Enhance road safety, operations and connectivity for vehicles, pedestrians, cyclists and transit to support the community building goals of the City.
- Improve the intersection geometrics and operations in order to enhance traffic operations and safety for all users at the Wellington / Woolwich / Macdonell intersection.

The project is being completed as a **Schedule “C”** project in accordance with the Municipal Class Environmental Assessment (*October 2000, amended in 2007, 2011 and 2015*). In developing the recommendations, the study will consider technical, aesthetic and structural requirements, as well as socio-economic, cultural heritage and natural environment factors.

Public comments

Public and technical agency consultation will be fundamental in developing the study recommendations. Two virtual public open houses will be held during the study to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

For more information

For any questions or comments, or to be added to the study mailing list, please contact:

Steven Di Pietro, P. Eng.
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2348
steven.dipietro@guelph.ca

Andrew McGregor, MCIP, RPP
Senior Planner, EA and Approvals
R.V. Anderson Associates Limited
905-685-5049 extension 4211
AMcGregor@rvanderson.com

This notice is issued August 12, 2021.

Notice of study commencement: Macdonell and Allan Structures Municipal Class Environmental Assessment

The study

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many community members as Allan's Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

In response, and as part of the broader **Downtown Infrastructure Revitalization Program** (<https://guelph.ca/living/construction-projects/downtown-infrastructure-revitalization/>), the City of Guelph has initiated a Municipal Class EA (Class EA) for improvements and modifications to the Macdonell and Allan structures. The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street. The location and approximate extent of the study area are shown on the map.



(<https://guelph.ca/wp-content/uploads/MacdonellAllanStrEA-StudyArea-1.jpg>).

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guelph.ca/downtownproject (<https://guelph.ca/living/construction-projects/downtown-infrastructure-revitalization/>).

For more information

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Steven Di Pietro, Project Engineer
Engineering and Transportation Services

City of Guelph

519-822-1260 extension 2348

steven.dipietro@guelph.ca (<mailto:steven.dipietro@guelph.ca>)

Andrew McGregor, Senior Planner
Environmental Assessment and Approvals

R.V. Anderson Associates Limited

905-685-5049 extension 4211

AMcGregor@rvanderson.com (<mailto:AMcGregor@rvanderson.com%20%0d>)

This notice is issued August 12, 2021.

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This entry was posted in [Environment](https://guelph.ca/category/environment/) (<https://guelph.ca/category/environment/>), [Getting Around](https://guelph.ca/category/getting-around/) (<https://guelph.ca/category/getting-around/>), [Planning and Building](https://guelph.ca/category/city-hall/planning-building/) (<https://guelph.ca/category/city-hall/planning-building/>), [Road construction](https://guelph.ca/category/construction-2/roads/) (<https://guelph.ca/category/construction-2/roads/>) and tagged [MacdonellAllanEA](https://guelph.ca/tag/macdonellallanea/) (<https://guelph.ca/tag/macdonellallanea/>) on **August 12, 2021** (<https://guelph.ca/2021/08/notice-of-study-commencement-macdonell-and-allan-structures-municipal-class-environmental-assessment/>).

Related news

[Construction notice: Victoria Road North lane reductions starting April 4](https://guelph.ca/2022/03/construction-notice/) (<https://guelph.ca/2022/03/construction-notice/>)

March 31, 2022

Construction notice: Speedvale Avenue West utility work starting April 4 (<https://guelph.ca/2022/03/construction-notice-speedvale-avenue-west-utility-work-starting-april-4/>)

March 31, 2022


West End Community Centre HVAC upgrades resumes March 28 (<https://guelph.ca/2022/03/west-end-community-centre-hvac-upgrades-resumes-march-28/>)

March 28, 2022

Construction notice: Alma Trunk Sanitary sewer reconstruction (<https://guelph.ca/2022/03/construction-notice-alma-trunk-sanitary-sewer-reconstruction/>)

March 28, 2022

[Visit the newsroom \(/news/\)](/news/)

 Report a mistake or problem on this page ([//forms.guelph.ca/IT/Website-feedback?Q9=https://guelph.ca/?page_id=111386&Q8=Notice of study commencement: Macdonell and Allan Structures Municipal Class Environmental Assessment](https://forms.guelph.ca/IT/Website-feedback?Q9=https://guelph.ca/?page_id=111386&Q8=Notice%20of%20study%20commencement%3A%20Macdonell%20and%20Allan%20Structures%20Municipal%20Class%20Environmental%20Assessment))



Print this page



Transportation Master Plan

Have your say about making Guelph's Transportation Master Plan a reality

Help us move Guelph forward! Do you walk, roll, ride a bike, take a bus, drive a car, or use some other form of travel? If so, we want your feedback to help understand how our preferred alternative for our Transportation Master Plan will look, work and feel in real life.

After evaluating four alternative solutions with community input, Council approved Alternative 3: Sustainability and Resiliency Focus as the preferred solution for transportation on May 26, 2021. Now we want to hear your thoughts about how the preferred solution will impact your transportation needs and wants as you move through the city and what we need to do to implement it successfully in Guelph.

Learn more and join the conversation

Check out the virtual presentation and short summary document on haveyoursay.guelph.ca/transportation and join us for a community conversation on:

August 19

2–3:30 p.m. or 6:30–8 p.m.

Virtual meeting (the link to the public meeting will be available on guelph.ca/TMP)

We will also talk to specific audiences to ensure our transportation plan is inclusive and representative of all road users in Guelph and supports the needs of equity-deserving and inherent rights asserting groups. Separate community conversations are being scheduled as safe sharing spaces for those who identify as one or more of the following: older adult, people living with financial strain, a person with specialized accessibility and mobility needs, Indigenous, Black, a newcomer to Canada or a person of color. If you would like to join one or more of these specific sessions, contact transportation@guelph.ca to receive the meeting date and link.

For updates on the project or to get involved, follow the project at haveyoursay.guelph.ca/transportation.

For more information

Jennifer Juste, Project Manager
Engineering and Transportation Services
519-822-1260 extension 2791
transportation@guelph.ca
guelph.ca/tmp

Notice

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The study

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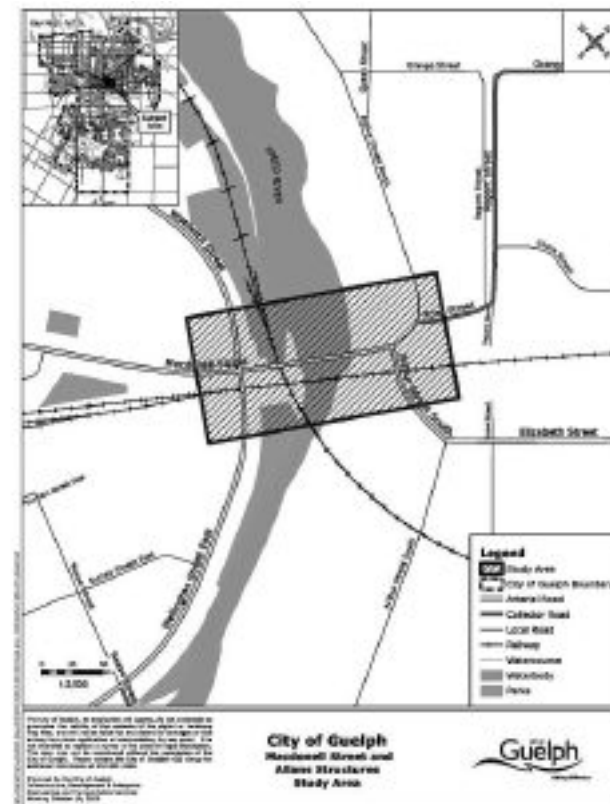
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For more information

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AMcGregor@rvanderson.com

This notice is issued August 12, 2021.

Don't rush the red



New red-light cameras will help make Guelph roads safer

The City is installing six red-light cameras at the following locations this year:

- Wellington Street East at Wyndham Street South (Ward 1)
- Eramosa Road at Stevenson Street North (Ward 2)
- Speedvale Avenue West at Dawson Road (Ward 3)
- Imperial Road at Willow Road (Ward 4)
- Scottsdale Drive at Stone Road West (Ward 5)
- Clair Road West at Gordon Street (Ward 6)

Road safety is our priority

We want to keep both you and our roads safe. Running a red light is more likely to cause significant injury than any other type of collision. Installing red-light cameras in Guelph will help decrease the severity of injuries from collisions by reducing right-angle collisions and drivers running red lights.

We all have a role to play in keeping Guelph roads safe. All road users who drive, walk, cycle or wheel should obey traffic signals at intersections and be aware of their surroundings. Please drive according to road conditions and don't rush a red light.

How red-light cameras work

Red-light cameras are live around the clock and take pictures of vehicles entering an intersection after the traffic signal turns red. Vehicles entering the intersection on a green or amber light are not photographed. Drivers making legal right and left turns are not issued tickets.

Tickets for violations are sent to the registered licence plate holder of the photographed vehicle within 30 days.

For more information

Traffic Services
519-822-1260 extension 3414
traffic@guelph.ca

Proposed development in your community

Public meeting for planning applications

Guelph City Council will hold a public meeting in accordance with the Planning Act to consider the following planning and development applications. The meeting will take place:

Monday, September 13, 2021
6:30 p.m.

This is a remote City Council meeting that can be watched online at guelph.ca/live

About the planning application(s)

103-105 Victoria Road North (File: OZS21-008)—Ward 1

The subject lands are approximately 1.4 hectares in size located at the northeast corner of Victoria Road North and Cassino Avenue.

A Zoning Bylaw Amendment application is proposing to change the zoning from Urban Reserve in part to a specialized Cluster Townhouse Zone to permit the development of 44 townhouse units along Cassino Avenue. A separate portion of the lands fronting along Victoria Road North is proposed to be rezoned to Residential—Single Detached to permit three single detached dwellings (2 existing, 1 new).

The planner to contact for this application:
Michael Witmer
Senior Development Planner
519-837-5616 extension 2790
michael.witmer@guelph.ca

710 Woolwich Street (File: OZS21-010)—Ward 3

The subject site is approximately 1.4 hectares in size and located on the west side of Woolwich Street, south of the intersection of Woolwich Street and Marilyn Drive.

A Zoning Bylaw Amendment is proposed to permit the site to be redeveloped with the potential for a mix of commercial and residential uses. A new retail building is proposed along the portion of the site closest to Woolwich Street and 96 stacked townhouse units are proposed on the rear or westerly side of the site.

The planner to contact for this application:
Katie Nasswetter
Senior Development Planner
519-837-5616 extension 2356
katie.nasswetter@guelph.ca

151 Bristol Street (File: OZS21-011)—Ward 5

The subject lands are approximately 1590 square metres in size and located along the north side of Bristol Street and south side of Emslie Street, between McGee Street and Yorkshire Street South.

The applicant is proposing to rezone the subject property to allow the development of a townhouse block with five units.

The planner to contact for this application:
Ryan Mallory
Planner 2
519-837-5616 extension 2492
ryan.mallory@guelph.ca

Speak at the meeting or provide written comments

If you wish to speak to City Council about the application you may register as a delegation at guelph.ca/delegations or by contacting the City Clerk's Office at **519-837-5603** or email clerks@guelph.ca **no later than Friday September 10, 2021 at 10 a.m.** When your registration is received, a confirmation message and instructions for participating in the remote public meeting will be provided. Instructions will also be provided during the meeting to ensure that those watching will be given the opportunity to speak.

If you prefer to comment in writing, please send your written comments to the City Clerk's Office **no later than Friday, September 10, 2021 at 10 a.m.**

Important information about making a submission

If a person or public body does not make oral or written submissions at a public meeting of Guelph City Council before the bylaw is passed, the person or public body:

- is not entitled to appeal the decision of Guelph City Council to the Ontario Land Tribunal; or
- may not be added as a party to the hearing of an appeal before the Ontario Land Tribunal unless, in the opinion of the Tribunal, there are reasonable grounds to do so.

For more information

Planning documents and background material for these applications are available online at guelph.ca/development. Alternate document formats are available upon request.

The staff report will be available at noon on Friday, September 3, 2021 at guelph.ca/development.

Contact the City Clerk's Office

If you would like to be notified of City Council's decision with respect to a planning application, you must make a written request to:

Stephen O'Brien, City Clerk
City of Guelph, 1 Carden Street, Guelph ON N1H 3A1
519-837-5603 or TTY 519-826-9771
clerks@guelph.ca

Notice

Macdonell and Allan Structures Municipal Class Environmental Assessment

The study

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City of Guelph
519-822-1260 extension 2348
steven.dipietro@guelph.ca

Andrew McGregor, MCIP, RPP
Senior Planner, EA and Approvals
R.V. Anderson Associates Limited
905-685-5049 extension 4211
AMcGregor@rvanderson.com



This notice is issued August 12, 2021.

Wyndham Street Municipal Class Environmental Assessment

The study

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of downtown Guelph. The Downtown Streetscape Manual, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

In response, and as part of the broader Downtown Infrastructure Revitalization Program, the City of Guelph has initiated a Municipal Class EA (Class EA) to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street. The study will consider options for the Wyndham Street corridor, including lane reduction from four to two lanes, and the implementation of a traffic circle in St. George's Square. The location and approximate extent of the study area are shown on the map.

The process

The purpose of the study is to enhance road safety, operations and connectivity for all users including vehicles, pedestrians, cyclists, and transit through the Wyndham Street corridor, and improve the St. George's Square at the Wyndham/Quebec/Douglas intersection to enhance traffic operations and safety for all users. In developing the recommendations, the study will consider technical, aesthetic and structural requirements, as well as socio-economic, cultural heritage and natural environment factors.

The project is being completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 and 2015).

Public comments

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For more information

For any questions or comments, or to be added to the study mailing list, please contact:

David Di Pietro, P. Eng.
Project Engineer
Design and Construction
City of Guelph
519-822-1260 extension 3574
david.dipietro@guelph.ca

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APPENDIX 12-2-2

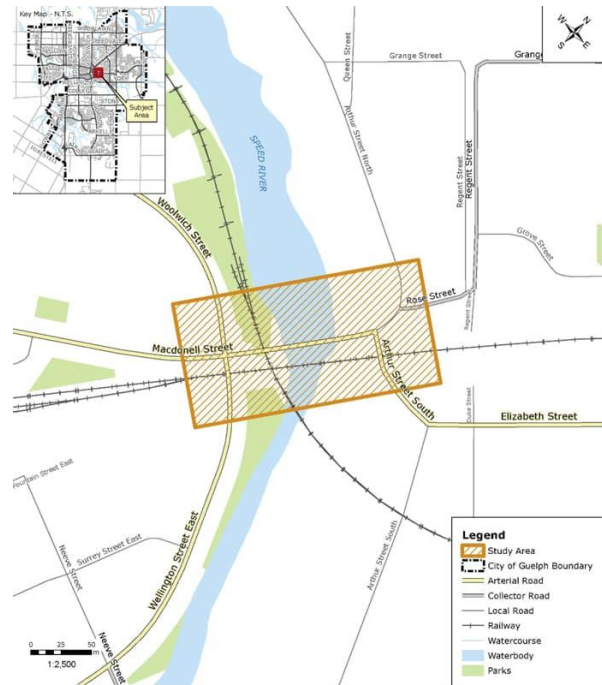
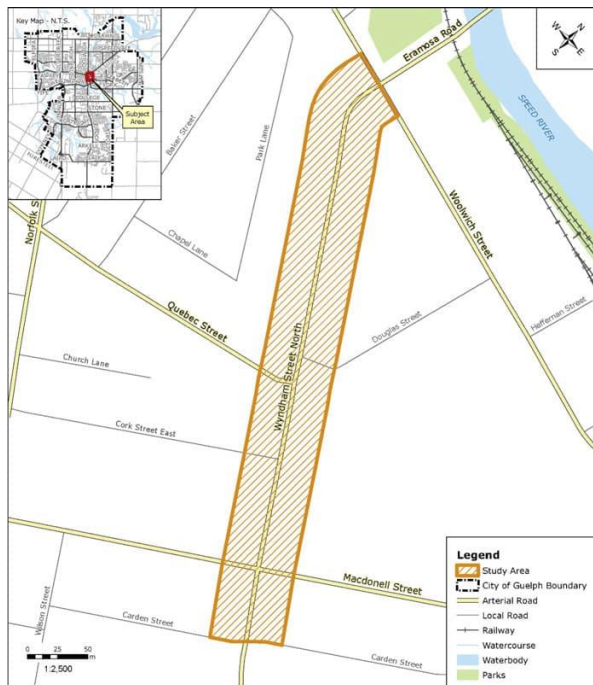
Notice of Public Open House #1



Downtown Renewal (Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies)

Notice of Open House – November 2, 2022

As part of the Downtown Renewal project, the City of Guelph is currently undertaking two Municipal Class Environmental Assessment (EA) Studies in the downtown core.



Drop in (in-person or online)

You are invited to join us in-person or online for the first Public Open House. This is a hybrid Open House where you can choose to attend in-person or online. The purpose of the open house is to share introductory material about the Environmental Assessments (EAs), answer your questions, and receive your feedback. Materials will be available through the City's Have Your Say platform at haveyoursay.guelph.ca/downtown-renewal from October 26 to November 16, 2022.

Date: Wednesday, November 2, 2022

Time: 6:30 to 8:30 p.m.

In-person Location: Guelph City Hall, 1 Carden Street

Online location: Zoom – visit haveyoursay.guelph.ca/downtownproject for the drop-in meeting link

The project team will be available in-person and via Zoom meeting to answer any of your questions from 6:30 to 8:30 p.m. Comments and feedback can be submitted in-person and online.

Learn more

Visit haveyoursay.guelph.ca/downtownproject, or by scanning the QR code.



For any questions or comments, or to be added to the mailing list, please contact:

David Di Pietro, P. Eng.
Project Engineer, Design and
Construction
City of Guelph
519-822-1260 extension 3574
david.dipietro@guelph.ca

Andrew McGregor, MCIP, RPP
Senior Planner, EA and Approvals
R.V. Anderson Associates Limited
905-685-5049 extension 4211
amgregor@rvanderson.com

Please note the online event may be recorded, and the recording may be shared on the City's website and social media channels. Any comments or questions submitted during the event may be published or shared and will be used to reflect overall feedback and help inform policy.

Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

(This notice was first issued October 13, 2022.)

Mila Khatri

From: Connor MacIsaac
Sent: October 13, 2022 10:28 AM
Cc: David Di Pietro; Andrew McGregor (AMcGregor@rvanderson.com)
Subject: City of Guelph Downtown Renewal - Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies - Notice of Open House
Attachments: City of Guelph_Notice of Open House_vf.pdf
Categories: Filed by Newforma

Dear Sir/Madam,

On behalf of the City of Guelph, you are invited to join us in-person or online for the first Open House for the Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies, being undertaken as part of the Downtown Renewal project.

This is a hybrid Open House where you can choose to attend in-person or online. The purpose of the open house is to share introductory material about the Environmental Assessments (EAs), answer your questions, and receive your feedback. Materials will be available through the City's Have Your Say platform at haveyoursay.guelph.ca/downtown-renewal from October 26 to November 16, 2022. Refer to the attached notice for more detail.

Open House #1:

Date: Wednesday, November 2, 2022

Time: 6:30pm to 8:30pm

In-person Location: Guelph City Hall (1 Carden Street, Guelph ON, N1H 3A1)

Online location: Zoom – visit haveyoursay.guelph.ca/downtownproject for the drop-in meeting link

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Kind Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

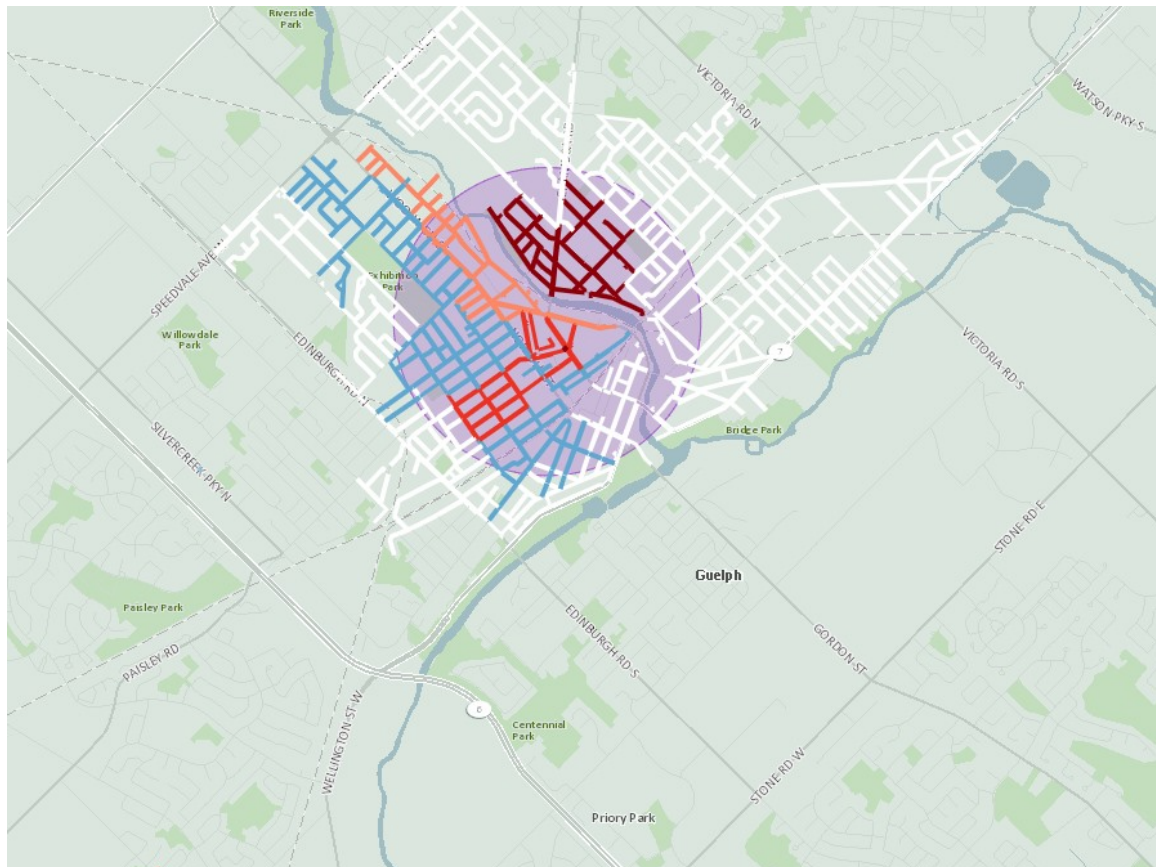
a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



Your Targeting Report

Route Ranking Report

Reaching the right people with the right message is a key driver of campaign success. The map below shows your selected trade area and the routes that make up your coverage. The routes are colour coded according to the penetration of your selected demographic variable(s) to show how closely it matches your ideal prospect.





Downtown
Renewal

You're invited!

Downtown Renewal Open House

Help us create a place for
everyone, downtown.

CITY OF
Guelph
Making a Difference

We're currently undertaking two Municipal Class Environmental Assessment (EA) Studies in the downtown core.

You are invited to join us in-person or online for the first Open House.

Wednesday, November 2, 2022

6:30–8:30 p.m.

In-person: Guelph City Hall, 1 Carden Street

Online: Zoom

Visit **haveyoursay.guelph.ca/downtown-renewal** after October 26 for the drop-in meeting link.



Questions?

mydowntown@guelph.ca

Accessible formats available by contacting
519-822-1260 extension 3893/TTY 519-826-9771.



From: Have Your Say Guelph <notifications@engagementhq.com>
Sent: October 11, 2022 11:22 AM
To: Tracy Suerich
Subject: Engagement opportunities in October

[EXTERNAL EMAIL] Do not click links or attachments unless you recognize the sender and know the content is safe.

During this Thanksgiving season, we're grateful for all who help to shape our community. Your feedback, involvement, and thoughtful contributions to Guelph help to make this the very best place to live!

Thank you all and keep it up!

Visit haveyoursay.guelph.ca to learn about current and upcoming engagement opportunities, and other projects we're working on.

In this issue:

- Reformatory District boundary engagement closes soon
- Casting your vote this October is a great way to have your say!
- Upcoming open house for the Downtown Renewal project
- Reporting back

Share your thoughts on the Reformatory District boundary

There are a few days left to participate online in the second open house. Learn how the study has been developed and provide your feedback on the draft proposed boundary for the Ontario Reformatory Heritage Conservation District area.

Visit haveyoursay.guelph.ca/reformatory-district by October 13 for more details or to participate.

Cast your vote in Guelph's municipal election this October

Participating in the democratic process by voting for your local ward councillors, City mayor and school board trustees is one of the best ways to have your say! Your vote makes a difference this October.

Do you live in Guelph or own property, are a Canadian citizen and over 18 years old? Then you're eligible to vote in the municipal election.

Advance polls have already started and election day is October 24. Find out how and where to cast your vote at guelph.ca/vote

Join us for the first Downtown Renewal project open house

As part of the Downtown Renewal project, we're currently undertaking two Municipal Class Environmental Assessment (EA) Studies in the downtown core. You are invited to join us in-person or online for the first public open house.

Drop-in at City Hall or on [Zoom](#), November 2, any time between 6:30 and 8:30 p.m. to learn about our Environmental Assessments (EAs), ask our project team questions, and provide feedback on important topics like car and bike lanes, and how each area of the study could be improved.

Visit haveyoursay.guelph.ca/downtown-renewal after October 26 to learn more!

Reporting back

- Did you see us at a Stormwater pop-up this summer to have your say, or try out our interactive stormwater model? See what we heard in this [summary](#)

You're receiving this email because you are a registered participant on Have Your Say Guelph.

Powered by [EngagementHQ](#)

[Unsubscribe](#)



City News

[@cityofguelph](https://cityofguelph.ca)
[@cityofguelph](https://cityofguelph.ca)
guelph.ca/news

Notices

Downtown Renewal (Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies)

Notice of Open House – November 2, 2022

As part of the Downtown Renewal project, the City of Guelph is currently undertaking two Municipal Class Environmental Assessment (EA) Studies in the downtown core.



Drop in (in-person or online)

You are invited to join us in-person or online for the first Public Open House. This is a hybrid Open House where you can choose to attend in-person or online. The purpose of the open house is to share introductory material about the Environmental Assessments (EAs), answer your questions, and receive your feedback. Materials will be available through the City's Have Your Say platform at haveyoursay.guelph.ca/downtown-renewal from October 26 to November 16, 2022.

Date: Wednesday, November 2, 2022

Time: 6:30 to 8:30 p.m.

In-person Location: Guelph City Hall, 1 Carden Street

Online location: Zoom – visit haveyoursay.guelph.ca/downtown-renewal for the drop-in meeting link

The project team will be available in-person and via Zoom meeting to answer any of your questions from 6:30 to 8:30 p.m. Comments and feedback can be submitted in-person and online.

Learn more

Visit haveyoursay.guelph.ca/downtown-renewal, or by scanning the QR code.

For any questions or comments, or to be added to the mailing list, please contact:

David Di Pietro, P. Eng.
Project Engineer, Design and Construction
City of Guelph
519-822-1260 extension 3574
david.dipietro@guelph.ca

Andrew McGregor, MCIP, RPP
Senior Planner, EA and Approvals
R.V. Anderson Associates Limited
905-685-5049 extension 4211
AMcGregor@rvanderson.com

Please note the online event may be recorded, and the recording may be shared on the City's website and social media channels. Any comments or questions submitted during the event may be published or shared and will be used to reflect overall feedback and help inform policy.

Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record. (This notice was first issued October 13, 2022.)

Your vote makes a difference

Do you live or own property in Guelph, are a Canadian citizen and over 18 years old? Then you're eligible to vote in the municipal election.

Ways to vote

In person

Vote at advanced polls at City Hall **October 8-10** and across the city **October 14-16**, or in your ward on **Election Day, October 24**.

By mail

Register to vote by mail at guelph.ca/vote before 4 p.m. on October 14.

Get your Voter Notification Card

If you registered to vote, you'll receive a Voter Notification Card in the mail. Haven't registered yet? Go to guelph.ca/vote until October 14 to do so. If you didn't receive a Voter Notification Card in the mail by October 10, call the City Clerk's Office at 519-837-5625 or email guelphvotes@guelph.ca.

What you need to bring with you to vote

To vote, you need a piece of identification (ID) that states your name and Guelph address such as a driver's license, Ontario Photo Card, lease or rental agreement.

A Canadian passport **cannot** be used as a piece of ID. Check out guelph.ca/vote for a full list of ID options.

Celebrating Diwali?

Celebrating Diwali on October 24? Don't let voting get in the way of your celebrations. Vote early in the advanced polls or register to vote by mail at guelph.ca/vote.

Learn more

guelphvotes@guelph.ca
519-837-5625/TTY 519-826-9771
guelph.ca/vote

Events

Guelph's Largest Clothing Swap

October 22, 11 a.m. to 3 p.m.

Old Quebec Street Shoppes

Is your closet full of clothes you don't wear anymore? Donate them in support of Waste Reduction Week and as part of the Circular Fashion Festival the City is participating in Guelph's largest clothing swap and sale. Clothing should be **recently laundered and free of stains, rips, or other damage**. Donations for all ages and genders are welcome. Hangers and freestanding clothing racks are also accepted.

For more information, visit <https://guelphtoolibrary.org/circular-fashion-festival/>

Accessible formats available by calling 519-822-1260 or TTY 519-826-9771

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Downtown Renewal (Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies)

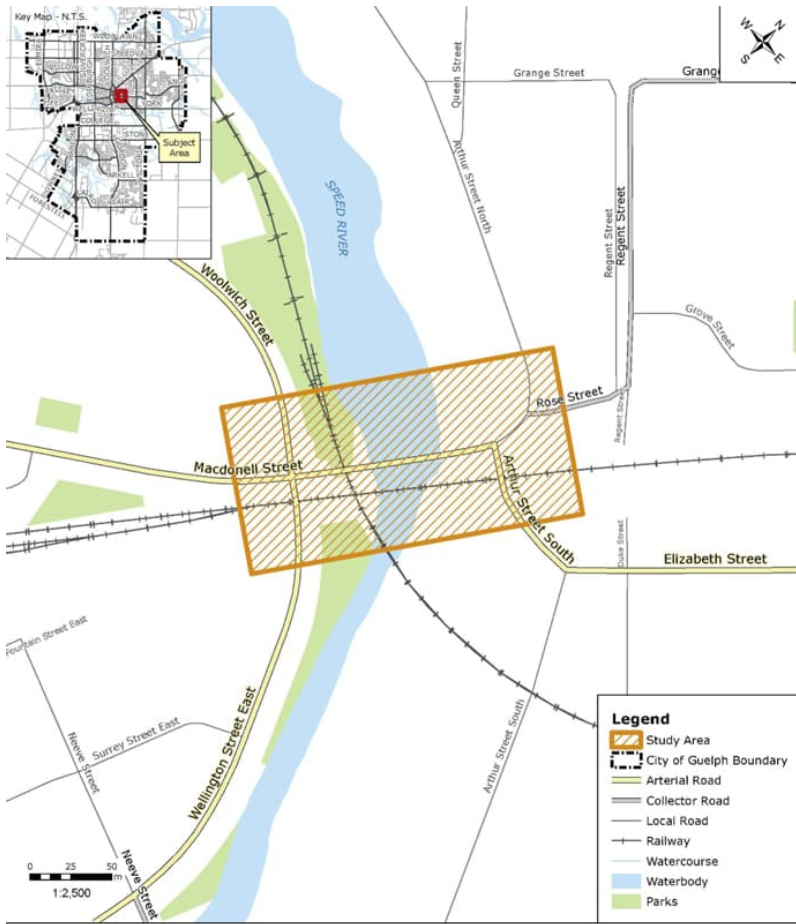


Church of Our lady in Guelph Ontario. An aerial view using a drone from the behind the church. Downtown Guelph is seen beautifully behind the churches 'sentinels'. The photo was taken by Eye Fly Media Inc. eyeflymedia.com

[Listen to this article](#)

00:02:54

As part of the Downtown Renewal project, the City of Guelph is currently undertaking two Municipal Class Environmental Assessment (EA) Studies in the downtown core.



Drop in (in-person or online)

You are invited to join us in-person or online for the first Public Open House. This is a hybrid Open House where you can choose to attend in-person or online. The purpose of the open house is to share introductory material about the Environmental Assessments (EAs), answer your questions, and receive your feedback. Materials will be available through the City's Have Your Say platform at haveyoursay.guelph.ca/downtown-renewal from October 26 to November 16, 2022.

Date: Wednesday, November 2, 2022

Time: 6:30 to 8:30 p.m.

In-person Location: Guelph City Hall, 1 Carden Street

Online location: Zoom – visit haveyoursay.guelph.ca/downtown-renewal for the drop-in meeting link

The project team will be available in-person and via Zoom meeting to answer any of your questions from 6:30 to 8:30 p.m. Comments and feedback can be submitted in-person and online.



Learn more

Visit haveyoursay.guelph.ca/downtown-renewal, or by scanning the QR code.

For any questions or comments, or to be added to the mailing list, please contact:

David Di Pietro, P. Eng.

Project Engineer, Design and Construction

City of Guelph

519-822-1260 extension 3574

david.dipietro@guelph.ca

Andrew McGregor, MCIP, RPP

Senior Planner, EA and Approvals

R.V. Anderson Associates Limited

905-685-5049 extension 4211

AMcGregor@rvanderson.com

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(This notice was first issued October 13, 2022.)

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Add to the story

Have a story idea?

Letter to the editor

Report a mistake

Ask a question

APPENDIX 12-2-3

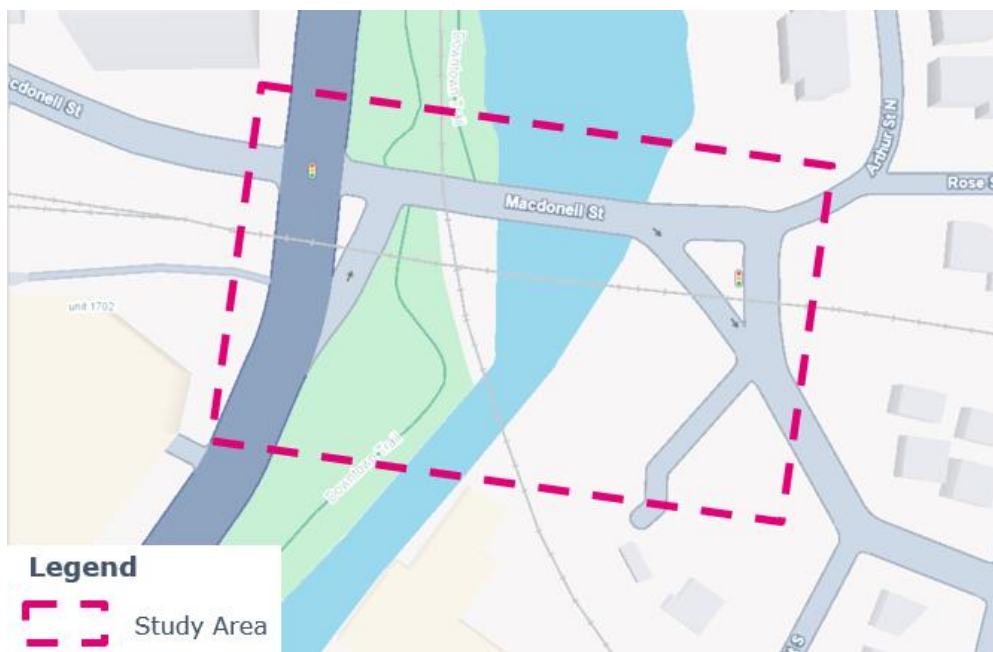
Notice of Public Open House #2



Downtown Renewal (Macdonell and Allan Structures Municipal Class Environmental Assessment Study)

Notice of Open House #2 – December 9, 2024

As part of the Downtown Renewal project, the City of Guelph is currently undertaking a Municipal Class Environmental Assessment (EA) Study in the downtown core for the Macdonell Bridge and Allan's Structures and Ward to Downtown pedestrian/cyclist crossing. Through the study, alternative and preliminary recommendations have been developed for review and input from the public.



Drop in (in-person or online)

You are invited to join us in-person for the second Public Open House for the Macdonell and Allan's Structures Class EA Study. The purpose of the open house is to share study progress, answer your questions, and receive your feedback. Materials will be available through the City's Have Your Say platform at haveyoursay.guelph.ca/downtownproject from December 9, 2024, to January 12, 2025.

Date: Monday December 9, 2024

Time: 6:00 to 8:00 p.m.

Location: Guelph City Hall, 1 Carden Street

The project team will be available to answer any of your questions from 6:00 to 8:00 p.m. Comments and feedback can be submitted in-person and online.

Learn more

Visit haveyoursay.guelph.ca/downtownproject, or by scanning the QR code.



For any questions or comments, or to be added to the mailing list, please contact:

Andrew Miller, P. Eng., PMP
Project Engineer, Design and
Construction
City of Guelph
519-822-1260 extension 3608
[andrew.miller @guelph.ca](mailto:andrew.miller@guelph.ca)

Andrew McGregor, MCIP, RPP
Senior Planner, EA and Approvals
R.V. Anderson Associates Limited
905-685-5049 extension 4211
amgregor@rvanderson.com

Any comments or questions submitted during the event may be published or shared and will be used to reflect overall feedback and help inform policy.

Information will be collected in accordance with the Freedom of Information and Protection of Privacy Act. With the exception of personal information, all comments will become part of the public record.

(This notice was first issued November 25, 2024.)

APPENDIX 12-2-4

Notice of Completion



APPENDIX 12-3

Public Open House Summary Reports



APPENDIX 12-3-1

Public Open House #1 Summary Report





Guelph Downtown Renewal

Public Open House and Have Your Say #1 Summary

February 2023

What We Heard Summary #1: Downtown Renewal

February 2023

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What We Heard Summary #1: Downtown Renewal

February 2023

Introduction

Background

The City of Guelph has a beautiful and vibrant downtown. The City also has aging water and sewer pipes, roads and sidewalks that must be replaced; some underground pipes are over 100 years old! The City is replacing aging infrastructure and upgrading utility services, and there is an opportunity to modernize and upgrade the streetscape.

Improving this important infrastructure will allow the City to provide quality service and support the vitality of the downtown.

Work like this takes time. It takes years to research, engage with the community, plan, and replace the infrastructure. This important project will improve the downtown for all businesses, residents, users and visitors. The goal of this initiative is to create a place for everyone downtown.



Figure 1: Map of Study Area

What We Heard Summary #1: Downtown Renewal

February 2023

The primary project study area is north of the railway tracks. Key streets include:

- Baker Street from Quebec Street to Woolwich Street (part of Baker District)
- Quebec Street from Wyndham Street North to Norfolk Street
- Wyndham Street from Carden Street to Woolwich Street
- Macdonell Street from Norfolk Street to Carden Street
- Woolwich Street from Macdonell Street to Norfolk Street
- Macdonell Street and Allan's Dam structures over the Speed River

Planning for Downtown Renewal began in the spring of 2021. During the planning phase, the City looks at the existing infrastructure and the future needs of downtown to determine what needs to be updated.

The City is completing two Municipal Class Environment Assessments (EAs) as part of the planning work. Environmental assessments involve collaborating with the community to support planning and decision-making for large projects.

The Wyndham Street EA will examine the function of the road for all users, including the number of traffic lanes and available options for active transportation.

The other EA will examine the Macdonell Street Bridge and Allan's Dam structures across the Speed River to confirm the required improvements and the preferred solution.

By the end of 2024, staff will present the EAs and get City Council endorsement on the overall Capital Implementation Plan to make the necessary infrastructure upgrades. Work on the final design will start in 2025. Construction will start as soon as 2026. The overall design and construction process is expected to last from eight to 10 years, depending on pace and capacity.

What we did

In November 2022, the City of Guelph hosted the first public open house for the Downtown Guelph Renewal. The open house followed a "drop-in" style, where materials were displayed in-person and online for public review. Project team members were available in person and online to provide additional context and answer questions.

After reviewing the display materials, members of the public were invited to complete a comment form (either hard copy or through Have Your Say). The form solicited thoughts about the goals and options for the Wyndham Street Corridor, the Macdonell Bridge and the Allan's Dam. As in previous rounds of engagement, participants were invited to identify potential issues and opportunities using a study area map. Some community members also submitted feedback to the project team via email. The mapping exercise and email feedback have been incorporated into the summary below.

What We Heard Summary #1: Downtown Renewal

February 2023

Date	Engagement Type	Participation
November 2, 2022	Open House (in-person)	46 attendees
November 2, 2022	Open House (virtual)	31 attendees
October 27 – November 30, 2022	Have Your Say webpage Main comment form/survey	720 visitors to the website, of which 164 completed the survey
October 27 – November 30, 2022	Emailed comments	Emailed submissions were received from 5 individuals and groups
December 1 – 18, 2022	Have Your Say webpage Follow-up survey on Allan's Dam options	281 visitors to the website, of which 140 completed the survey

A meeting was held with the Downtown Guelph Business Association (DGBA) on October 18, 2022, to complement the feedback collected from the public.

The project team also received survey feedback from a downtown business that was taken in as part of our ongoing research.

Engagement purpose

The purpose of these engagement activities was to:

- Introduce Downtown Guelph Renewal
- Set the context for the Wyndham Street Class EA and the Macdonell Class EA
- Present problem and opportunity statements and a list of all options before choosing the recommended option
- Solicit public feedback, questions and concerns about Downtown Renewal

Who we heard from

Survey participants completed a series of demographic questions (optional). The following is a summary of who we heard from. Of the 164 main survey participants:

- 62 were residents of downtown Guelph, 94 were residents of Guelph (outside downtown), and 17 worked at a business downtown
- 59 noted their preferred method of travel was cycling, 50 preferred walking and 33 preferred driving

What We Heard Summary #1: Downtown Renewal

February 2023

What we heard

Downtown as a destination

Motivation to visit Downtown

Participants were asked what motivates them to visit downtown now. A summary of their responses is below.

- Children's activities and special events
- Attending festivals, cultural events and live music; visiting the cinema
- Shopping, eating, entertainment; visiting restaurants, cafes, and patios
- Visiting the library
- Visiting the farmers' market and independent, unique businesses
- Downtown is a good place to walk or cycle; it provides access to trails and the river
- Accessing professional services, doctors' offices, the post office, the bank, City Hall
- Sense of community experienced downtown; meeting with friends
- Accessing the GO Train
- Historical architecture

Others indicated that they live/work in or near downtown, making it a convenient place to visit. Many participants noted that they like the downtown atmosphere or find it a nice place to visit. Participants were very supportive of local businesses in the area, with a few noting they have personal relationships with shop owners.

A few individuals indicated that they are not motivated to visit downtown due to challenges with parking and feel unsafe.

Downtown as a destination in the future

Participants were asked what would make downtown a destination for them in the future. A summary of their responses is below.

- More places for shopping, eating, and entertainment; more patios
- Increased safety and infrastructure for people who walk or bike; bike parking, protected bike lanes
- Pedestrian-only areas, wider sidewalks
- Fewer cars, less traffic; others called for more/cheaper parking
- Parks, greenspace, and gathering spaces accessible to all
- Activities for children
- Public events, live music, cultural festivals
- More visually appealing and vibrant, e.g., public art, more trees, more street furniture, less litter
- New library
- Accessibility and safety for all
- A grocery store
- Consideration for the needs of seniors

What We Heard Summary #1: Downtown Renewal

February 2023

Many participants emphasized a desire to make downtown a destination for *people*, reducing the focus on cars. As identified above, there is a strong desire to improve access, safety and vibrancy for the benefit of those who walk or bike.

Wyndham Street

The Wyndham Street study aims to identify recommendations that will:

- Enhance road safety, operations, and connectivity for people who walk, ride bikes, take transit and drive
- Improve how the Wyndham/Quebec/Douglas intersection (St. Georges Square) functions

Feedback on goals

Participants were asked if any other goals should be considered for the study. A summary of their responses is provided below.

- A pleasant streetscape and atmosphere, more green spaces and trees
- Increased safety for all road users, including those who walk and bike, aligning with the City's modal split goal
- Making downtown more pedestrian-friendly and family-friendly
- Considerations for accessibility and universal design
- Maintaining parking for individuals with accessibility concerns
- Dedicated loading zones for businesses
- Compassionate response for those experiencing homelessness and mental health challenges
- Reduce negative impacts on the environment; consider climate change mitigation and adaptation
- Consider connections within and outside of the downtown core
- Retaining the "village" or "small-town" feel of downtown; ensure downtown is a "destination"
- Consider the impact of winter weather in design
- Consideration of the needs/input of businesses in the process
- Addressing local crime

What We Heard Summary #1: Downtown Renewal

February 2023

Feedback on options

Participants were asked which option(s) best enhance road safety, operations and connections for the Wyndham Street study area. The results are shown in the figure below. Participants were able to select as many options as they liked.

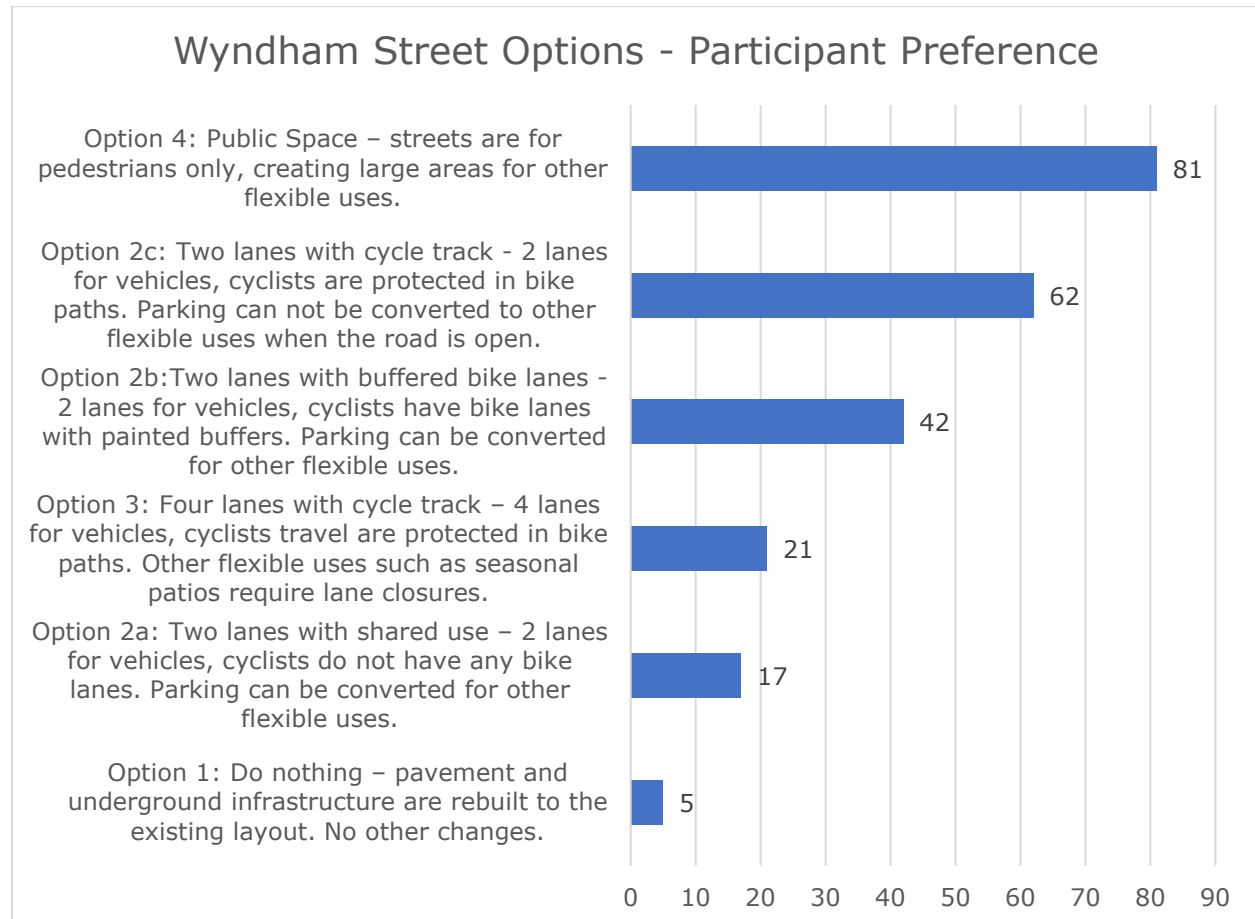


Figure 2: Participant preference for Wyndham Street options, n=158

As shown in Figure 2, Option 4 (public space) was the most popular option, followed by Option 2c (two lanes with cycle track) and Option 2b (two lanes with buffered bike lanes).

Participants felt that Option 4 provided the safest option for pedestrians. By creating a pedestrian-only area downtown, participants felt there would be greater opportunities for community-building, vibrant gathering spaces and increased foot traffic for local businesses.

While there was much support for Option 4 (public space), several participants commented that it might be too ambitious and/or more of a long-term goal. Some participants would like to see transit and cycling accommodated within the design. Others questioned if a lack of car access could negatively impact local businesses or those with accessibility needs. Option 4 also raised concerns about the lack of use during winter.

What We Heard Summary #1: Downtown Renewal

February 2023

Participants supported separated/protected bike lanes (Options 2b and 2c). Looking at Options 2b and 2c together, having two lanes with buffered bike lanes or a cycle track was the most popular option. Participants liked that these options offered a balance between uses (i.e., public space, parking, patios) while also focusing on safety for those who bike. Some participants liked the flexibility of Option 2b, while others preferred the physical separation offered by the cycle track (Option 2c). In addition, an email submission from an active transportation advocacy group indicated preference for a modified version of Option 2c (two lanes with cycle track). The group also recommended a pilot program and consideration for a hybrid option with a flexible zone.

Participants agreed that four lanes of traffic (Option 3) are not needed for this area. There was a desire to strike a balance for all modes of transportation. Participants liked the idea of slowing down traffic rather than having downtown act as a thoroughfare. Participants indicate mixed support for angled parking.

Comments on evaluation

Participants were asked what was important to them when evaluating these options. A summary of their responses is below.

- Safety and priority for those who walk or bike
- Decreasing the use of vehicles and associated noise
- The flexibility of uses; balance for all road users
- Vibrancy and attractiveness of downtown
- Provision for public space, green space
- Accessibility
- Maintaining a sense of community, increasing opportunities for interaction
- Considerations for the needs of businesses
- Efficiency
- The amount/location of parking
- Environment and climate change
- Health and wellness
- Cost to implement

What We Heard Summary #1: Downtown Renewal

February 2023

Wyndham / Quebec / Douglas Intersection and St. George's Square

Feedback on options

Participants were asked which option(s) best supported (s) their vision for the Wyndham/Quebec/Douglas intersection and St. George's Square. The results are in the figure below. Participants were able to select as many options as they liked.

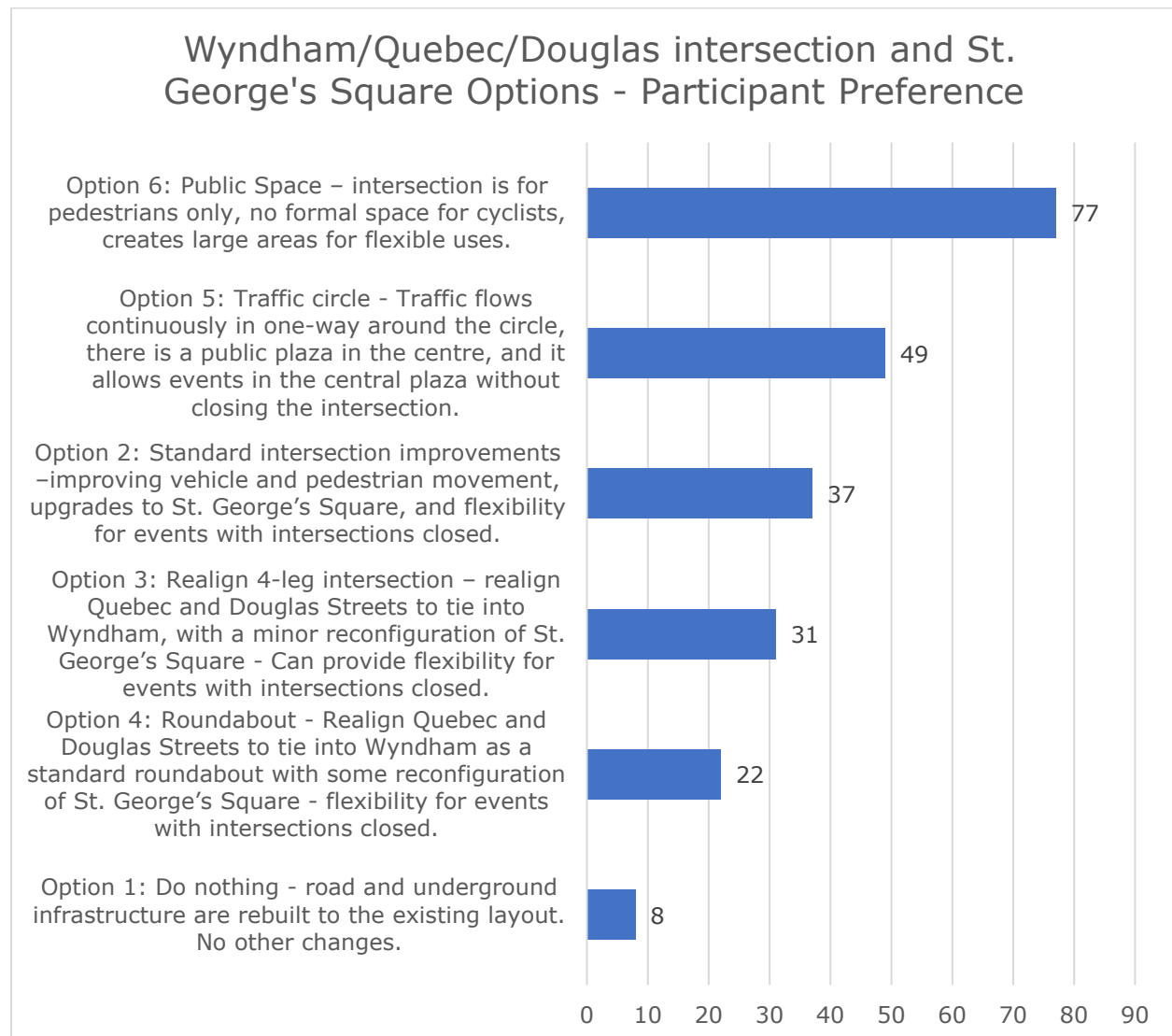


Figure 3: Participant preference for Wyndham/Quebec/Douglas intersection and St. George's Square options, n=154

As shown in Figure 3, Option 6 (public space) was the most popular option. Support was also shown for Option 5 (traffic circle).

Several participants indicated that they would like to see St. George's Square as a truly public space (Option 6), given it is the "heart of downtown". It was felt that Option 6 would provide a pleasant atmosphere and the most safety for pedestrians.

What We Heard Summary #1: Downtown Renewal

February 2023

Participants noted that accommodations need to be made for cyclists, those with accessibility concerns, emergency vehicles and deliveries.

Other participants liked the balance offered by Option 5 (traffic circle), providing flexibility for public space while also accommodating traffic flow through the area. The ability to access the public space without closing the intersection was seen as a benefit. Others noted that they would not want to attend an event with traffic around them. Participants note that the safety of pedestrian crossing and access to the public space will need to be considered.

In contrast, an email submission from an active transportation advocacy group indicated preference for Option 2 (standard intersection improvements) with modifications that include a pilot study, consideration of closing Douglas to vehicles and making Douglas bi-directional for bikes and remove on-street parking.

Comments on evaluation

Participants were asked what was important to them when evaluating these options. A summary of their responses is provided below.

- Safety for those who walk or bike
- Traffic flow for all methods of travel; balance the needs of road users
- Liveability, opportunities for gathering and placemaking
- Access to local businesses; business vibrancy
- Cost of improvements
- Accessibility
- Beauty and aesthetics
- The flexibility of public spaces
- Noise reduction and overall comfort for users of the space
- Environment and air quality
- Access to green space
- Degree of speed reduction or traffic calming
- Consideration of the fountain in the final design

Macdonell Bridge and Allan's Structures

The study aims to identify recommendations that will:

- Address structural deficiencies identified in recent bridge inspections
- Enhance road safety, operations, and connectivity for people who walk, ride, bike, take transit and drive to support the community-building goals of the City
- Improve traffic operations and safety at the Wellington / Woolwich / Macdonell intersection

Feedback on goals

Participants were asked if any other goals should be considered for the study. A summary of their responses is provided below.

What We Heard Summary #1: Downtown Renewal

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- Consideration of a dedicated/protected crossing to increase safety for people who walk or bike across the structures and those with accessibility needs; reduce focus on cars
- Connections of existing trails across the structures
- Aesthetics and attractive design of the structures; opportunity for creating a "gateway" to downtown and acknowledge local history
- Reducing environmental harm and protecting/naturalizing the Speed River, support/enhance local wildlife and fish; consider the river's history and possible future use
- Considerations for climate change and flooding risks
- Consider hydro-electric power generation

Participants were generally not supportive of the "do nothing" options or the options to remove the bridge(s). Some participants, including the correspondence from the active transportation advocacy group, asked for an option for a walking/cycling underpass.

Feedback on options

Many participants requested additional information about the structures, the options presented, and their potential impacts to make an informed response.

Participants wanted to know more about the traffic flow through the area, existing connections and usage, and how this might be impacted by removing the bridge(s) and/or closing them to car traffic. Participants also wondered about the environmental implications of dam modifications and the potential impacts on the river and wildlife. There were also requests to learn more about the costs and trade-offs of various options. Participants also encouraged the project team to consider the downtown as a whole, including, for example, the [Ward to Downtown](#) pedestrian bridge.

Participants were asked which option(s) they preferred for the Macdonell Bridge, Allan's Dam Bridge, Sluiceway, and Spillway. The results are in the following figures.

What We Heard Summary #1: Downtown Renewal

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Macdonell Bridge Options

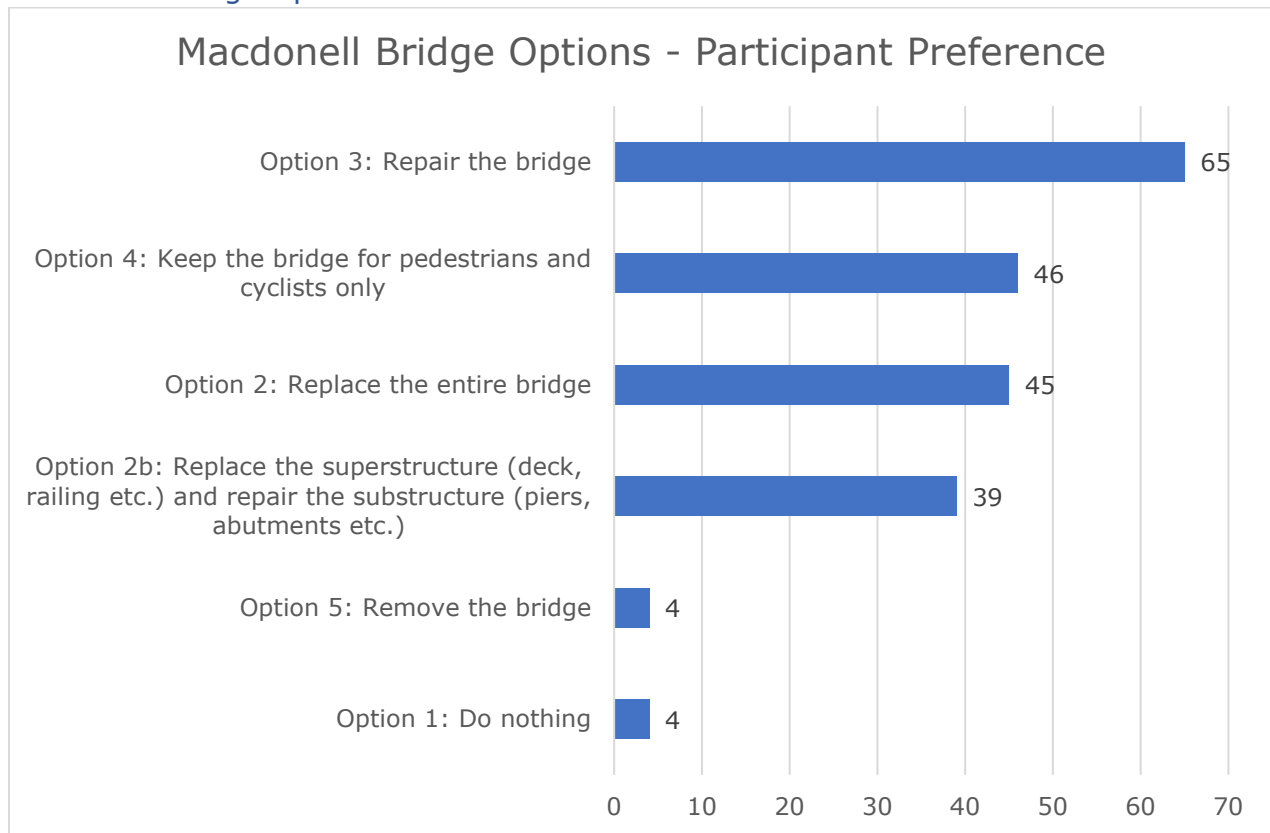


Figure 4: Participant preference for Macdonell Bridge options, n=139

Participants most favoured Option 3 (repair the bridge), with support for Option 4 (keeping the bridge option for pedestrians and cyclists only) and replacing the entire bridge or the superstructure (Options 2 and 2b).

While some participants indicated a desire to keep the Macdonell Bridge open for all road users, there was a widespread call to accommodate better the safety of those who walk or bike. There was a concern about removing the bridge in terms of traffic flow and access for emergency vehicles.

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Allan's Dam Bridge Options

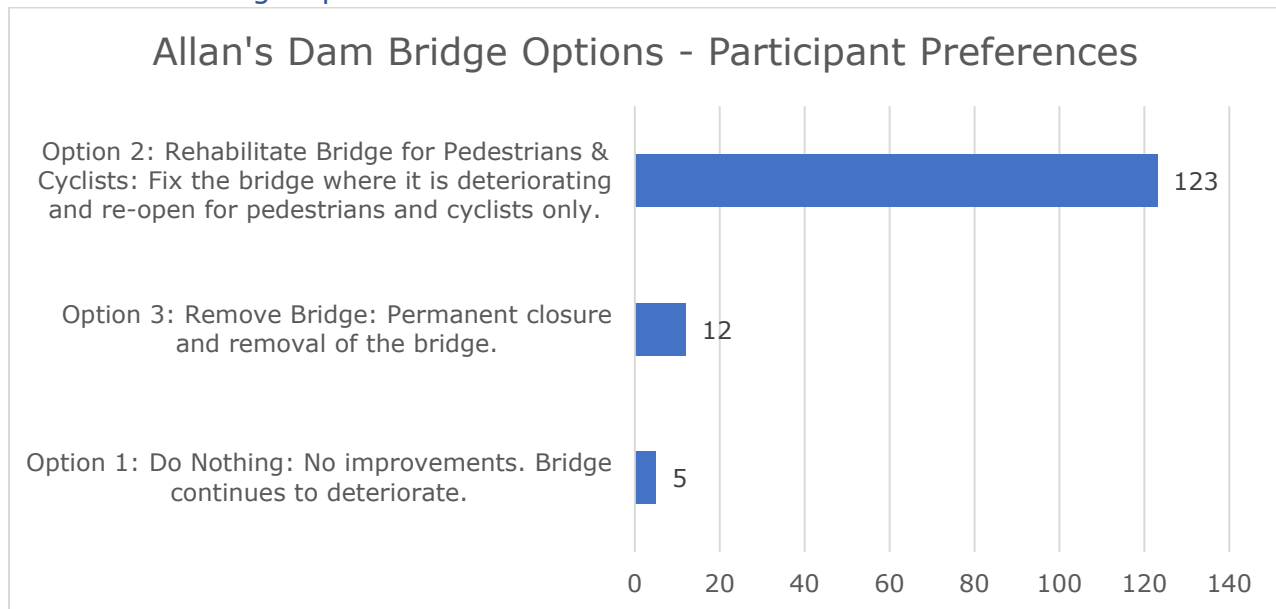


Figure 5: Participant preference for Allan's Dam Bridge Options, n=137

As shown above, there was significant support for Option 2 (rehabilitating the bridge for pedestrians and cyclists). Participants felt that rehabilitating the bridge for pedestrians and cyclists would provide a safe connection for these road users and encourage more residents to walk or cycle downtown. Participants note the historical importance of the bridge and would like to see it maintained.

A few participants were concerned that creating a pedestrian and cycling bridge would be a duplication of the Ward to Downtown bridge located nearby and, therefore, not a good use of City funds. Participants encouraged the project team to examine all the bridges downtown holistically.

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Allan's Dam Sluiceway & Spillway Options

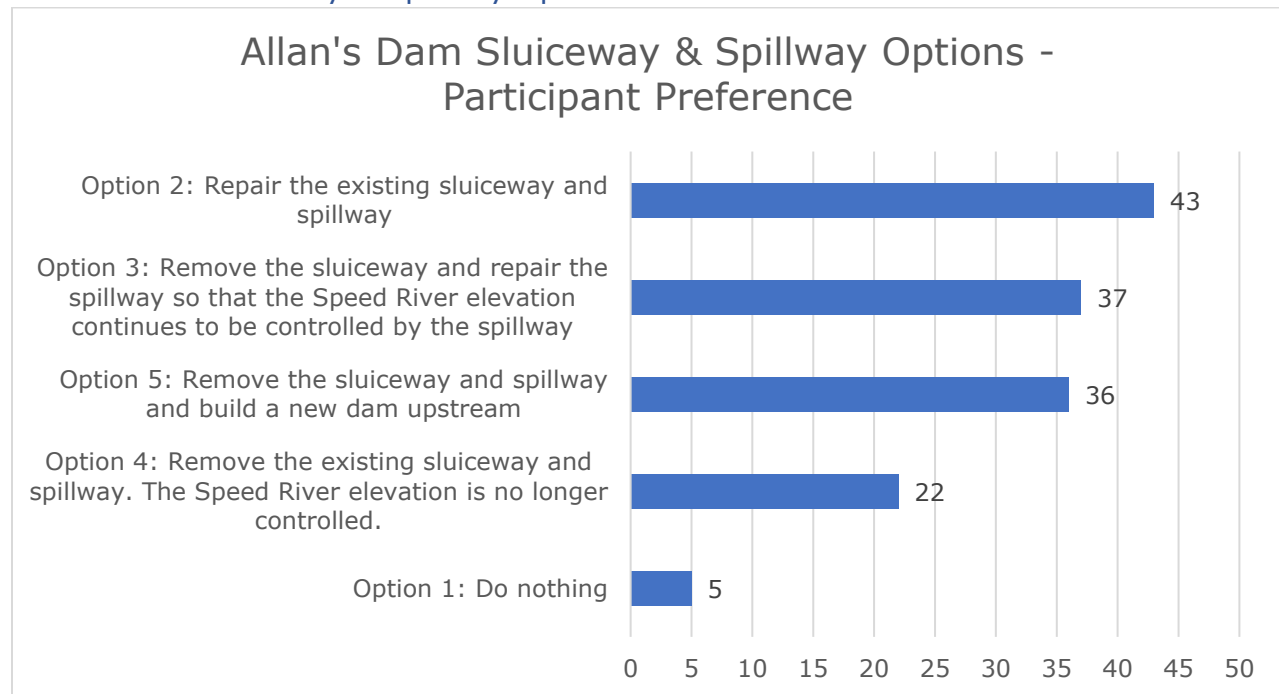


Figure 6: Participant preference for Allan's Dam sluiceway and spillway options, n=115

Participants most favoured Option 2 (repair the existing sluiceway and spillway). There was also support for Option 3 (remove the sluiceway and repair the spillway) and Option 5 (remove the sluiceway and spillway and build a new dam upstream).

Participants noted the importance of flood control. More information was requested on how these options would impact flood protection. Several participants wanted to see the river return to its natural state.

Comments on evaluation

Participants were asked what was important to them when evaluating these options. A summary of their responses is provided below.

- Safety for those who walk or cycle
- Maintaining connections for all road users, particularly those who walk or cycle
- Maintaining the flow of vehicle traffic
- Environmental considerations; flood protection
- Wildlife, biodiversity, and ecosystem health; river protection and restoration
- Cost; impact on taxpayers
- Aesthetics and beautification as a "gateway" to downtown
- Protecting the heritage or character of the area
- Livability
- Accessibility

What We Heard Summary #1: Downtown Renewal

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Additional feedback

Participants provided the following additional feedback for consideration.

- Be bold, forward-thinking and approach Downtown Renewal holistically
- Consider the needs of local businesses in the process
- Ensure those who live downtown are included in the process, and key stakeholders, such as cyclists, to achieve a balance of perspectives
- Focus on the needs of those who walk or cycle rather than solely on the movement of cars
- Look to other cities that have had success in renewing their downtown cores
- Consider climate change, sustainability and the City's net-zero goals
- Ensure equitable access and accessibility for all
- Consider a pilot of a pedestrian-only area
- Consider future transportation uses (e.g., micro-mobility)
- A desire for traffic calming and speed reduction downtown
- Concern for the safety of pedestrians and cyclists at the Quebec / Norfolk intersection and Norfolk Street between Church Lane and Cork Street East
- Pavers used on Carden Street and Wilson Street should be replicated elsewhere downtown

Feedback received relating to other City initiatives

During engagement, some comments received were beyond the scope of the Downtown Renewal project. These comments are captured and will be shared with respective city departments to inform other projects and initiatives. Comments were received on the following topics.

- Services and support for those experiencing homelessness
- Crime and police presence
- Preference for certain types of stores or services
- Programming for downtown spaces
- Heritage and preservation of architecture
- Improvements to public transit
- Housing and density

DGBA feedback

Highlights of the feedback provided at the meeting with DGBA are provided below.

- Desire for this project to improve and enhance the economic vitality of Downtown Guelph
- Importance of downtown as a commercial district, which addresses the needs of the business community
- Importance of placemaking; solutions for the downtown need to create a feeling of destination

What We Heard Summary #1: Downtown Renewal

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Next steps

Feedback from the Open House and Have Your Say will be used by the City and its consultants along side research and best practices to inform the information gathering studies (i.e. Capital Implementation Plan, Environmental Assessments of the Macdonell Street bridge and Wyndham Street). Additional opportunities for public and stakeholder engagement will continue as the project progresses.

APPENDIX 12-3-2

Public Open House #2 Summary Report





Guelph Downtown Renewal Winter Engagement 2024 Summary

December 2024

**What We Heard – Winter 2024: Downtown Infrastructure Renewal –
Macdonell and Allan Structures Municipal Class Environmental Assessment**

January 2025

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What We Heard – Winter 2024: Downtown Infrastructure Renewal – Macdonell and Allan Structures Municipal Class Environmental Assessment

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Introduction

Background

The Macdonell Bridge (Structure No. 112), located on Macdonell Street over the Speed River and known to many community members as Allan's Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph.

Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitation, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge, are also required.

In response, and as part of the broader [Downtown Infrastructure Renewal Program](#), the City of Guelph initiated a Municipal Class Environmental Assessment (Class EA) for improvements and modifications to the Macdonell and Allan structures. The study considers options for the entire Macdonell Street Bridge area, including all three structures and the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

The project is being completed as a Schedule "B" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011, 2015, 2023, and 2024).

The Downtown Infrastructure Renewal Program is one of many projects under the overarching Downtown Renewal efforts. The [Downtown Renewal Program](#) will transform and revitalize how Downtown looks, feels, and functions — making it future-ready to support growth to 2051 and beyond.

What We Heard – Winter 2024: Downtown Infrastructure Renewal – Macdonell and Allan Structures Municipal Class Environmental Assessment

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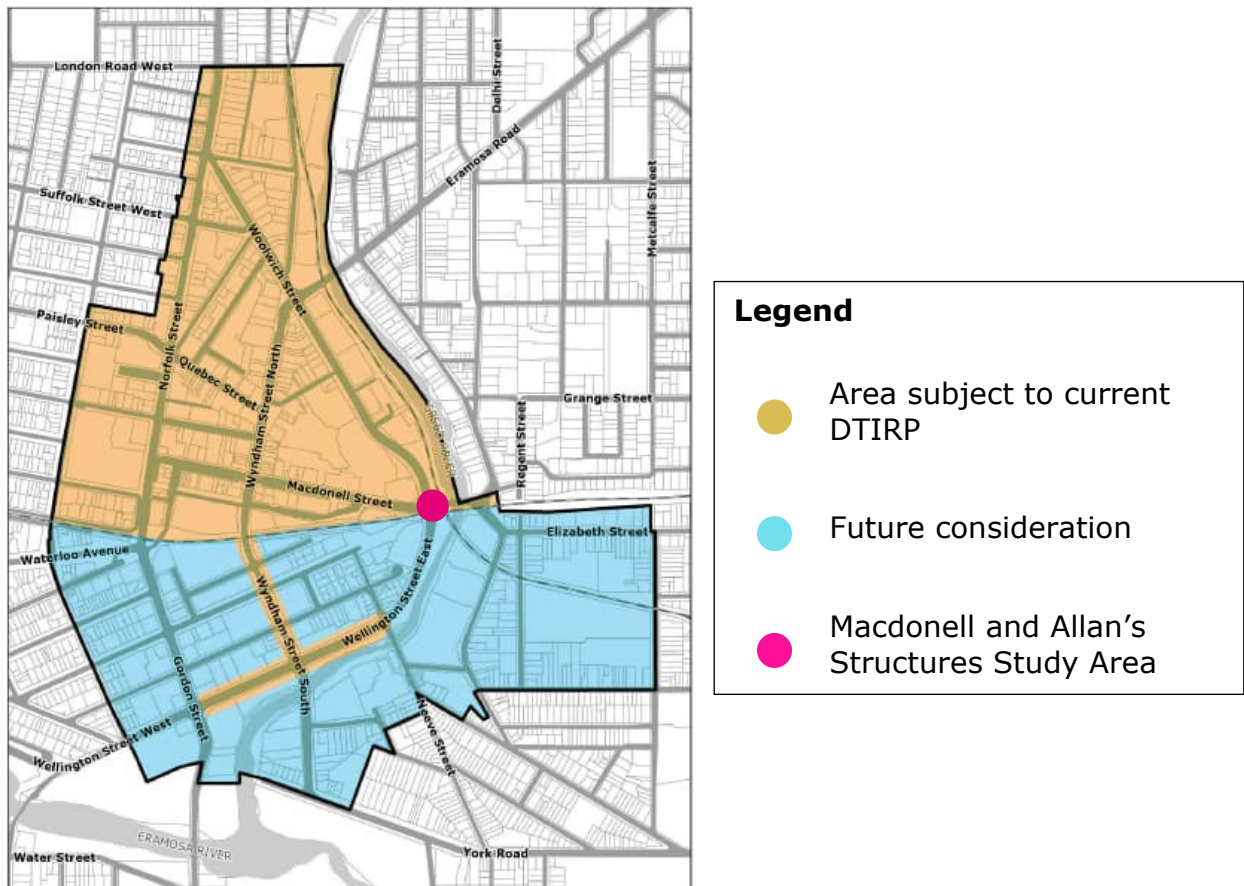


Figure 1: Map of the Downtown Infrastructural Renewal Program (DTIRP) and the Macdonell and Allan's Structures Study Areas

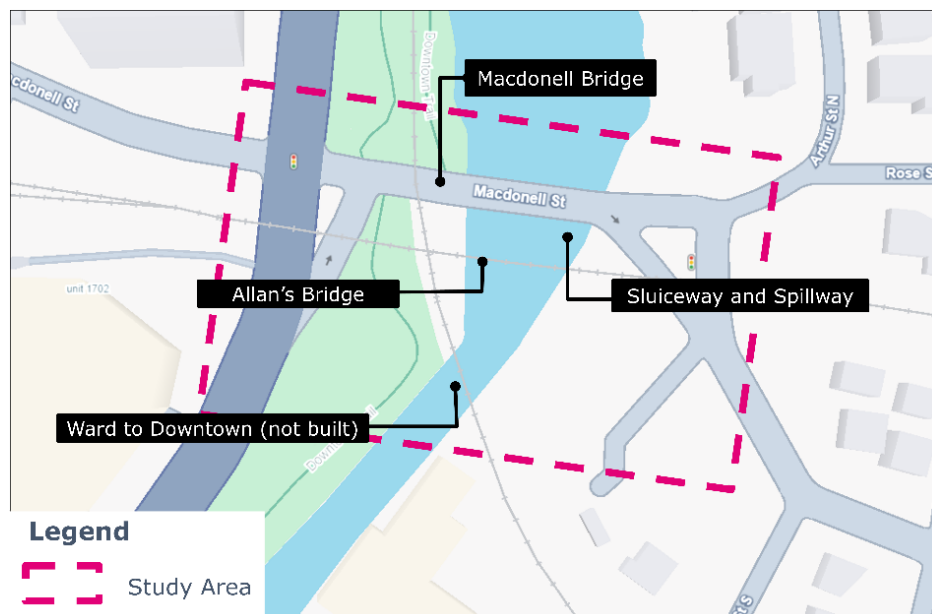


Figure 2: Close-up map of the Macdonell and Allan's Structures Study Areas, highlighting the different structures

What We Heard – Winter 2024: Downtown Infrastructure Renewal – Macdonell and Allan Structures Municipal Class Environmental Assessment

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Figure 1 and 2 show the location of the DTIRP project study area in relation to the Macdonell and Allan Structures Study Area.

Planning for Downtown Infrastructure Renewal began in the spring of 2021. During the planning phase, the study looked at the existing infrastructure and the future needs of Downtown to determine what needed to be updated.

The goals of the Macdonell and Allan Structures environmental assessment include addressing structural deficiencies identified in recent bridge inspections for the Macdonell Bridge, the Allan's Bridge, and the Sluiceway and Spillway. It also includes reviewing options for the Ward to Downtown pedestrian/cyclist crossing and enhancing road safety, operations, and connectivity for vehicles, pedestrians, cyclists, and transit to support the community-building goals of the City.

After evaluating options for what to do with each structure, project staff have put forward the following recommendations:

- **Macdonell Bridge** - Replace and widen the bridge to accommodate active transportation on the north side.
- **Allan's Dam Bridge Structure** - Remove the bridge and have its heritage commemorated in some way.
- **Sluiceway and Spillway** - Rehabilitate and repair the structures.
- **Ward to Downtown Bridge** - Construct a new pedestrian and cyclist crossing south of the Guelph Junction Railway (GJR) rail bridge, with a simpler design than the previously proposed Ward to Downtown bridge.

What we did

The City hosted the fifth public open house as part of the Downtown Infrastructure Renewal Program (DTIRP) on December 9, 2024. The event shared space with several other concurrent city projects that impact the Downtown area. Along with the DTIRP project purpose and goals, the open house shared the options considered for each structure, the criteria used to evaluate each option, and the recommendations for the structures. In addition, ideas for commemorating the Allan's Dam Bridge, including potentially forming a heritage committee, were presented. The open house followed a "drop-in" style format, where materials were displayed in-person and online for public review. Project team members were available in person throughout the open house to provide additional context and answer questions.

After reviewing the display materials, the public was invited to provide feedback through:

- a hard-copy survey at the open house, or
- the [Have Your Say webpage](#).

The survey solicited thoughts and feedback as the recommendations for the Macdonell and Allan Structures Municipal Class EA become finalized.

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Open house and survey feedback are incorporated in the summary below.

- The open house was held on December 9, 2024. At least 75 individuals were in attendance.
- The Have Your Say survey was available from December 9, 2024 – January 12, 2025. Two hundred and three (203) visitors contributed to the survey.

Who we heard from

Almost all (98% of survey respondents) identified themselves as Guelph residents (residing within or outside Downtown).

Survey participants replied to the demographic questions as follows:

Ninety-two (92) were Guelph residents living Downtown, one-hundred-and-seven (107) lived in Guelph outside of Downtown, and four (4) lived outside of Guelph. A few respondents further specified their occupation or relation to Downtown, with eight (8) identifying as students, nine (9) as business owners or operators in the Downtown area, and eight (8) as business owners or operators outside of the Downtown area.

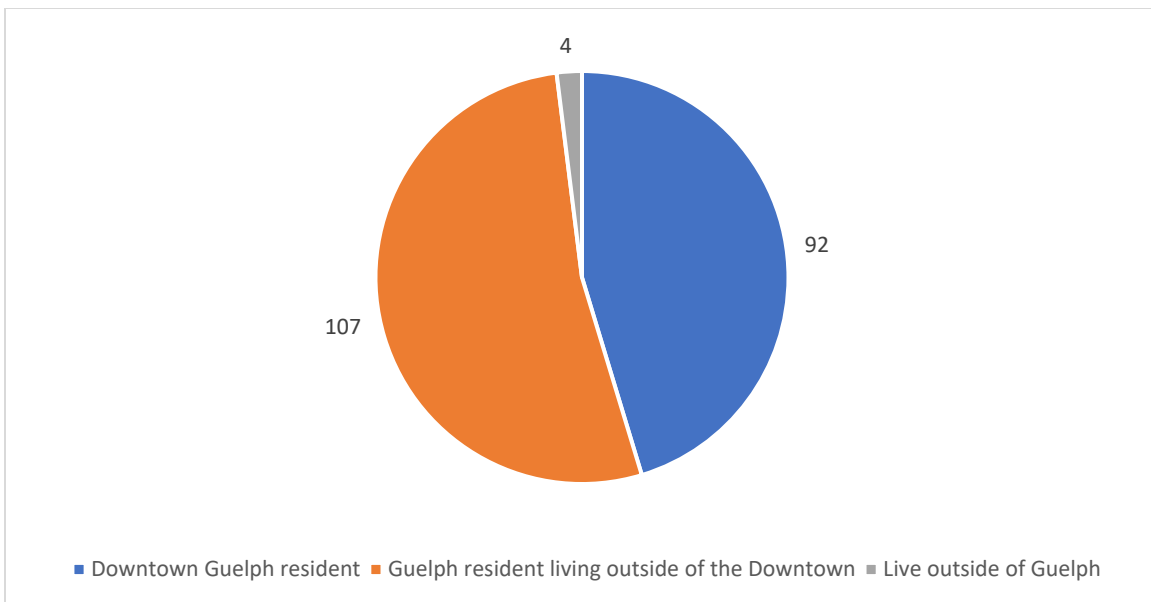


Figure 3: Share of participants who reside in Guelph inside or outside of Downtown or live outside of Guelph (n=203)

What we heard

All structures

Each recommendation for the structures received support from most participants, with an average of 72% of participants choosing "strongly or somewhat agree" across all structures. The Macdonell Bridge's recommendation received the strongest support, with 82% selecting "strongly or somewhat agree," while the

What We Heard – Winter 2024: Downtown Infrastructure Renewal – Macdonell and Allan Structures Municipal Class Environmental Assessment

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Allan's Dam Structure received the weakest support, 59% selecting "strongly or somewhat agree."

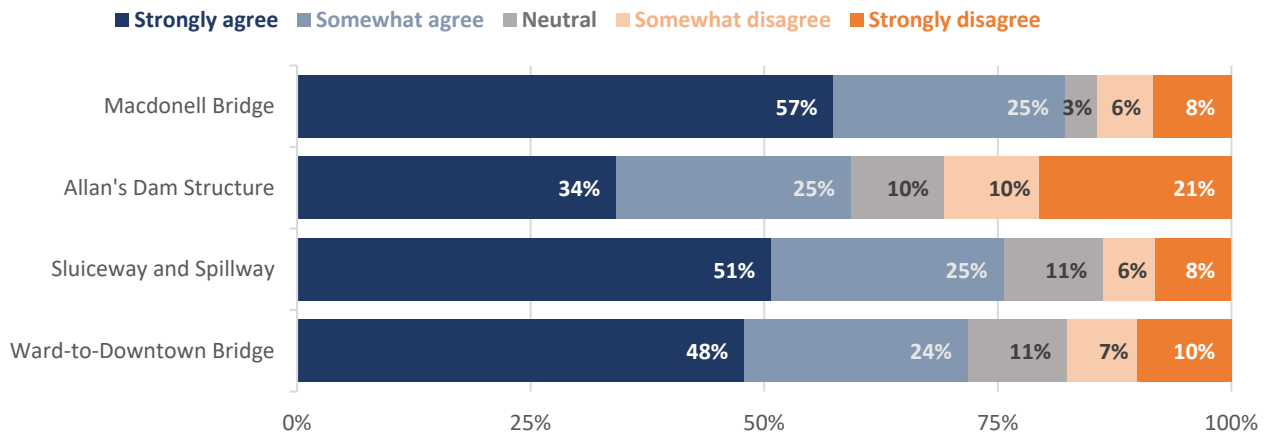


Figure 4: Comparing the Likert-scale scores of the recommendations for each structure.

Comparing sentiment amongst residents and businesses

Average ratings were calculated for each demographic to compare how they rated the recommendations for the structures. The ratings correspond to 1 for strongly disapprove and 5 for strongly approve.

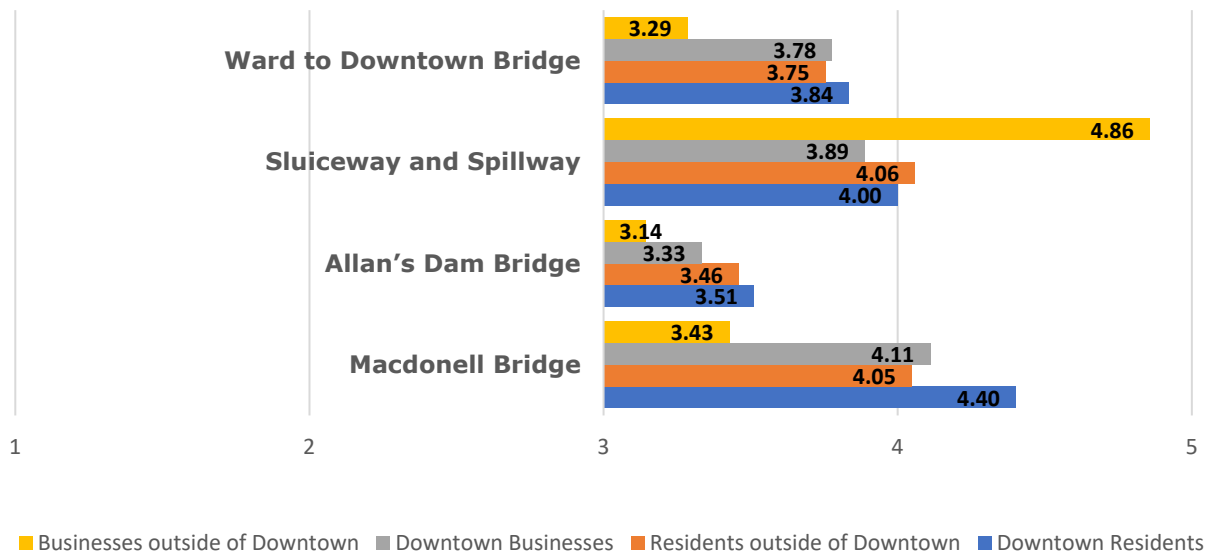


Figure 5: Average rating out of 5 of recommendations for each structure per participant demographic.

Residents living downtown and outside the Downtown rated the recommendations for each structure similarly, with the difference in average rating hovering around 2% for the Ward to Downtown Bridge, the Sluiceway and Spillway, and the Allan's

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Dam Bridge. The largest difference in average rating was with the Macdonell Bridge, where the average approval rating among Downtown residents was around 8% higher with downtown residents. The lower rating from residents outside of Downtown may be explained by those residents' higher usage of the Macdonell Bridge to access Downtown. Suggestions to improve the recommendation provided by some participants do not clarify the difference in rating, as the responses are similar regardless of the residents' location.

The differences in rating were higher between businesses outside of Downtown and within Downtown, as well as between businesses and residents. However, the sample size of businesses was small, with only 17 out of 203 identifying as business owners or operators, and about each half representing a business inside or outside of downtown. Therefore, each answer had a heavier weight when calculating the average. Businesses outside of Downtown gave the recommendations the lowest ratings for all structures except the sluiceway and spillway, where they gave the highest average rating. Very few participants provided detailed reasoning for their rating and echoed sentiments similar to other participants. The large difference in ratings compared to other demographics may simply be due to the low sample size and not indicative of businesses outside of Downtown having a common unique concern. On the other hand, the average rating provided by businesses in the Downtown area was more aligned with the average rating of residents both outside and inside the Downtown.

Both the average ratings and the detailed feedback provided by residents indicate that sentiment about the recommendations for the structures is generally shared among these different groups.

Macdonell Bridge

Recommendation

The EA recommends the removal of the current Macdonell Bridge and replacing it with a new, wider bridge that would accommodate a multi-use path on the north side and a wider sidewalk on the south side.

This recommendation received strong support. Ideas to enhance the recommendation are captured below:

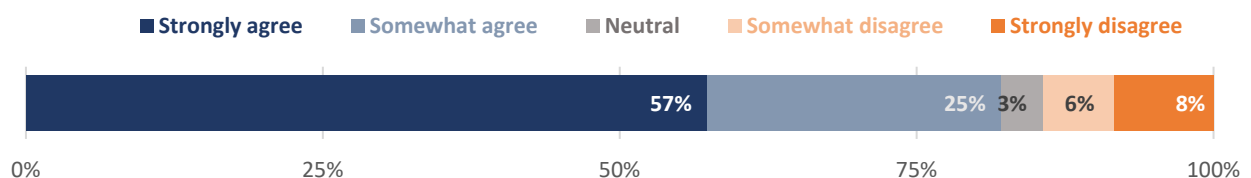


Figure 6: Likert-scale scores for the recommendation for the Macdonell Bridge.

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[Amongst those who agreed with the recommendation](#)

Improved safety for all users

Most participants called for improved safety by widening the bridge and reconfiguring adjacent intersections. Some ideas include providing more opportunities to cross the street, removing slip lanes, and adding more traffic lights.

Improvements for pedestrians and active transportation

Along with safety, many participants provided specific suggestions for a better experience for non-vehicular road users, resulting in improved safety for all. For the active transportation route, many participants encouraged separating it as much as possible from vehicles, through grade separating the path, using barriers and bollards. For pedestrians, many participants suggested wide sidewalks on both sides of the bridge to help avoid conflicts between pedestrians and faster-moving cyclists on the active transportation path.

Leisure and heritage

Some participants suggested considering the bridge as a destination rather than something to pass through. Ideas included adding seating to watch the water, considering sightlines to important landmarks, and designing the bridge to reflect the area's heritage.

[Amongst those who disagreed with the recommendation](#)

Location of bike lanes

Some participants were against bike lanes on the Macdonell Bridge and felt that adding them was unnecessary. Some suggested that bike lanes or active transportation routes should be completely separated and provided on another structure.

Costs and need for change

A few participants expressed that funds should not be spent on the bridge and should be kept the way it is.

Evaluation criteria

When evaluating the options for the Macdonell Bridge, participants ranked traffic and environmental and climate change impacts as the two most important evaluation criteria. While still deemed important, costs, heritage, archaeological and cultural impacts were ranked lower, with about 25% of participants ranking each as very important.

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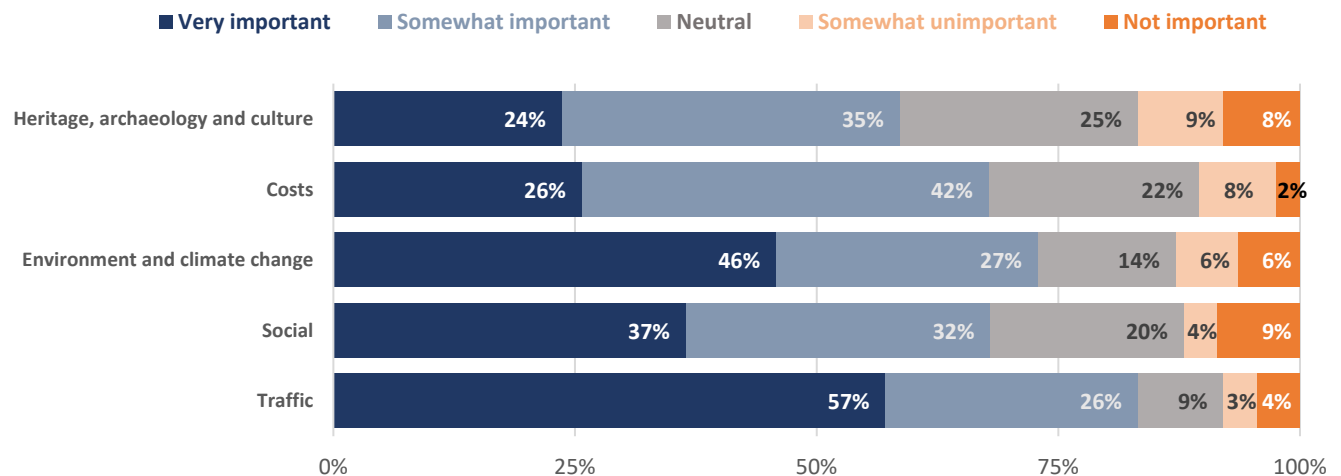


Figure 7: Likert-scale scores for each evaluation criterion for the MacDonnell Bridge.

Participants were asked to share their thoughts about the evaluation criteria and were invited to suggest new criteria for consideration.

Participants overwhelmingly commented on the need for improved safety for all road users, especially pedestrians and cyclists, on the Macdonell Bridge and surrounding intersections. Participants proposed an explicit safety criterion or clearly defining the existing traffic criterion to include traffic calming, accommodating all road users, and not designing around increasing vehicle use.

A few participants also proposed a criteria for visual appeal, though they acknowledged that this may be part of the social or heritage, archaeological and cultural impacts criteria.

Allan's Dam Bridge

Recommendation

The EA recommends fully removing the Allan's Dam Bridge and exploring opportunities to commemorate the bridge's heritage.

This recommendation was supported by most participants, with over half choosing strongly or somewhat agreeing. Compared to the other structures in the study, it is the most controversial, with the lowest proportion of participants choosing "strongly agree" votes and the highest proportion of "strongly disagree." Ideas and suggestions to support or reject the recommendation are summarized below:

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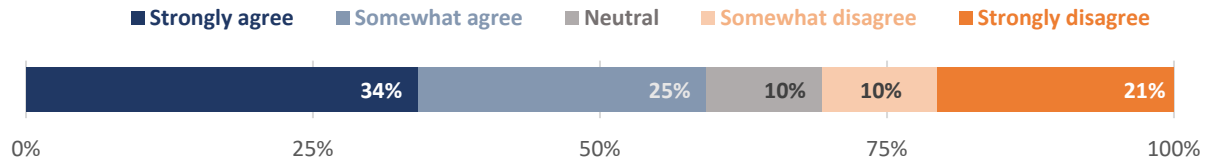


Figure 8: Likert-scale scores for the Allan's Dam Bridge recommendation.

Among those who agreed with the recommendation

Heritage considerations

Many participants are against spending resources on heritage commemoration for the bridge, with a few specifying that a plaque would be enough.

Active transportation

A few participants who agreed with the recommendation noted that the area's pedestrian and active transportation capacity is improved through other means, as planned with the Macdonell Bridge and the Ward to Downtown Bridge.

Among those who disagreed with the recommendation

Repurpose

Most participants who disagreed with the recommendation want the bridge preserved and repurposed into a vehicle-free space. Ideas include turning the Allan's Dam Bridge into a park, a spot for fishing, or watching the water. Many suggested the bridge should be dedicated as an exclusive active transportation route instead of on the Macdonell Bridge and the Ward to Downtown Bridge.

Heritage

Most participants who disagreed also wanted to preserve the bridge for its heritage value, expressing that keeping the bridge was preferred over heritage commemoration.

Evaluation criteria

Most participants who provided feedback on the evaluation criteria expressed confusion about why traffic was evaluated, resulting in 29% of respondents deeming it unimportant. Otherwise, feedback on the other criteria focused on heritage, archeological and cultural impacts, echoed the sentiment shared in the feedback for the recommendation regarding preservation and repurposing instead of removing the bridge.

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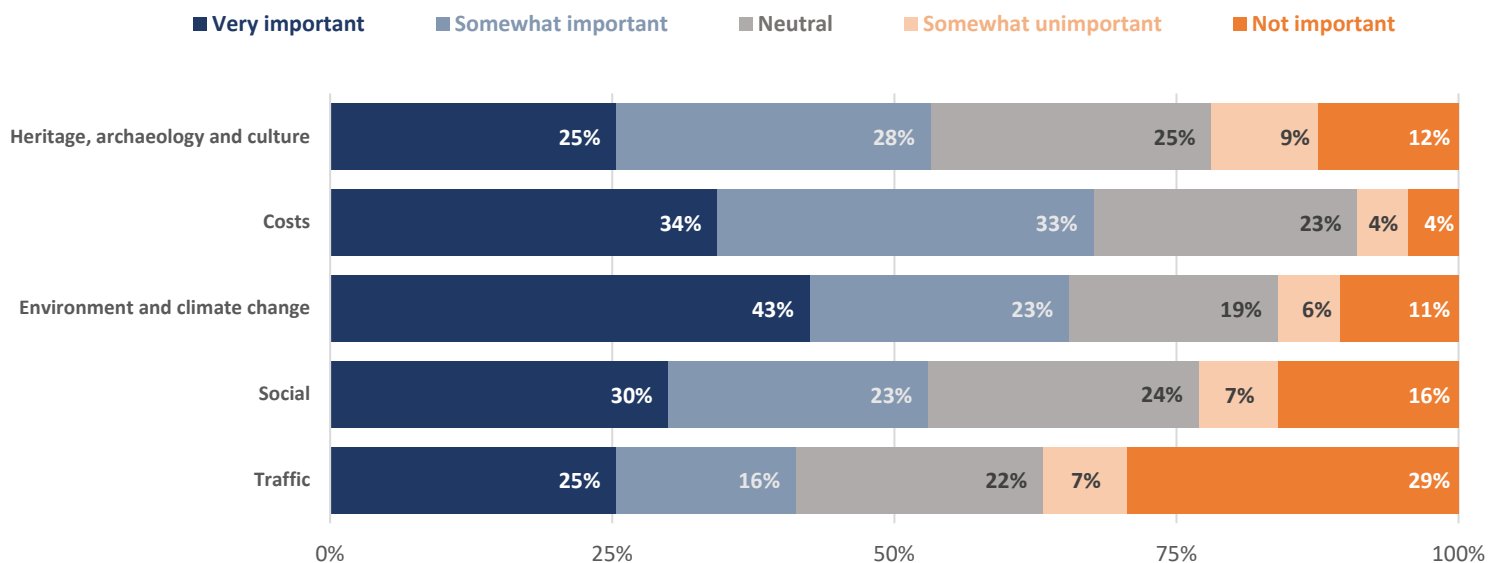


Figure 9: Likert-scale scores for each Allan's Dam bridge evaluation criteria.

Heritage commemoration and formation of a committee

Participants were asked if they would support the development of a heritage commemoration committee as part of the recommendation for the Allan's Dam Bridge, if they were interested in being contacted by the City, and if and when they were ready to form a committee.

The chart below compares the support for forming a committee between participants who agreed with the recommendation and those who disagreed. For the sake of comparison, neutral responses were excluded from this chart. While approximately two-thirds of participants agreed and one-third disagreed, support for forming a committee was nearly evenly split among respondents, with just over half not in favour. A larger portion of those opposed to forming a committee came from those who agreed with the recommendation. This outcome is reflected in the feedback for enhancing the recommendation, where many participants wanted the bridge removed without further resources to be spent on forming the committee. Interestingly, a small portion of respondents opposed to forming a committee were still interested in participating in joining if the City forms one.

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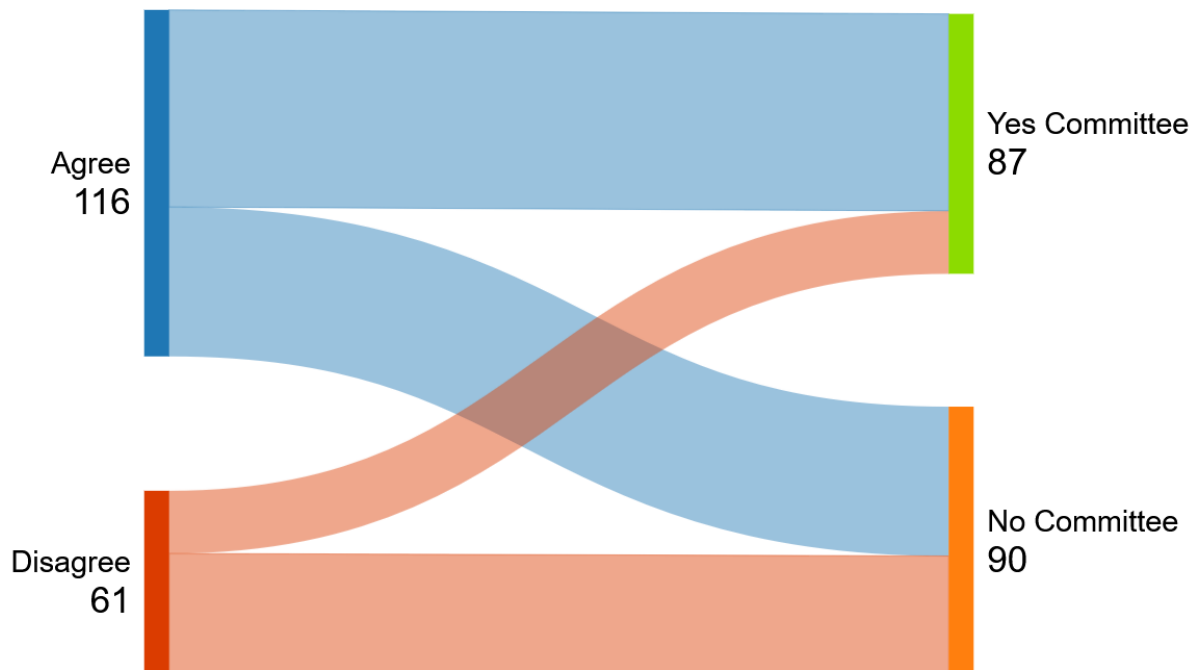


Figure 10: Chart following participants' responses on agreeing or disagreeing with the Allan's Dam Bridge recommendation and creating a heritage committee.

Sluiceway and Spillway

Recommendation and further considerations

The EA recommends undertaking necessary repairs to the existing sluiceway and spillway with full rehabilitation.

This recommendation received strong support. Participants commonly expressed a need to improve the river's health, with different ideas on how to achieve this from participants who agreed or disagreed with the recommendation. These ideas and other themes are captured below:

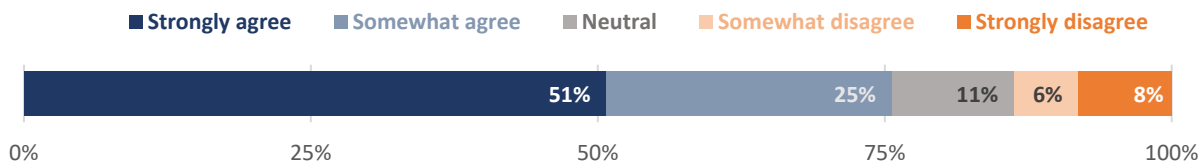


Figure 11: Likert-scale scores for the recommendation for the sluiceway and spillway.

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[Among those who agreed with the recommendation](#)

Water interactions

A few participants suggested modifying the structures to allow fish and even people in boats to pass through.

Visual aesthetics and cleanliness

Some participants hoped rehabilitation would improve the current sluiceway and spillway structures' aesthetics and suggested using natural materials such as stone. In addition, a few participants suggested finding a design that could prevent debris from building up and make it easily cleanable if it does.

[Among those who disagreed with the recommendation](#)

Naturalization

Most participants who disagreed with the recommendation advocated for fully removing the structure. This would allow for the naturalization of the river. Participants suggested that this would improve the river's health, cleanliness, and natural habitats and avoid flooding in the event of a large surge of water.

Cost

Relatedly, some participants raised concerns over the ongoing and lifetime costs of keeping the sluiceway and spillway instead of removing them.

Evaluation criteria

Participants chose environmental and climate change impacts as the most important evaluation criterion. Feedback on the criteria was aligned with the feedback on the recommendations. Whether they agreed or disagreed with the recommendation, most participants felt that the sluiceway and spillway had an important impact on the environment and future climate change events that could affect water levels.

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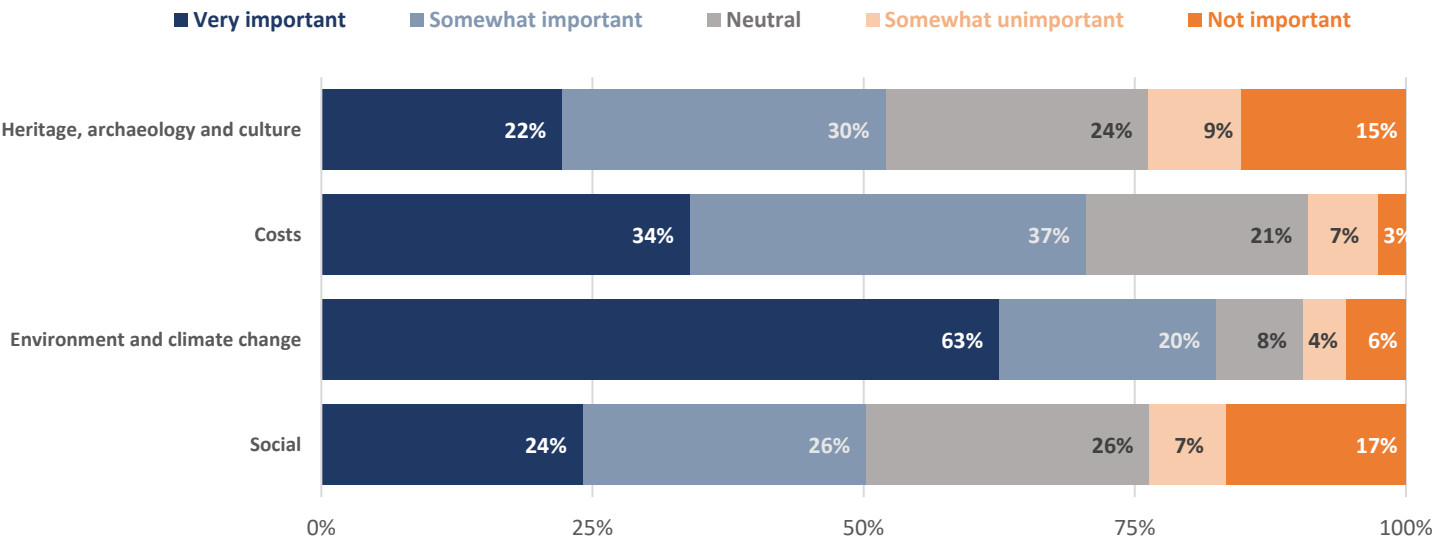


Figure 12: Likert-scale scores for each evaluation criteria for the sluiceway and spillway.

Ward to Downtown Bridge

Recommendation and further considerations

The EA recommends constructing a multi-use trail bridge on the south side of the GJR Rail Bridge based on a modified design that is simpler than the original design which was cancelled in 2023.

This recommendation received general support. Those who supported the recommendation emphasized that it would enhance the safety of the study area. Participants who disagreed felt it was redundant with the other structures. Further ideas to support or disagree with the recommendation are provided below:

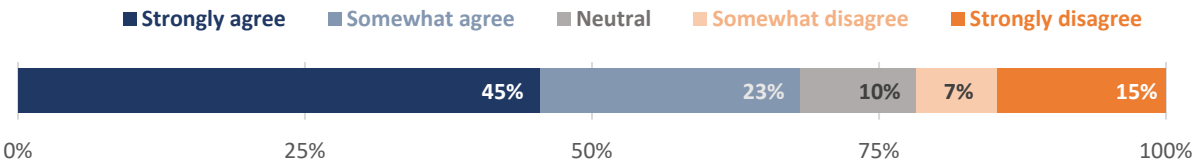


Figure 13: Likert-scale scores for the Ward to Downtown Bridge recommendation.

Among those who agreed with the recommendation

A few participants recommended adding a widened portion to the bridge to allow for a lookout onto the river, which could include benches. When considering cost, some participants suggested that savings from going with a simpler bridge design can go towards improving active transportation infrastructure in the connecting areas. Finally, numerous participants emphasized the importance of a good visual aesthetic for the bridge that honours the area's heritage while still achieving the goal of a simpler design.

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Among those who disagreed with the recommendation

Participants who disagreed with the recommendation generally felt it was unnecessary due to the other structures nearby in the study area. Many felt that the active transportation path included in a widened Macdonell Bridge would make a Ward to Downtown crossing redundant. Another idea shared by a few participants is to use the Allan's Dam Bridge as a dedicated vehicle-free crossing instead of removing it.

Evaluation criteria

The criteria were ranked in a fairly balanced manner, with traffic, social, and environmental and climate change impacts ranking as the most important.

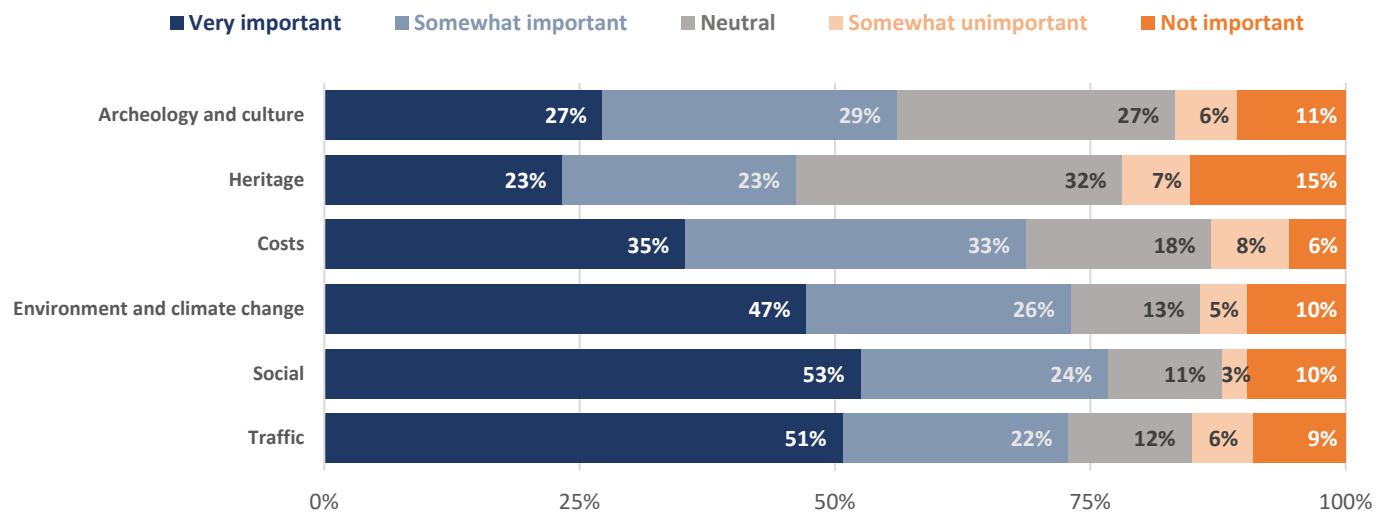


Figure 14: Likert-scale scores for each Ward to Downtown Bridge evaluation criteria.

Feedback on the evaluation criteria echoes the same sentiments as the feedback shared for the recommendation, namely the concern over the cost when there are other crossing options close by.

Regarding neutral feedback

Throughout this report, feedback on the recommendations for the structures was categorized by those who "agree" and "disagree." Most participants who selected "neutral" did not leave further feedback that could be included in the report. For the few that did, their feedback was incorporated into the category that fit best.

Next steps

As part of the Class Environmental Assessment process, the City and its consultants will review all input from the open house and the Have Your Say survey to inform the final recommendations.

APPENDIX 12-4

Public Open House Materials



APPENDIX 12-4-1

Public Open House #1 Slides



Welcome

Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies

Please review the materials and provide your comments through the survey
or online, by November 16, 2022.

Or attend the Public Open House, November 2, 2022.
Drop in between 6:30 – 8:30

Details available on Haveyoursay.Guelph.ca



**Downtown
Renewal**

Creating a place for everyone.

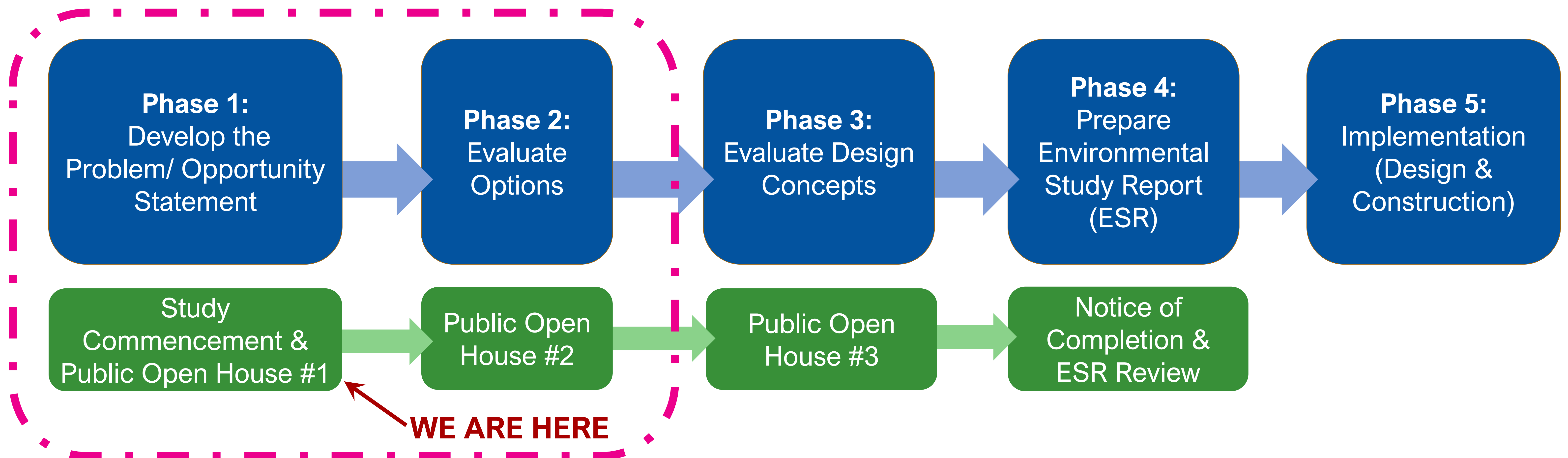


Project Approach and Timeline



What is the Municipal Class Environmental Assessment Process?

- Ensures all reasonable options are considered.
- Reduces impact on the natural, cultural, social and economic environment.
- Input from the public, stakeholders and technical agencies is essential.



— . — Wyndham Street Class EA

What is the Downtown Renewal Project?

Downtown Guelph is a thriving and vibrant place. It also has aging water and sewer pipes, roads and sidewalks. Some of it is over 100 years old and needs to be replaced. Replacing and improving this downtown infrastructure is key to Guelph's overall long-term sustainability. Through downtown renewal we will:

- Improve the way we move through downtown, for people who walk, ride bikes, take transit and drive.
- Replace aging infrastructure (underground pipes, sidewalks etc.) to support development.
- Build flexible streets to create beautiful and vibrant public spaces.

This all contributes to the economic vitality of Downtown Guelph.



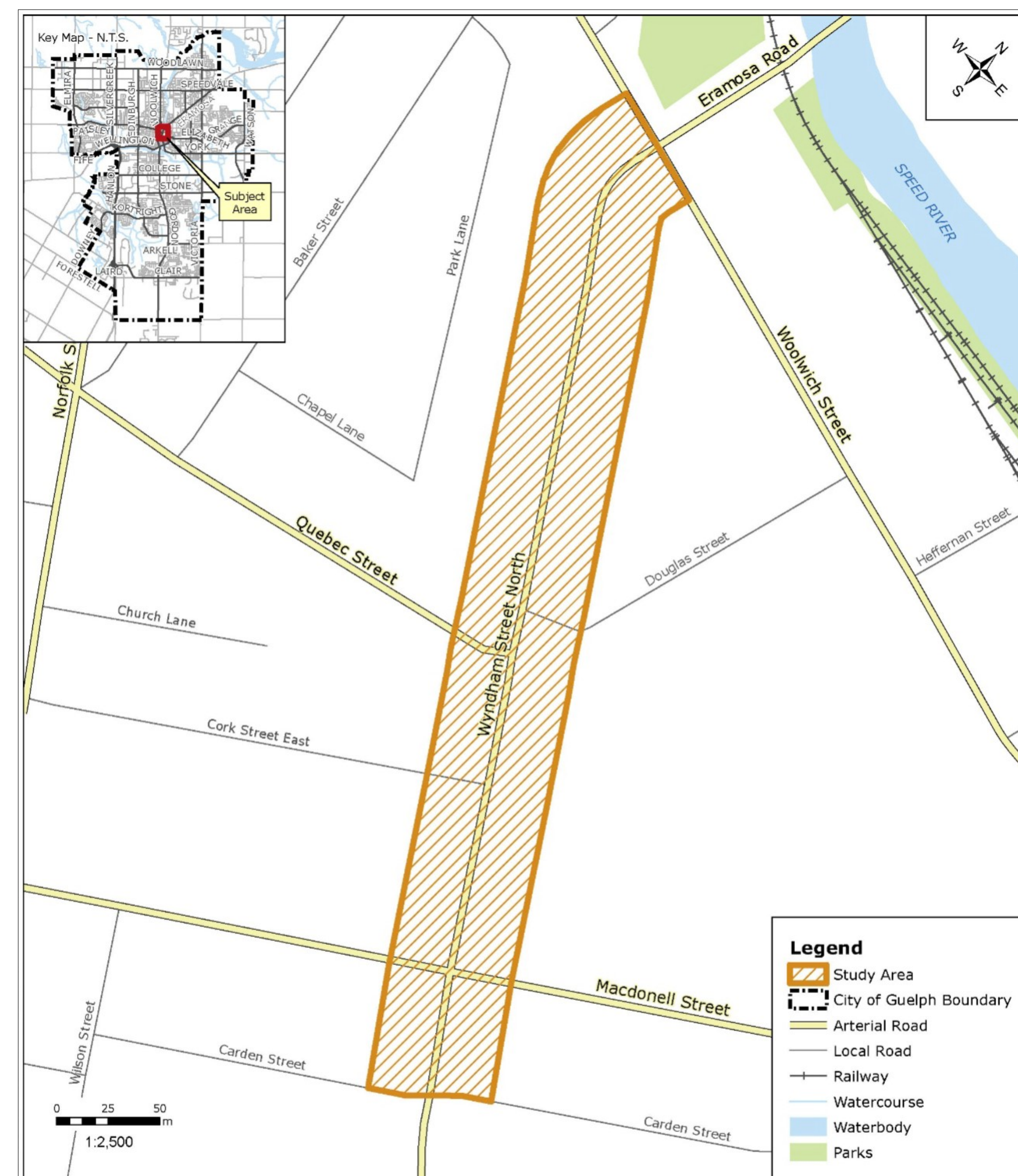
Note: Map is for informational purposes only and is not to scale

4a) Downtown Renewal Study Area

Wyndham Street EA – Study Area and Goals

One of the Environmental Assessments focuses on Wyndham Street between Carden Street and Woolwich Street. The goals are to:

- Enhance road safety, operations, and connections for people who walk, ride bikes, take transit and drive.
- Improve how the Wyndham Street /Quebec Street / Douglas Street intersection (and St. Georges Square) functions.



5a) Wyndham Street Class EA Study Area

Wyndham Street – Options

1. Do Nothing

2. Two-Lanes

- a. Shared Use
- b. Protected Bike Lanes
- c. Bike path

3. Four-Lanes

4. Public Space (no vehicle lanes)

Note: The options presented focus on the number of lanes available for vehicles and the types of accommodation for cycling.

The decision made on these two elements will determine how the roadway will function and how much space is available within the rest of the roadway for parking and other uses such as events, plantings or seating areas.

Consider how flexible spaces in a street can play different roles in our community such as seasonal patios, seating areas, parking or planting zones areas.

Option 1: Do Nothing

- Pavement and underground infrastructure is replaced, and the street configuration stays the same.
- Four-lanes for vehicles.
- One lane can be closed and used for other flexible purposes (i.e., events, patios) with seasonal barriers.
- Cyclists continue to share the road without added protection.

Note: This option is required to be considered under the Municipal Class EA planning process as a baseline for comparison.



7a) Do-Nothing Cross-Section



7b) Existing Cross-Section

What's working well in downtown already?

Option 2a: Two-lanes with Shared Use

- Two-lanes for vehicles.
- Cyclists share the road with vehicles and have no additional protection.
- Parking areas can be used for other flexible purposes.



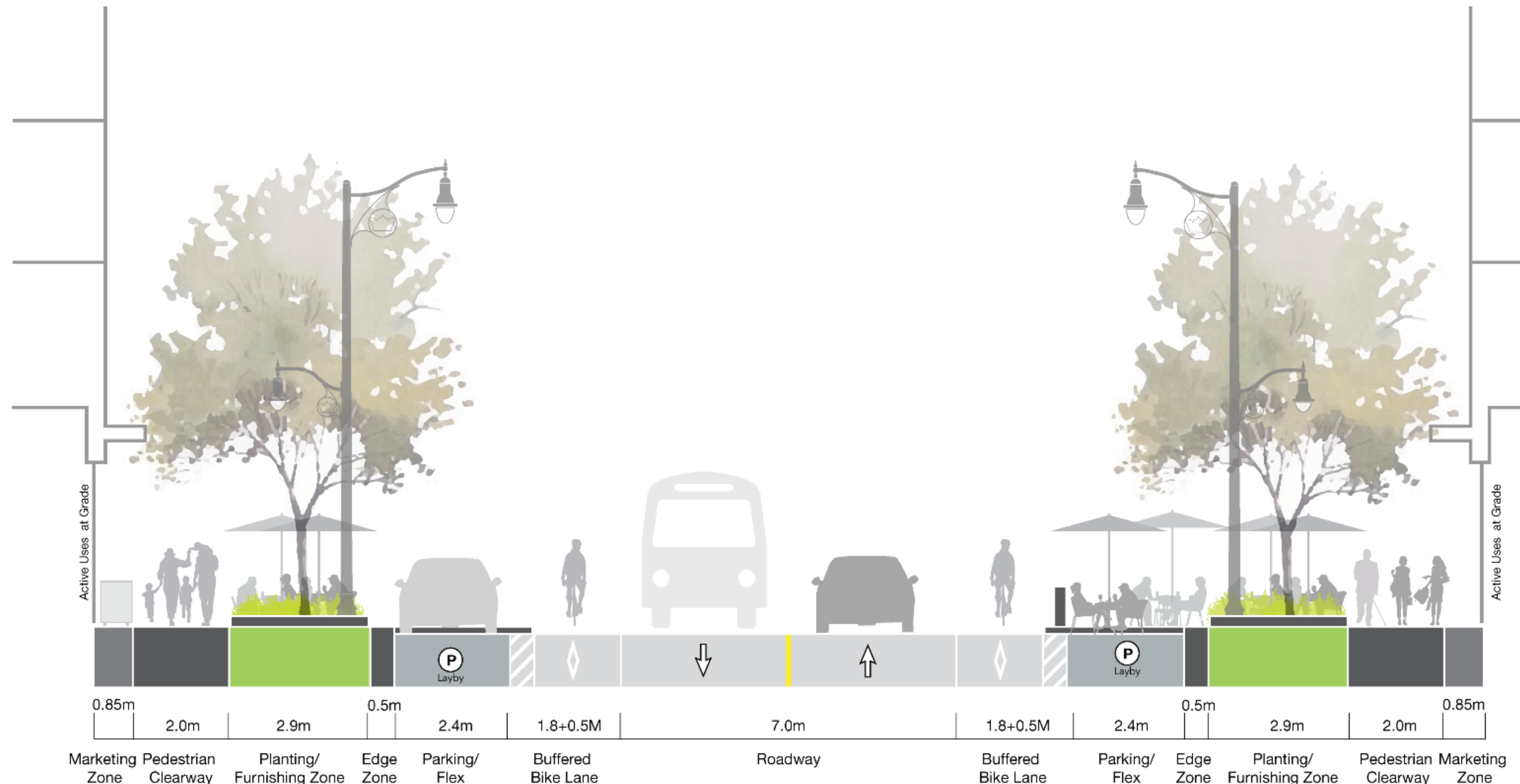
How do these changes work for people who drive, walk, cycle and take transit?

How do these changes work for downtown businesses?

8a) Two-lane Configuration with Shared Use Lanes

Option 2b: Two-lanes with Buffered Bike Lanes

- Two-lanes for vehicles.
- Bike lanes are separated from vehicles and parking areas with a painted buffer.
- Parking areas can be used for other flexible purposes.



9a) Two-lane Configuration with Buffered Bike Lanes

Think about your last visit downtown. How would this option change your experience?

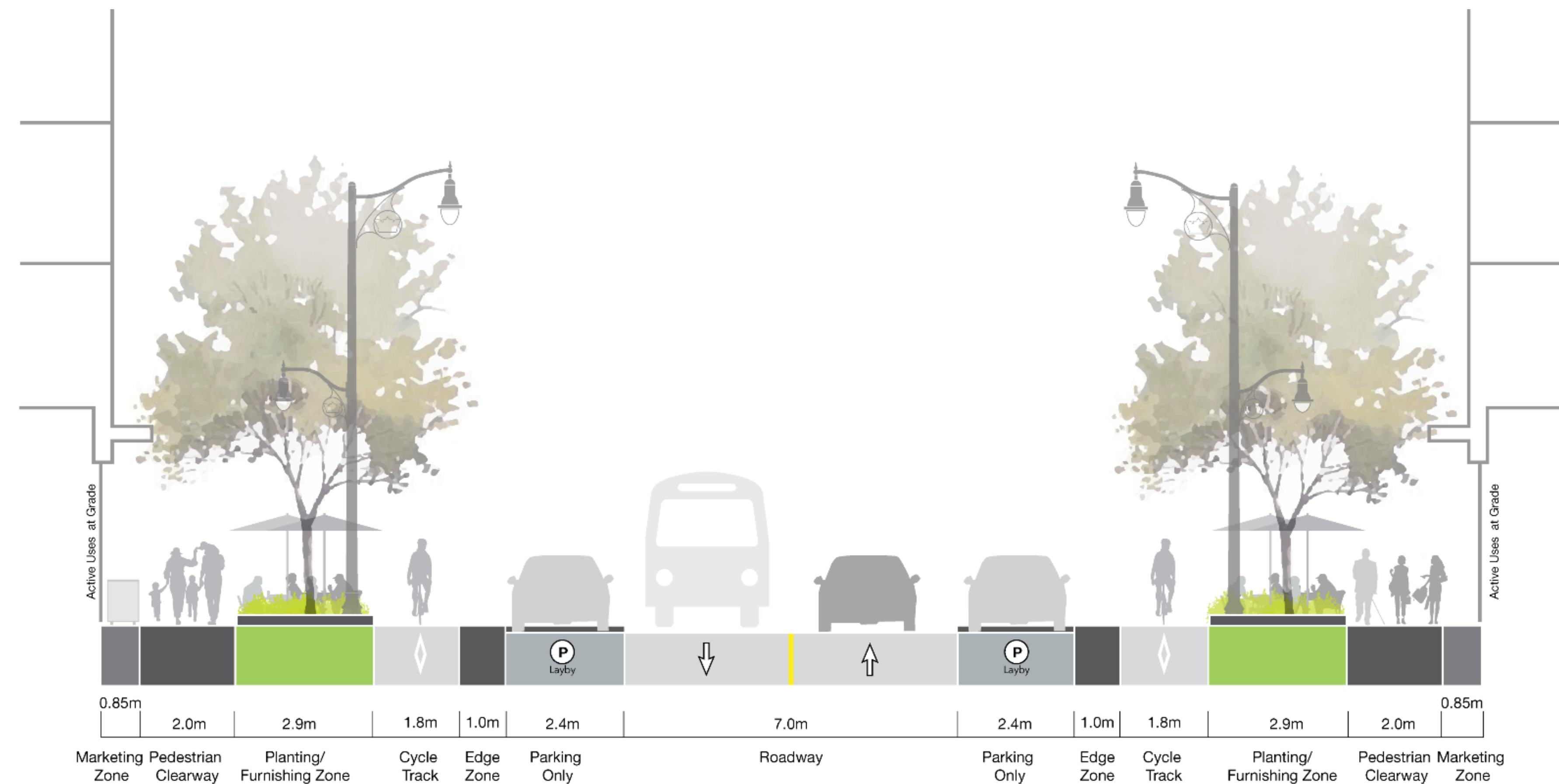


9b) Example of a buffered bike lane (Source: ZICLA)

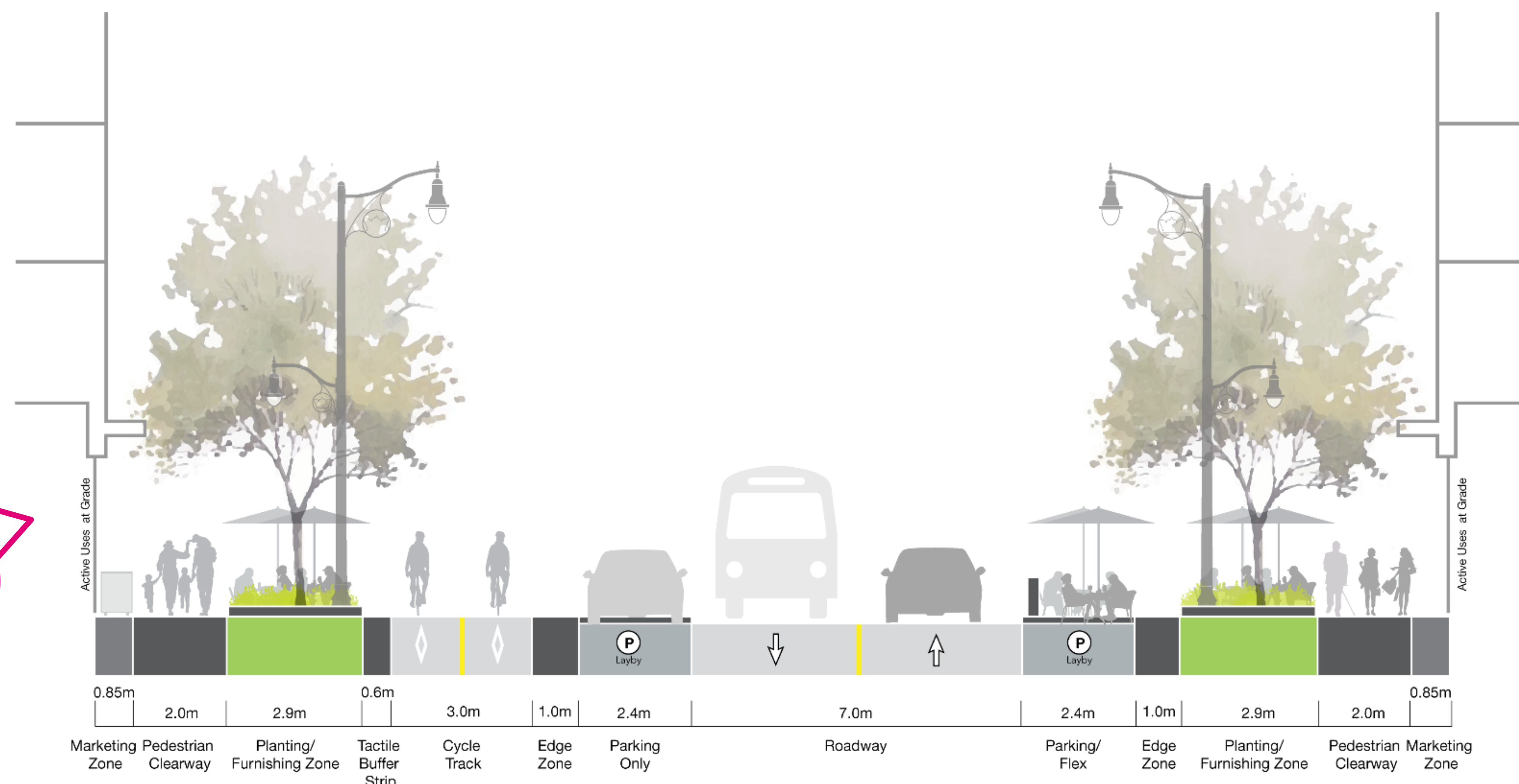
Option 2c: Two-lanes with Bike Path

- Two-lanes for vehicles.
- Cyclists have a separate lane with physical buffer providing separation from vehicles and parking areas (one or both sides of the road).
- Areas beside the bike path can be used for parking. Other purposes for the parking spaces can be accommodated only when the bike path is closed.

Think of how people and cars can safely move around cyclists. Do any of these options stand out for you?



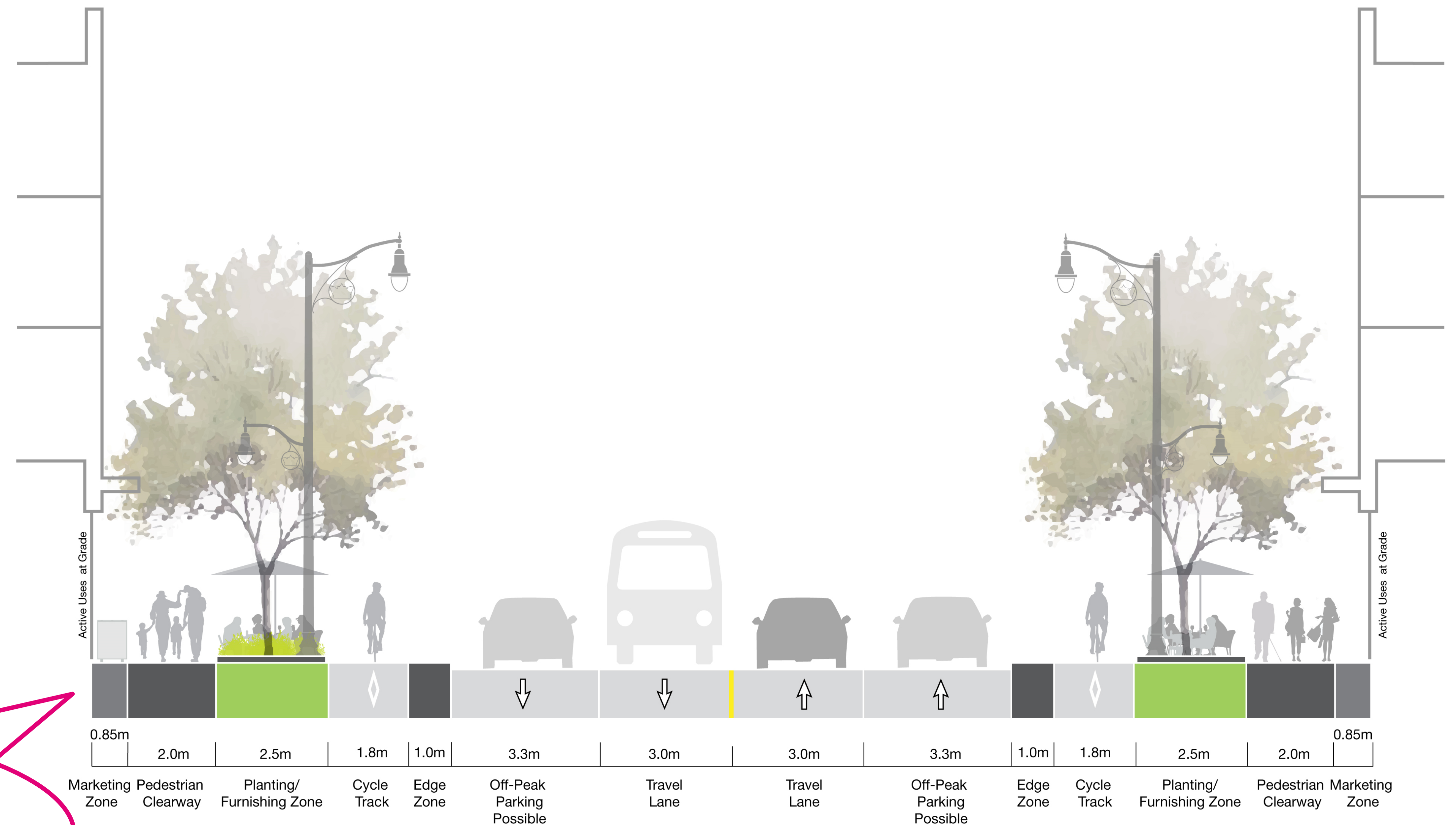
10a) Two-lane Configuration with one-way bike path



10b) Two-lane Configuration with two-way bike path

Option 3: Four-lanes with Bike Path

- Four-lanes for vehicles.
- Cyclists have a bike path with physical buffers providing separation from vehicles and parking areas (one or both sides of the road).
- Areas beside the bike path can be used for parking. Parking spaces can have other flexible uses only when the bike path is closed.



What does more
vehicular traffic do for
your downtown?

11a) Four-lane Configuration with one-way bike path

Option 4: Public Space (no vehicle lanes)

- Street is reserved for pedestrian-only use (no lanes for vehicles or cyclists).
- Large areas for flexible uses.

Have you ever been to a downtown event that closed off roads? What did you like or dislike about it?

What does more pedestrian traffic do for your downtown?



12a) Public Space Configuration

Wyndham / Quebec / Douglas / Intersection & St. George's Square Options

1. Do Nothing
2. Standard Intersection Improvements
3. Realigned Four-leg Intersection
4. Roundabout
5. Traffic Circle
6. Public space (no vehicle lanes)

How can we improve St. George's Square?

What would make downtown events be even better?

When moving around this intersection, what option feels safest or most comfortable to you?

These options are not finalized yet. Is there something in one option you'd like to see combined with something from another?

Option 1: Do Nothing

- Below-ground infrastructure replaced, and existing St. George's Square and intersection is re-instated.
- No improvements to the existing intersection.
- St. George's Square configuration remains as is.

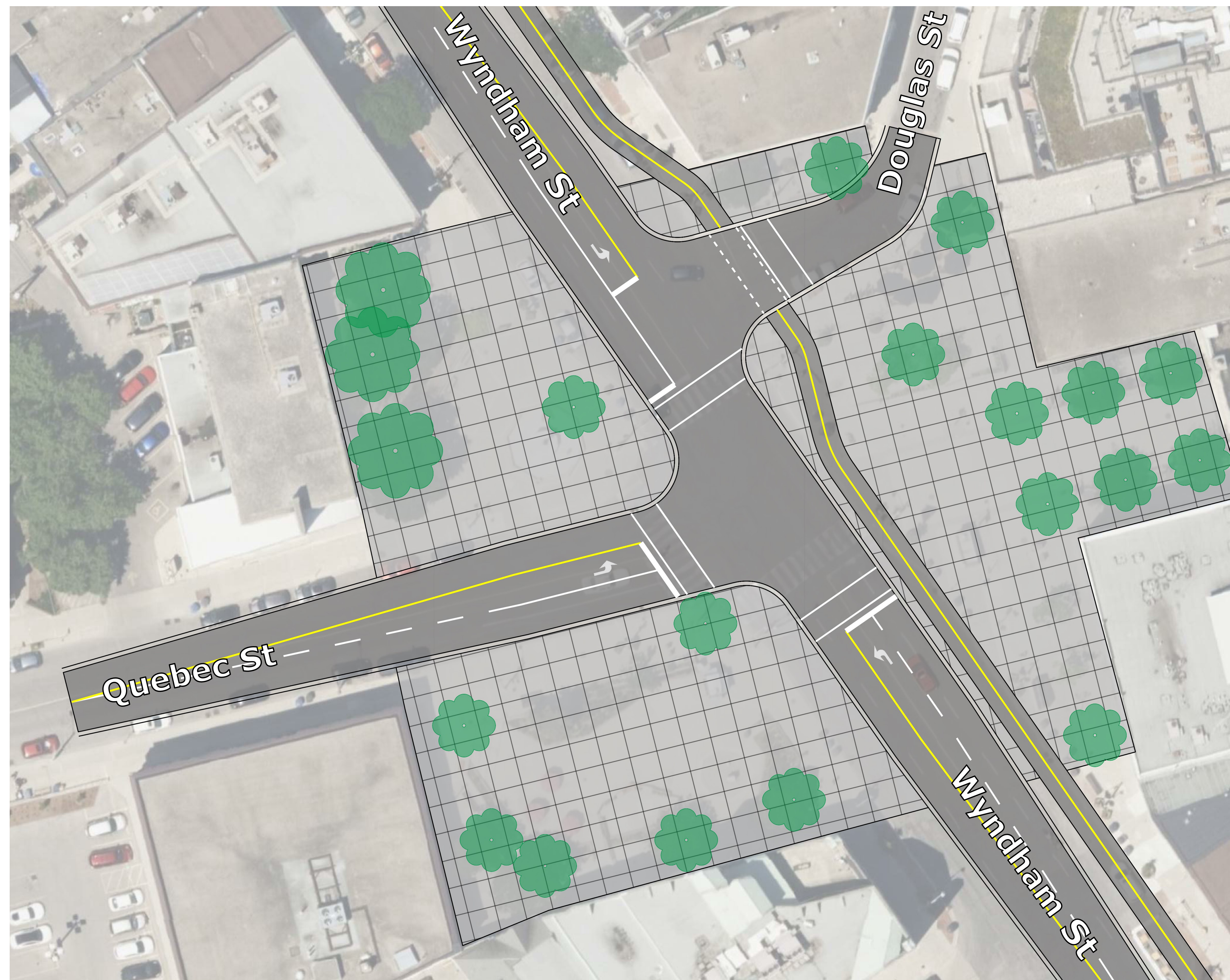
Note: This option is required to be considered under the Municipal Class EA planning process as a baseline for comparison.



14a) Existing Intersection Configuration

Option 2: Standard Intersection Improvements

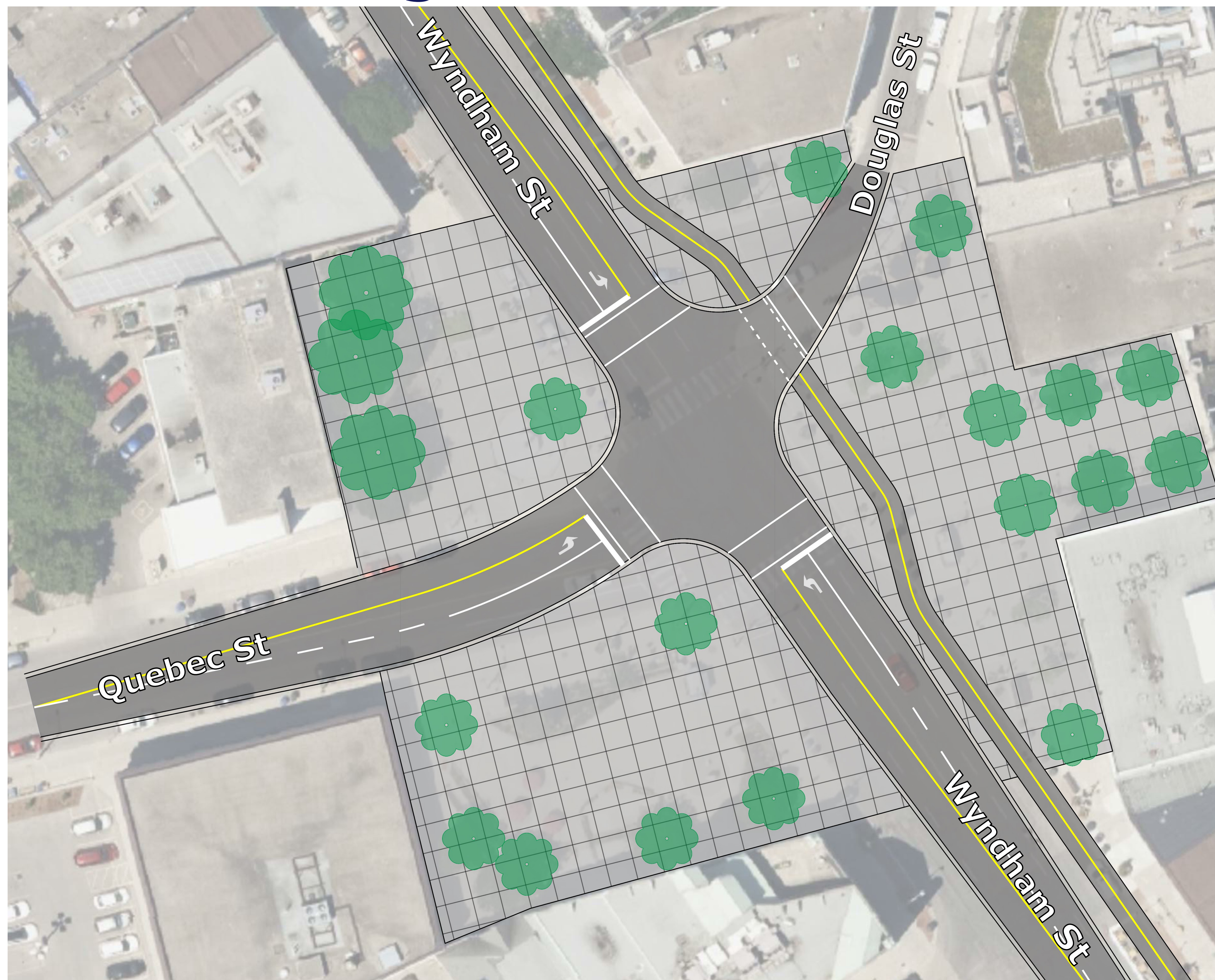
- Improvements that make it easier to drive, walk and cycle (i.e. left turn lanes, signage etc.).
- Reconstruction of St. George's Square in its current configuration.
- Provides flexibility for events but requires intersection closures.



15a) Standard Intersection Configuration. Note: Assumes two-lane cross-section with two-way bike path (actual number of travel lanes and accommodation for cyclists to be confirmed).

Option 3: Realigned Four-leg Intersection

- Realign Quebec Street and Douglas Street to tie into Wyndham Street at a standard four-leg intersection.
- Minor reconfiguration of St. George's Square to make room for new intersection alignment.
- Can provide flexibility for events when intersection is closed

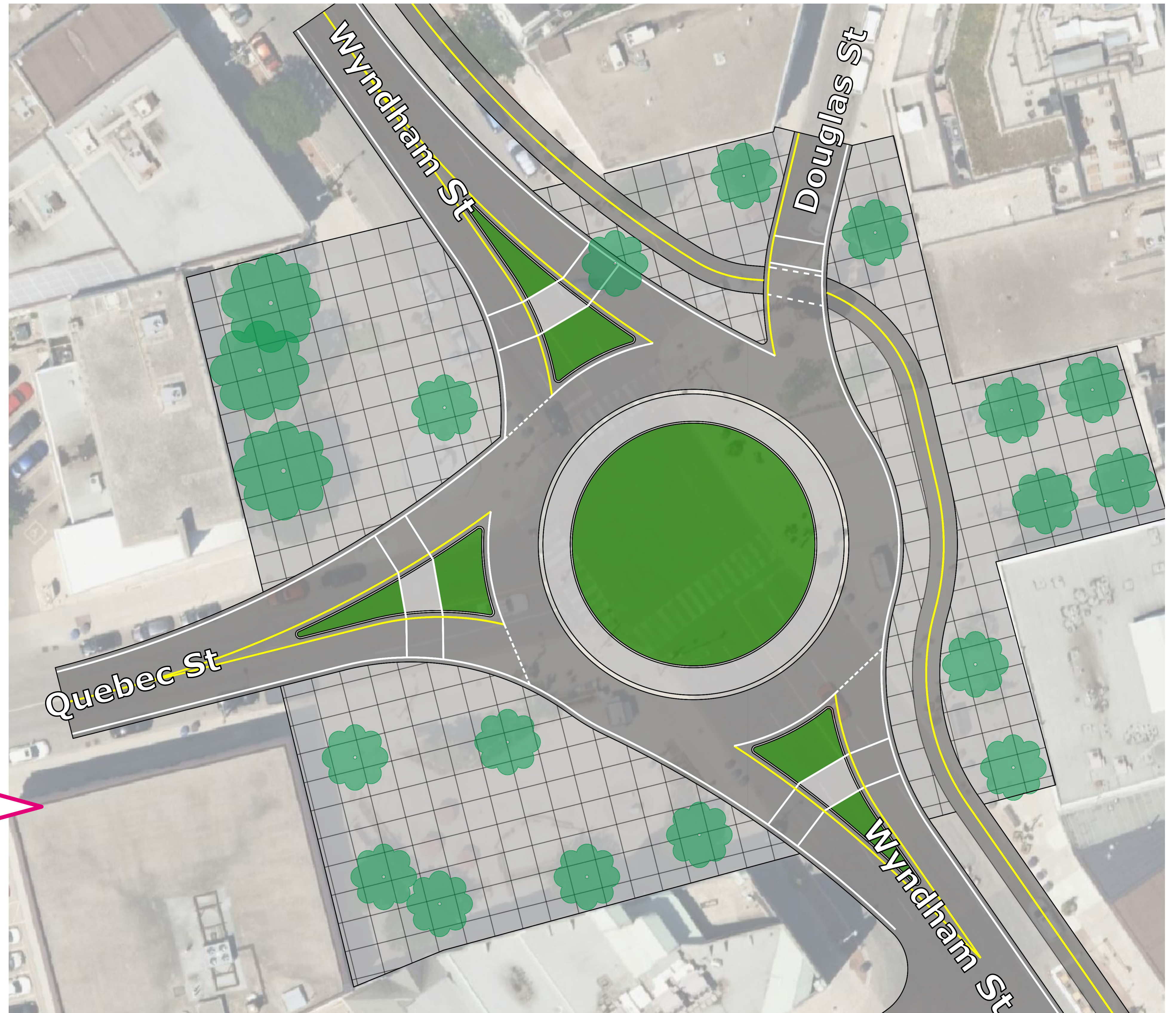


16a) Realigned four-leg Intersection Configuration. Note: Assumes two-lane cross-section with two-way bike path (actual number of travel lanes and accommodation for cyclists to be confirmed). 16

Option 4: Roundabout

- Realign Quebec Street and Douglas Street to tie into Wyndham Street as a standard roundabout.
- Some reconfiguration of St. George's Square to make room for new intersection layout.
- Can provide flexibility for events when intersection is closed

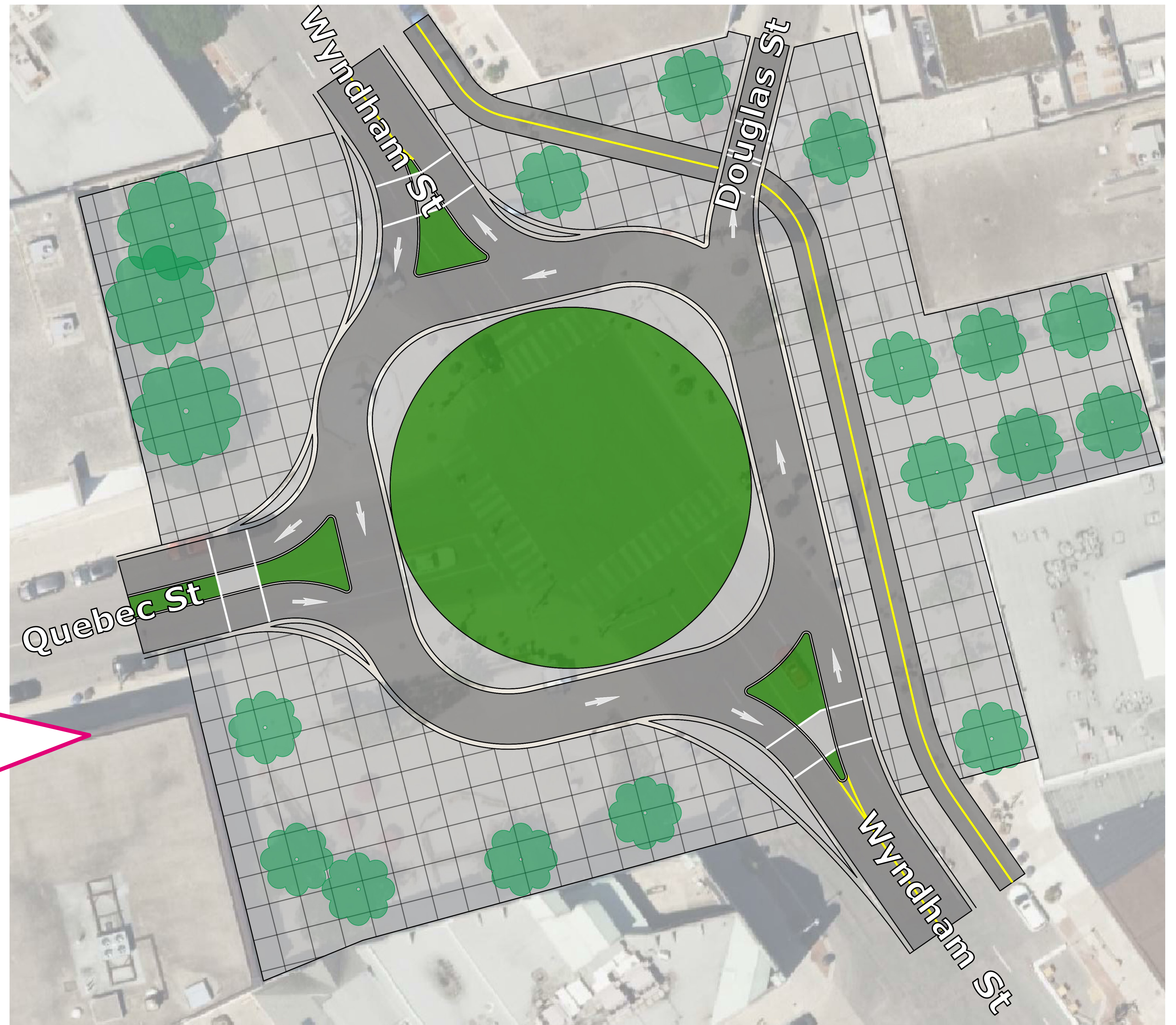
How does a change like this one work for people who drive, walk, cycle and take transit?



Option 5: Traffic Circle

- Traffic flows continuously in one-way around the circle.
- Public plaza in the centre.
- Allows for events in the centre without closing the intersection.

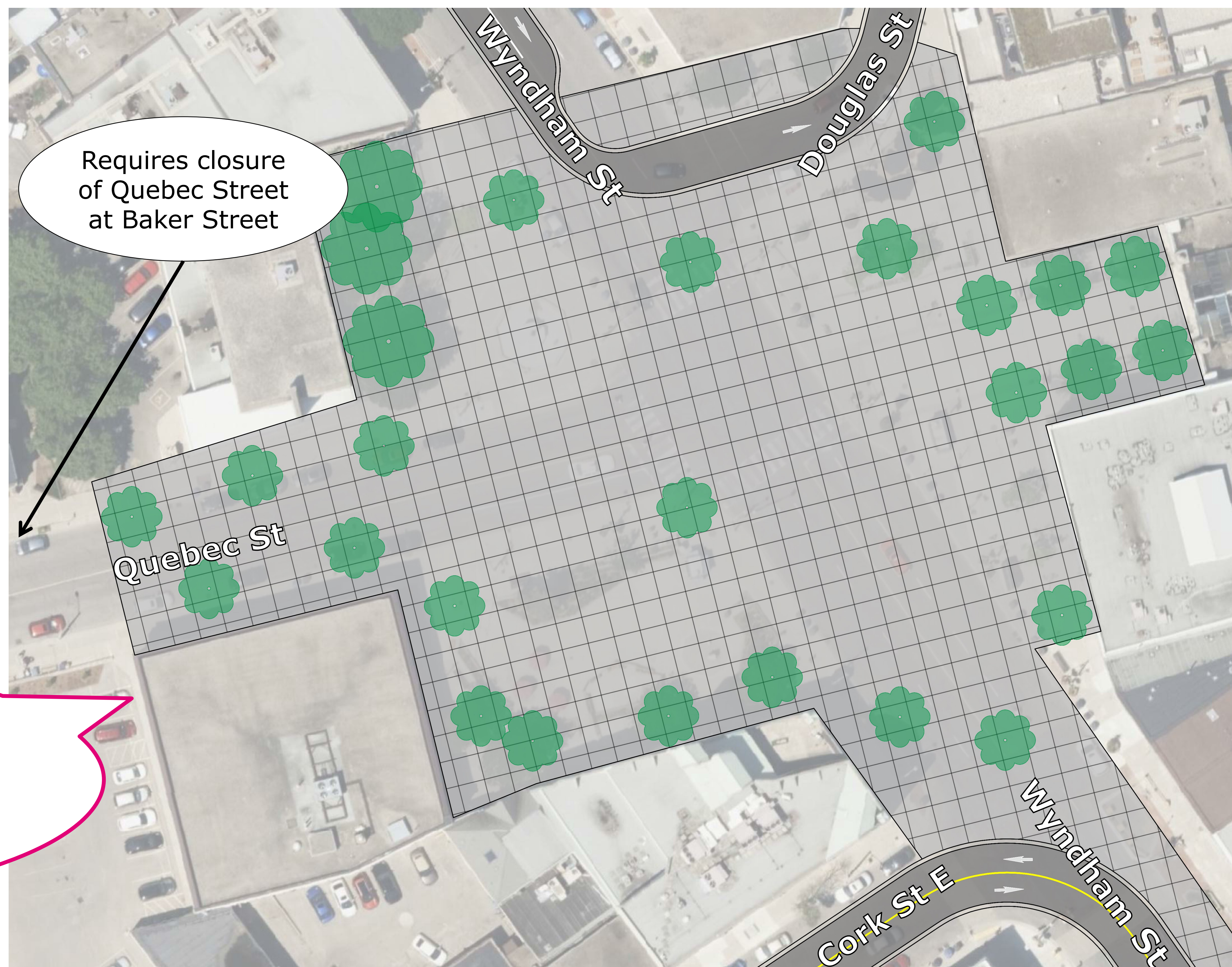
This option focuses more on public space, and the Roundabout focuses on vehicle movement.
How would you like to use this part of downtown?



18a) Traffic Circle Intersection Configuration. Note: Assumes two-lane cross-section with two-way bike path (actual number of travel lanes and accommodation for cyclists to be confirmed).

Option 6: Public Space (no vehicle lanes)

- Closure of Quebec Street, Douglas Street and Wyndham Street prior to intersection.
- Intersection is reserved for pedestrian-only use.
- No formal accommodation of cyclists.
- Large area for flexible uses.



Macdonell Bridge and Allan Structures EA – Study Area and Goals

This Environmental Assessment focuses on the Macdonell Street Bridge area as a whole. The goals are to:

- Address structural deficiencies identified in recent bridge inspections.
- Enhance road safety, operations, and connectivity for vehicles, pedestrians, cyclists and transit to support the community building goals of the City.
- Improve traffic operations and safety at the Wellington / Woolwich / Macdonell intersection.



20a) Macdonell and Allan Structures Class EA Study Area

Macdonell Bridge Options

- 1. Do Nothing:** No improvements.
- 2. Rehabilitate the Bridge:** Undertake repairs to the existing bridge.
- 3. Replace the Bridge**
 - a) Replacement of the entire bridge.
 - b) Replacement of the superstructure (deck, railing etc.), and rehabilitation of the substructure (piers, abutments etc.).
- 4. Keep Existing Bridge for Pedestrians and Cyclists Only:** Close bridge to vehicular traffic. Bridge becomes pedestrian and cyclist crossing only.
- 5. Remove Bridge:** Remove the bridge and redirect vehicular, pedestrian and cyclist traffic to other crossings.



21a) Macdonell Bridge Aerial View



21b) Macdonell Bridge

Allans Dam Bridge Options

1. **Do Nothing:** No improvements. Bridge continues to deteriorate.
2. **Rehabilitate Bridge for Pedestrians & Cyclists:** Fix the bridge where it is deteriorating and re-open for pedestrians and cyclists only.
3. **Remove Bridge:** Permanent closure and removal of the bridge.



22a) Allans Dam Bridge



22b) Allans Dam Bridge Closure

Allans Dam Sluiceway & Spillway Options

1. **Do Nothing:** No improvements. Sluiceway and spillway continue to deteriorate.
2. **Rehabilitate Sluiceway and Spillway:** Undertake necessary repairs to the existing sluiceway and spillway.
3. **Remove Sluiceway and Rehabilitate Spillway:** Speed River elevation continues to be controlled by existing spillway, with no sluiceway operations.
4. **Remove Sluiceway and Spillway:** Complete removal of the existing sluiceway and spillway. Speed River elevation is no longer controlled.
5. **Remove Sluiceway and Spillway and Build a New Dam:** Speed River elevation is controlled by a new dam.

Removing these structures would change the way the area looks and functions. Talk to the project team to learn more.



23a) The sluiceway is a concrete channel with a metal gate to carry excess water.





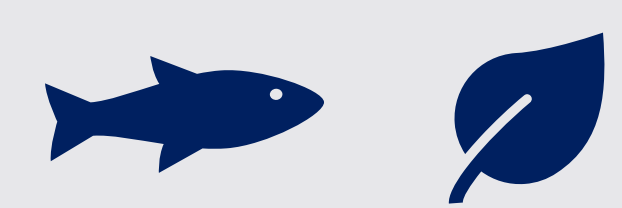

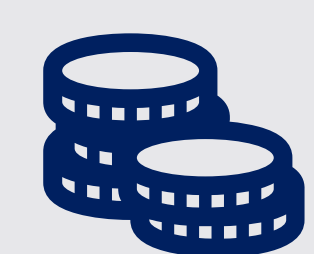
23b) The spillway forms a weir to control the Speed River elevation, previously used for the now-removed Allan's Mill.

Next Steps

- Review and address the comments submitted at and following the Open House.
- Consult with additional stakeholders and technical agencies, as required.
- Evaluate solutions and complete supporting studies – Fall 2022 / Winter 2023.
- Present preliminary recommendations at second Open House – Spring 2023 (date to be confirmed).

Next Steps – Evaluation of Options

The options will be comparatively evaluated based on criteria representing the broad definition of the environment, as described in the *EA Act*.

Criteria	Description
Technical 	How will the option serve the existing and future vehicular, pedestrian and cycling traffic needs? Does the alternative adequately address the structural requirements of the project?
Socioeconomic Environment 	What impacts will the option have on the local community (e.g., compatibility with area land use, impacts on local businesses, property requirements, access restrictions, etc.)? Will the option support the function of Downtown business?
Natural Environment 	How does the option affect existing vegetation, water quality, fisheries/wildlife and habitat? Does the option address climate change mitigation and adaptation considerations?
Cultural Heritage Resources 	Will the option affect archaeological, cultural heritage resources or Indigenous communities?
Financial 	What is the capital cost of the option? What is the cost for utility relocations and property acquisitions? What are the operation and maintenance costs?

Remember to provide your comments on the sheets available, or online, at haveyoursay.guelph.ca/downtown-renewal by November 16, 2022.



APPENDIX 12-4-2

Public Open House #2 Slides



Welcome

Macdonell and Allan's Structures Municipal Class Environmental Assessment Studies

Public Open House

December 9, 2024
6:00pm – 8:00pm

Please review the materials and provide your comments on the sheets available,
or online, by January 12, 2025.

Staff are available to answer your questions.



**Downtown
Renewal**

Creating a place for everyone.



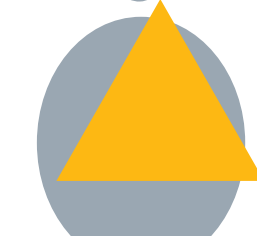
Project Approach and Timeline



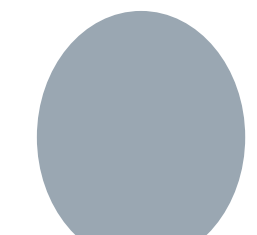
2021: Start project planning



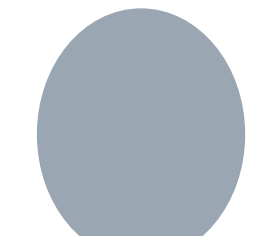
August 2021: Start community engagement



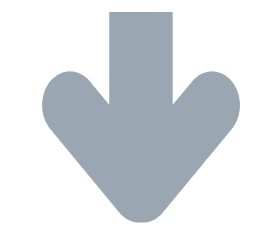
2021 to 2025: Planning Phase: Macdonell Bridge and Allan's Structures Environmental Assessment



2026: Detailed design



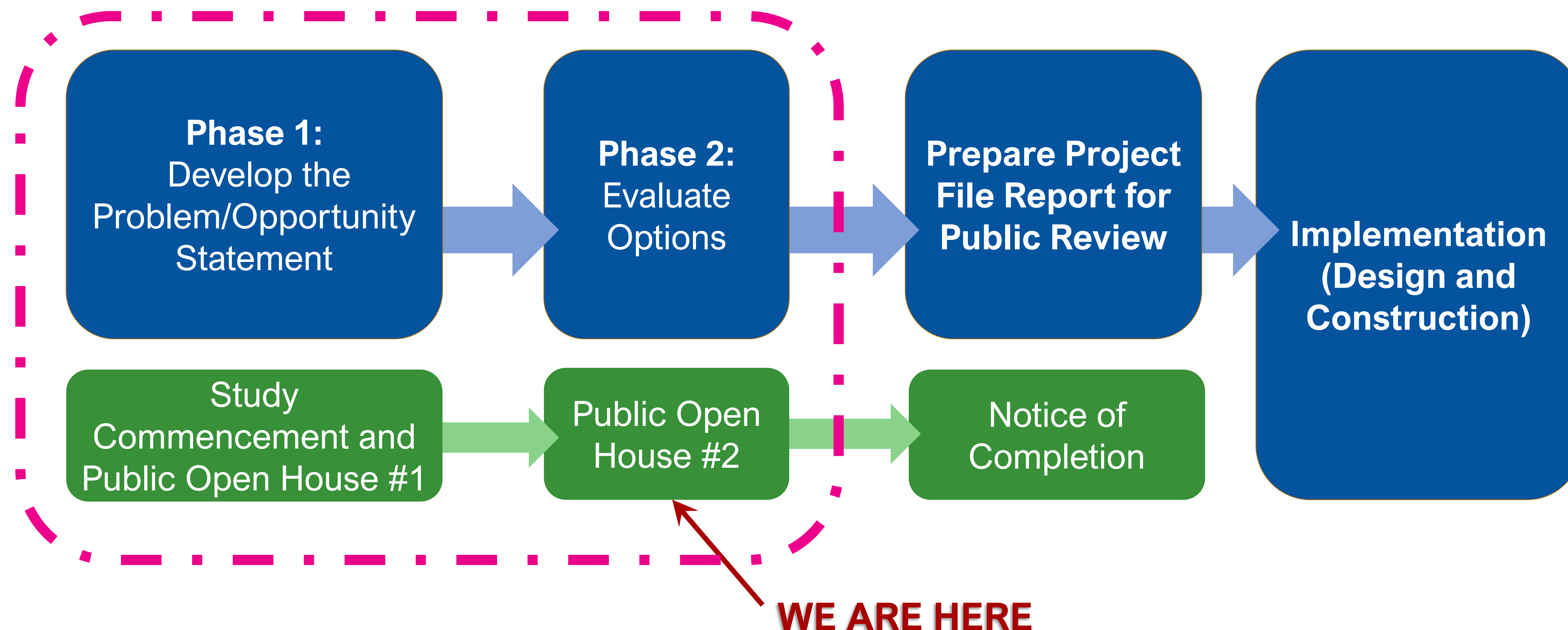
2028: Anticipated start of construction



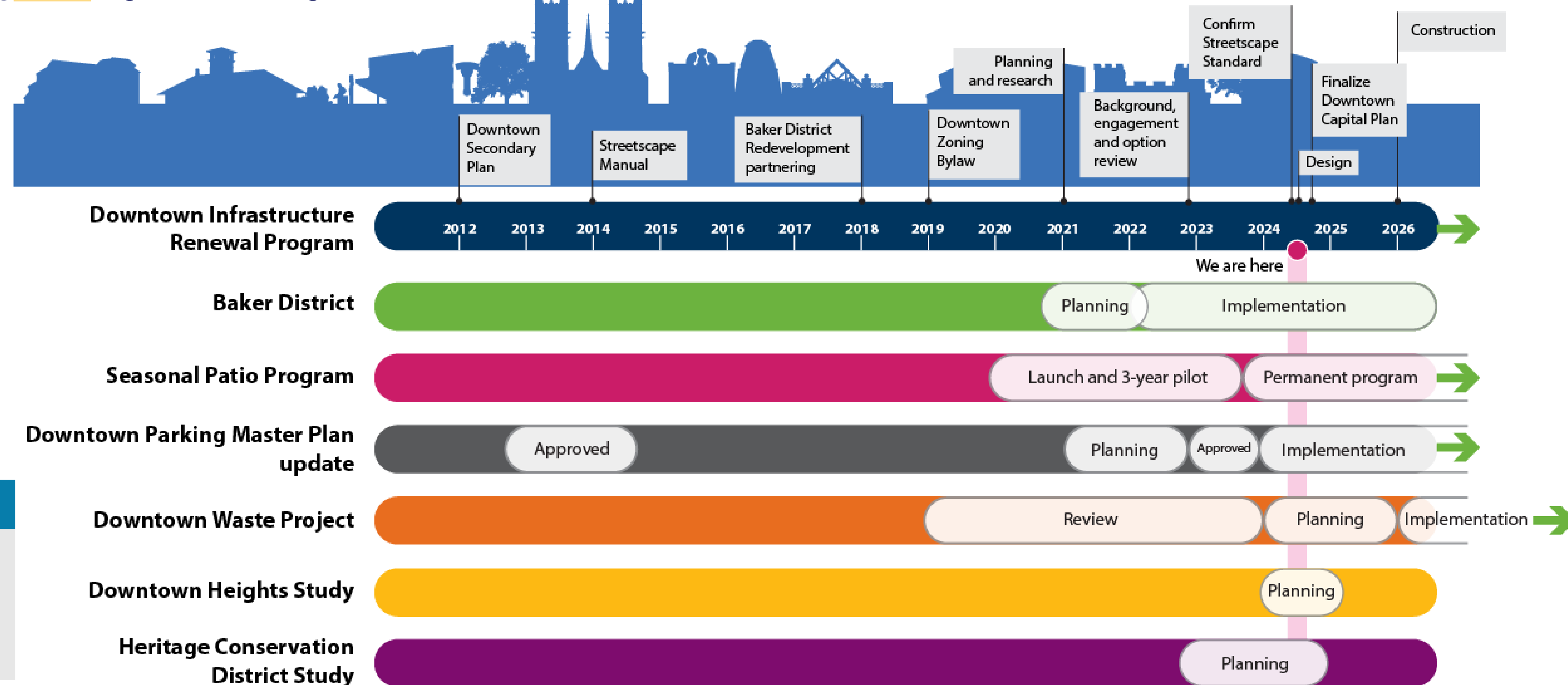
Involve
community
throughout
planning, design,
and construction

What is the Municipal Class Environmental Assessment Process?

- Ensures all reasonable options are considered.
- Reduces impact on the natural, cultural, social and economic environment.
- Input from the public, interested parties, Treaty partners, and technical agencies is essential.

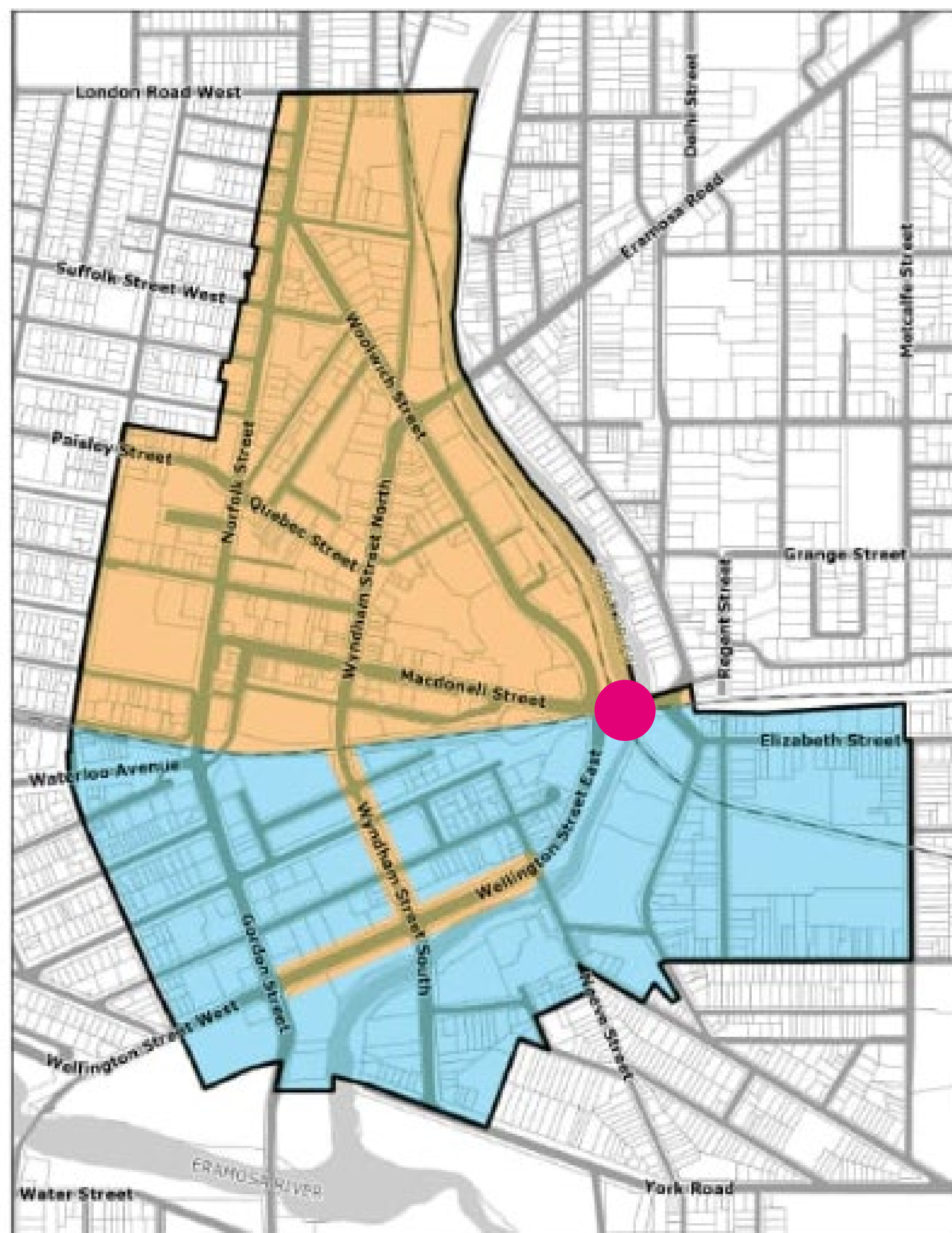


Our Future Downtown



The **Downtown Renewal program** is a massive undertaking by the City to transform how Downtown Guelph looks, feels, and functions, while also preserving its unique cultural heritage. The City is making significant investments in infrastructure, redevelopment, and transportation to make Downtown Guelph an even stronger cultural and civic hub for the City and the wider area. Our renewal efforts will create the foundation for future prosperity, sustainability, and well-being in the Downtown.

Downtown Infrastructure Renewal Program (DTIRP)



Scope: Reconstruction of municipal infrastructure within the Downtown Secondary Plan area north of the Metrolinx corridor, plus Wyndham Street South and Wellington – area in orange.

Drivers: Enable growth and maintain state of good repair of municipal infrastructure within the Downtown Secondary Plan Area.

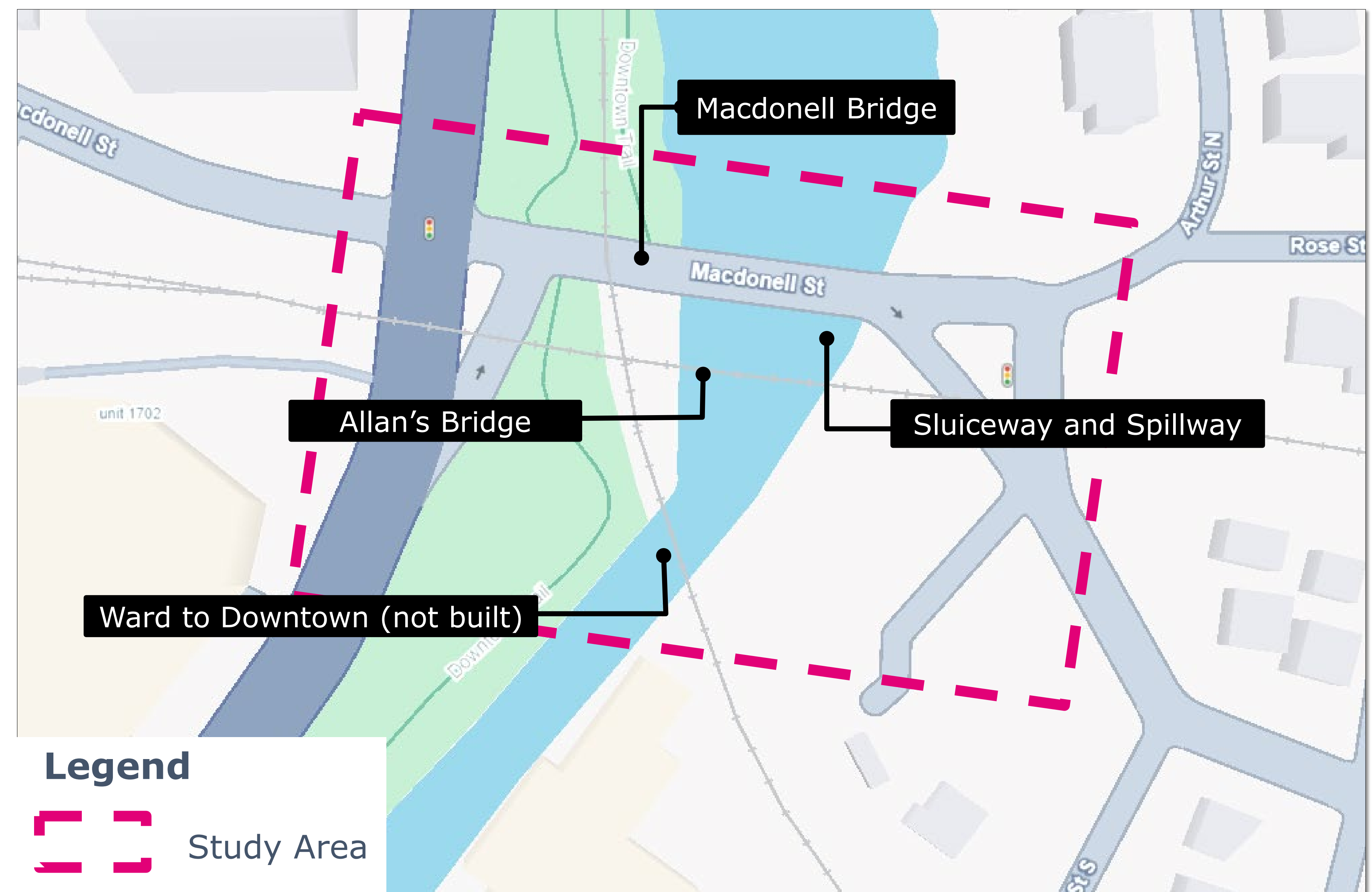
Projects:

- Wyndham St. N. EA (completed)
- **Macdonell and Allan's Structures EA**
- Capital Implementation Plan
- Wyndham/Wellington Water and Wastewater Capacity Improvements – DTIRP Phase 0
- Wyndham St N. – DTIRP Phase 1

Legend
 Area subject to current DTIRP
 future consideration

Macdonell Bridge and Allan Structures EA – Study Area and Goals

- Part of the City's Downtown Infrastructure Renewal Program (DTIRP).
- Address structural deficiencies identified in recent bridge inspections:
 - Macdonell Bridge,
 - Allan's Bridge, and
 - Sluiceway and Spillway.
- Review options for the Ward to Downtown pedestrian/cyclist crossing.
- Enhance road safety, operations, and connectivity for vehicles, pedestrians, cyclists and transit to support the community building goals of the City.



Macdonell and Allan's Structures Class EA Study Area

Ward to Downtown Pedestrian Bridge

- Original study initiated in 2017 in response to the predicted future growth and to provide connectivity across the Speed River between St. Patrick's Ward and Downtown.
- Recommended a crossing approximately 40m south of Macdonell Street, immediately south of the GJR Rail Bridge.
- Ultimately cancelled due to impacts on heritage properties due to excavation, property impacts, and cost.
- The need and justification, and alternatives review added to the scope of the Macdonell and Allan's Structures Class EA.



Recently cancelled 2023 tendered Ward to Downtown Pedestrian Bridge

Macdonell Bridge Alternatives

1. **Do Nothing:** No improvements. Bridge continues to deteriorate.
2. **Rehabilitate Bridge:** Undertake repairs to the existing bridge.
3. **Rehabilitate and Widen the Bridge to Accommodate AT on Both Sides:** Undertake repairs to the existing bridge and widen up to 4.3m to accommodate active transportation (AT) facilities on both sides of the bridge.
4. **Replace Bridge for Vehicular Traffic Only:** Full bridge replacement with no opportunity for active transportation facilities improvements.
5. **Replace and Widen Bridge to Accommodate AT on North Side:** Full bridge replacement and widen up to 0.2m to accommodate a multi-use path on the north side and sidewalk on the south side.

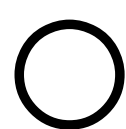
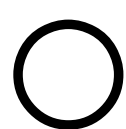
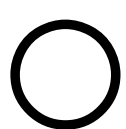
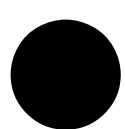



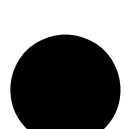







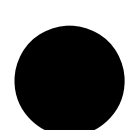
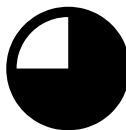



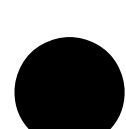
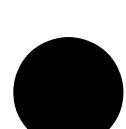



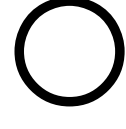






Macdonell Bridge Aerial View



Macdonell Bridge

Evaluation of Macdonell Bridge Alternatives

EVALUATION CRITERIA	1. Do Nothing		2. Rehabilitate Bridge		3. Rehabilitate + Widen Bridge to Accommodate AT on Both Sides		4. Replace Bridge for Vehicular Traffic Only		5. Replace + Widen Bridge to Accommodate AT on North Side	
STRUCTURAL / TECHNICAL		Structural issues not addressed. Bridge will require replacement in 10 years.		Additional maintenance may still be required. Bridge will still require replacement in 10 years.		Potential durability issues as additional maintenance may still be required. Rehabilitated portion of bridge will still require replacement in 10 years.		Structural issues completely addressed. Provides opportunity to mitigate bridge runoff draining onto GJR property.		Structural issues completely addressed. Provides opportunity to mitigate bridge runoff draining onto GJR property.
TRAFFIC OPERATIONS & SAFETY		No improvements to active transportation.		No improvements to active transportation.		Accommodates active transportation facilities. Improves connectivity to Downtown and nearby trails. Improves safety.		Minimal improvements to active transportation facilities. Improves safety.		Accommodates active transportation facilities, improves connectivity to Downtown and nearby trails, and improves safety.
SOCIAL ENVIRONMENT		No construction staging or traffic impacts. No improvements to AT connectivity to Downtown and trails.		Complex construction staging and traffic impacts. Long construction duration.		Requires up to 2.4m encroachment into property at NE quadrant. Long construction duration. Improves AT connectivity to Downtown and trails.		Complex staging and traffic impacts during construction. Temporary encroachment into private property at NE quadrant.		Potential for minor property encroachment and reconfiguration of driveway at NE quadrant depending on future intersection configuration. Improves AT connectivity to Downtown and trails.
NATURAL ENVIRONMENT & CLIMATE CHANGE		No anticipated impacts on the terrestrial or aquatic environment.		Some minor and/or temporary anticipated impacts on the aquatic habitat and terrestrial environment.		Some minor and/or temporary anticipated impacts on the aquatic habitat and terrestrial environment.		Some minor and/or temporary anticipated impacts on the aquatic habitat and terrestrial environment.		Some minor and/or temporary anticipated impacts on the aquatic habitat and terrestrial environment.
HERITAGE / ARCHAEOLOGICAL / CULTURAL IMPACTS		No impacts to archaeological or cultural heritage resources.		No impacts to archaeological or cultural heritage resources.		May impact areas with potential for archaeological resources in northwest corner. Potential for minor impacts to adjacent cultural heritage resources.		No archaeological impacts anticipated, but potential for minor impacts to adjacent cultural heritage resources.		May impact areas with potential for archaeological resources in northwest corner. Potential for minor impacts to adjacent cultural heritage resources.
COST		Increased maintenance costs.		\$4M Capital costs; \$12.6M Lifecycle costs.		\$6.4M Capital costs; \$13.1M Lifecycle costs.		\$8.3M Capital costs; \$9.5M Lifecycle costs.		\$8.8M Capital costs; \$10.3M Lifecycle costs.
OVERALL SCORE	12.0		12.0		13.0		12.0		15.0	
EVALUATION SUMMARY	Not Recommended		Not Recommended		Not Recommended		Not Recommended		Recommended to be Carried Forward	

The recommended solution for Macdonell Bridge is **Alternative 5 – Replace and Widen Bridge to Accommodate AT on North Side.**

Allan's Bridge Alternatives

1. **Do Nothing:** No improvements. Bridge continues to deteriorate.
2. **Minor Rehabilitation for Heritage Purposes Only:** Bridge is rehabilitated for cultural heritage purposes only with no active transportation accommodation.
3. **Rehabilitate Bridge for Pedestrians and Cyclists:** Superstructure is replaced to accommodate active transportation facilities.
4. **Remove Bridge:** Bridge is removed entirely.

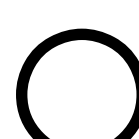

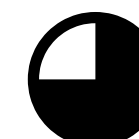





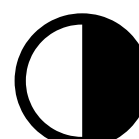
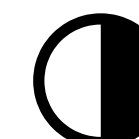


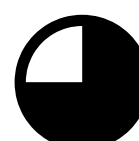
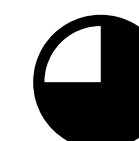


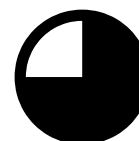
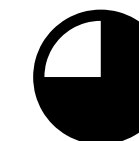


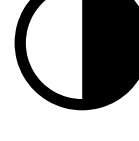


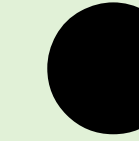


Allan's Bridge



Allan's Bridge Closure

Evaluation of Allan's Bridge Alternatives

EVALUATION CRITERIA	1. Do Nothing		2. Minor Rehabilitation of Bridge for Heritage Purposes Only		3. Rehabilitate Bridge for Pedestrians & Cyclists		4. Remove Bridge	
STRUCTURAL / TECHNICAL		Structural issues not addressed. Bridge would continue to deteriorate.		Some structural issues addressed. Bridge may require additional maintenance in the future.		Most structural issues addressed. Reduced concerns for durability and maintenance in the future.		No structural, durability or safety issues with the removal of the bridge.
TRAFFIC OPERATIONS & SAFETY		No improvements.		Relocation of active transportation users to alternative routes (Macdonell St. & Ward to Downtown pedestrian bridge).		Improves active transportation facilities and connectivity to Downtown. Sightline issues with oncoming trains for pedestrians and cyclists.		Relocation of active transportation users to alternative routes (Macdonell St. & Ward to Downtown pedestrian bridge).
SOCIAL ENVIRONMENT		No changes to existing aesthetics of the bridge. Does not improve recreational value as active transportation is not accommodated.		No changes to existing aesthetics of the bridge. Does not improve recreational value as active transportation is not accommodated.		3rd active transportation crossing within study limits not required.		Removal of bridge will improve views of Speed River from Macdonell Bridge.
NATURAL ENVIRONMENT & CLIMATE CHANGE		No anticipated impacts on the natural environment. No changes.		No anticipated impacts on the natural environment. No changes.		No anticipated impacts on the natural environment.		Some minor anticipated impacts from bridge removal. Reduces overall footprint within Speed River.
HERITAGE / ARCHAEOLOGICAL / CULTURAL IMPACTS		Continued deterioration of cultural heritage resource. Confirm archaeological.		Minor impact to a Known Built Heritage Resource. Confirm archaeological.		Significant impact to a Known Built Heritage Resource. Confirm archaeological.		Removal of a Known Built Heritage Resource can be mitigated through commemorative strategy.
COST		\$2.9M Lifecycle costs.		\$800K Capital costs; \$1.6M Lifecycle costs.		\$1.9M Capital costs; \$2.4M Lifecycle costs.		\$740K Capital costs.
OVERALL SCORE	12.0		12.0		9.0		13.0	
EVALUATION SUMMARY	Not Recommended		Not Recommended		Not Recommended		Recommended to be Carried Forward	

Commemorating Allan's Bridge

A *Heritage Committee* could be formed to develop a commemorative strategy for Allan's Bridge, which may include:

- A commemorative plaque celebrating Allan's Bridge
- A lookout point where the Bridge was located
- Keeping key components of the existing bridge
- Replicate the Allan's Bridge aesthetics on the new Ward to Downtown Multi-Use Trail Crossing



Burgoyne Bridge Plaque



Burgoyne Bridge, St. Catharines

Allan's Dam Sluiceway and Spillway Alternatives

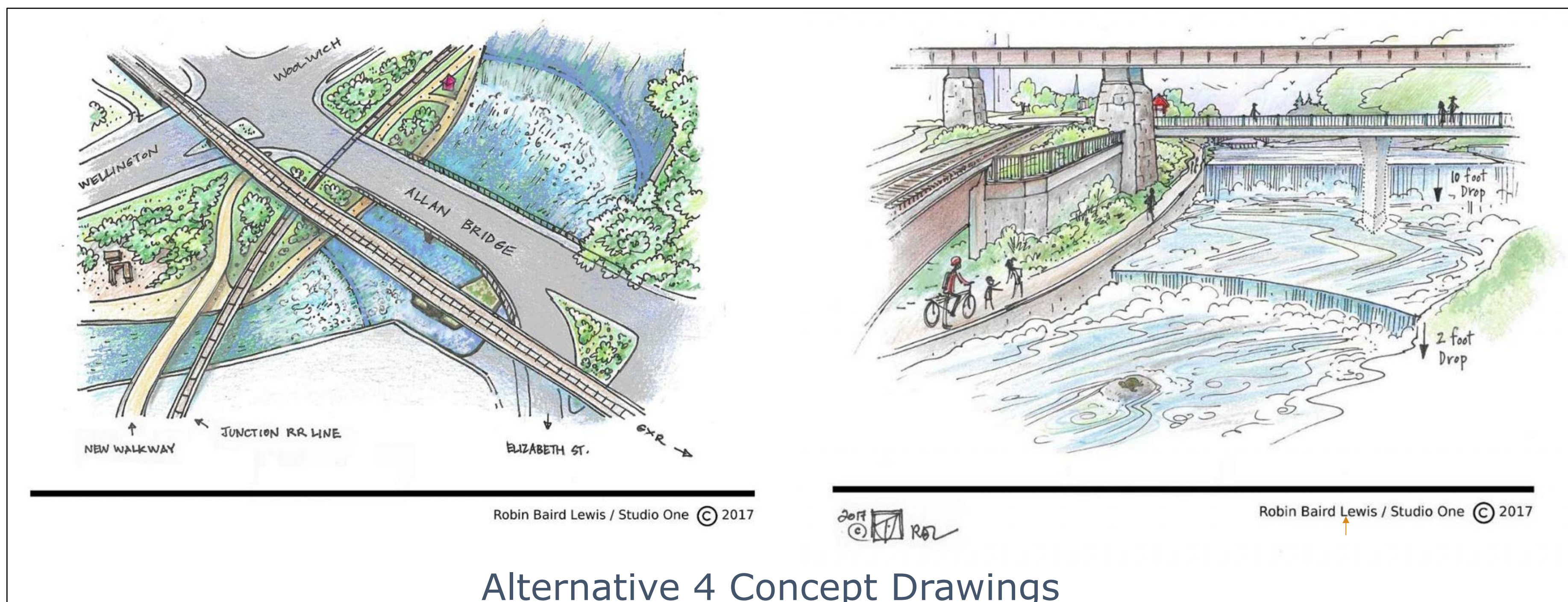
1. **Do Nothing:** No improvements. Sluiceway and spillway continue to deteriorate.
2. **Rehabilitate Sluiceway and Spillway:** Undertake necessary repairs to the existing sluiceway and spillway.
3. **Remove Sluiceway and Spillway:** Complete removal of the existing sluiceway and spillway. Speed River elevation is no longer controlled.
4. **Remove Sluiceway and Spillway and Build a New Dam Upstream with an Active Transportation Underpass:** Speed River elevation is controlled by a new dam further upstream, and an active transportation underpass is implemented under Macdonell Bridge.



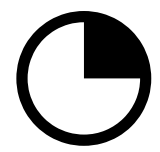

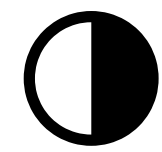
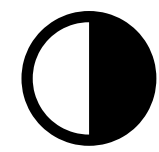
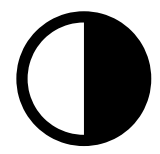

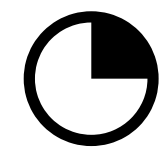
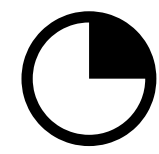
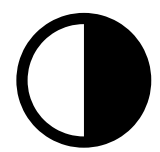

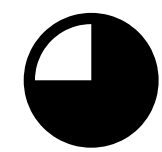
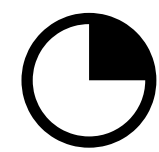
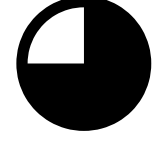

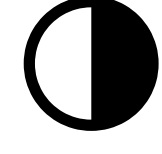
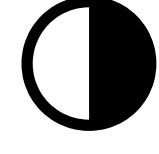
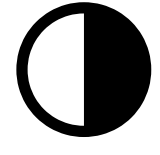
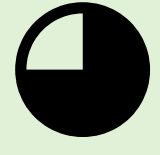
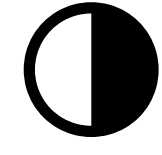
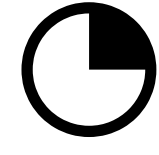
The sluiceway is a concrete channel with a metal gate to carry excess water.



The spillway forms a weir to control the Speed River elevation, previously used for the now-removed Allan's Mill.



Evaluation of Allan's Dam Sluiceway and Spillway Alternatives

EVALUATION CRITERIA	1. Do Nothing		2.Rehabilitate Sluiceway and Spillway		3. Remove Sluiceway and Spillway		4. Remove Sluiceway and Spillway and Build a New Dam Upstream with an Active Transportation Underpass	
STRUCTURAL / TECHNICAL		Continued degradation over time may lead to failure, impacting river levels.		Addresses structural issues and maintains hydraulic function of the river.		Significant impact on hydraulic function of the river.		Significant impact on hydraulic function of the river. Requires additional studies to quantify full impacts. External agency permitting is unlikely for a new dam.
SOCIAL ENVIRONMENT		Continued degradation over time may lead to failure, impacting river levels and enjoyment of property.		Water levels maintained as is. No significant impact to properties abutting Speed River or public recreation.		Potential for impacts on property values and enjoyment of property by altering water levels. Major impacts to public recreation uses of river. Could allow for creation of a cycling underpass.		Improved aesthetics but potential for significant impacts on property values and enjoyment of property by lowering water levels. Impacts public recreation uses of the river. Could allow for creation of a cycling underpass.
NATURAL ENVIRONMENT & CLIMATE CHANGE		No anticipated impacts on the natural environment. Fails to mitigate future impacts associated with climate change.		Some minor anticipated impacts on the aquatic environment from rehabilitation of the sluiceway and spillway.		Removal will impact the aquatic and terrestrial environments. Provides opportunity for restoring NHS and significant valleylands.		Impacts to the aquatic environment anticipated from removal of the sluiceway and spillway. Additional impacts when the new dam is built, provides no environmental benefit.
HERITAGE / ARCHAEOLOGICAL / CULTURAL IMPACTS		Continued deterioration of cultural heritage resource.		Minor impact to two Known Built Heritage Resource. Positive impacts are also identified through retention and rehabilitation of the structures.		Removal of two Known Built Heritage Resources. Confirm archaeological.		Removal of two Known Built Heritage Resources. Confirm archaeological.
COST		Increased maintenance and future reconstruction costs.		~\$415K Capital Cost.		~\$2M Capital Cost.		~ \$15M Capital Cost (based on similar examples).
OVERALL SCORE	9.0		13.0		8.0		5.0	
EVALUATION SUMMARY	Not Recommended		Recommended to be Carried Forward		Not Recommended		Not Recommended	

Ward to Downtown Multi-Use Trail Bridge Alternatives

1. **Do Nothing:** The Ward to Downtown Multi-Use Trail Bridge Crossing is not constructed and pedestrians and cyclists would be redirected to other bridges/crossings.
2. **Construct 2023 Tendered Bridge:** Crossing is constructed as per the City's recently cancelled 2023 Tender design.
3. **Construct a Modified Structure on South Side of GJR Rail Bridge:** Crossing is constructed in the previously identified location with a modified design using micro piles to reduce excavation requirements.
4. **Construct a Modified Structure on North Side of GJR Rail Bridge:** Crossing is constructed along the north side of the existing GJR Bridge.



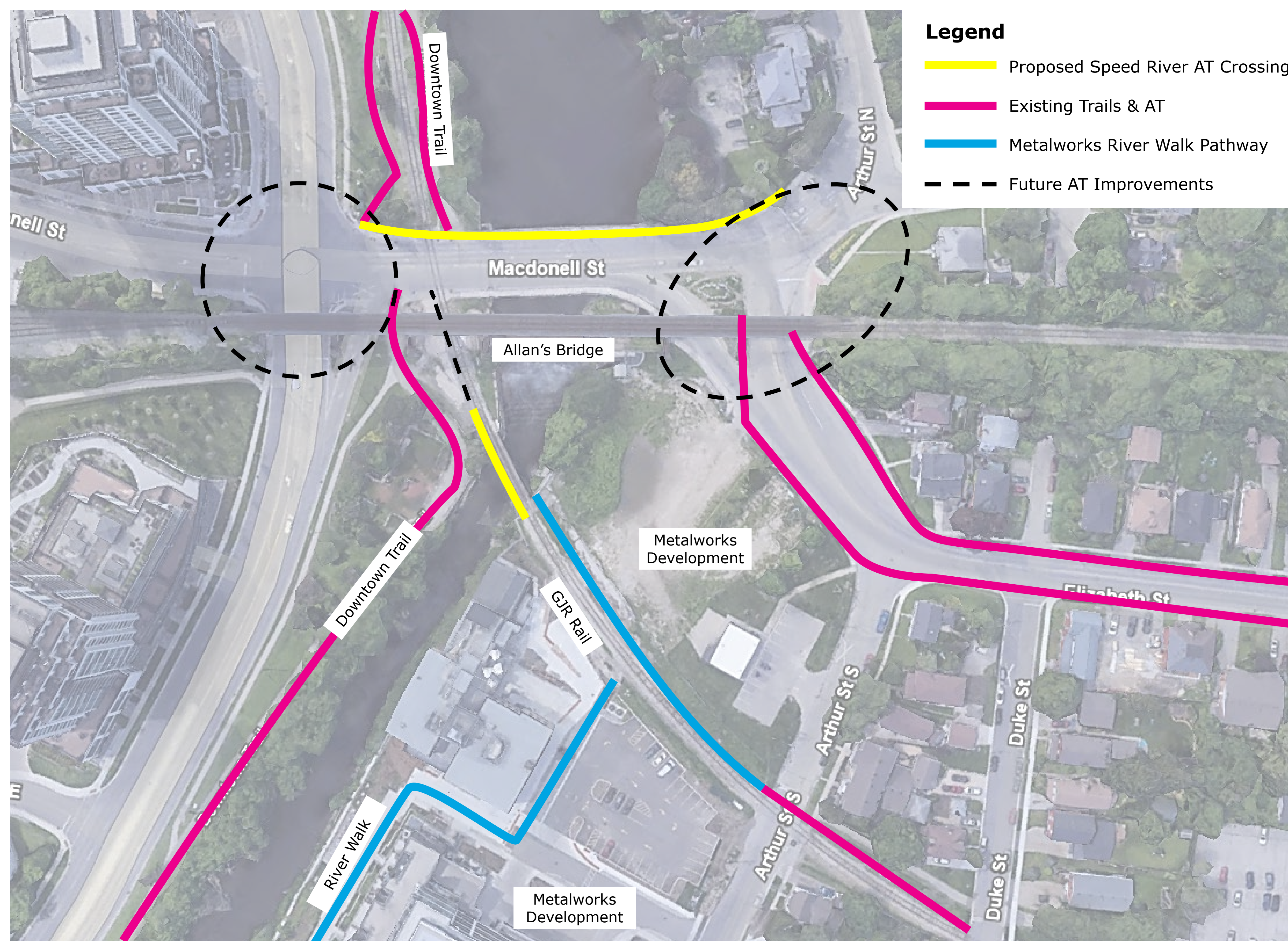
Proposed location of Ward to Downtown Multi-Use Trail Bridge Crossing

Evaluation of Ward to Downtown Pedestrian Bridge Alternatives

EVALUATION CRITERIA	1. Do Nothing		2. Construct 2023 Tendered Bridge		3. Construct a Modified Structure on South Side of GJR Rail Bridge		4. Construct a Modified Structure on on North Side of GJR Rail Bridge	
STRUCTURAL / TECHNICAL	●	No constructability or hydraulic concerns.	◐	Excessive impacts at east abutment due to excavation requirements and PTE requirements.	●	Excavation issues and PTE requirements mitigated through modified design and use of Micro (Helical) Piles.	◐	Construction complexities due to lack of space along west side of river, north of rail.
TRAFFIC OPERATIONS & SAFETY	◐	Safety concerns not addressed as pedestrians may continue using GJR railway bridge to cross the Speed River as a shortcut instead of using Macdonell or Allan's bridges.	●	Improves safety as pedestrians and cyclists will use the bridge to cross Speed River and avoid using the GJR railway bridge.	●	Improves safety as pedestrians and cyclists will use the bridge to cross Speed River and avoid using the GJR railway bridge.	◐	Improves safety but introduces accessibility issues due to elevation/stairs required at west end.
SOCIAL ENVIRONMENT	◐	Does not improve connectivity to Downtown Guelph and trails.	◐	Provides recreational benefit by improving connectivity to Downtown Guelph and trails. Impacts private / heritage property at east side.	●	Provides recreational benefit by improving connectivity to Downtown Guelph and trails, without impacting property.	●	Provides recreational benefit by improving connectivity to Downtown Guelph and trails, without impacting property.
NATURAL ENVIRONMENT & CLIMATE CHANGE	◐	No anticipated impacts on the natural environment. No changes.	◐	No anticipated impacts on the natural environment.	◐	No anticipated impacts on the natural environment.	◐	No anticipated impacts on the natural environment.
HERITAGE / ARCHAEOLOGICAL / CULTURAL IMPACTS	●	No impacts to archaeological or cultural heritage resources.	◐	Obstructs views to the Wellington Street Rail Bridge from the south. Construction activities have significant potential to impact Known Built Heritage Resource (Spring Mill Distillery).	◐	Obstructs views to the Wellington Street Rail Bridge from the south. Significantly reduces potential to impact Known Built Heritage Resource (Spring Mill Distillery).	●	No obstruction of views to Rail Bridge. Significantly reduces potential to impact Known Built Heritage Resources due to location.
COST	●	No costs.	◐	Approx. \$5M Capital Cost.	◐	Approx. \$3.3M Capital Cost (Lower construction costs due to simplified structure).	◐	Approx. \$4.5M Capital Cost (Higher construction costs, mainly due to elevation requirements).
OVERALL SCORE	14.0		10.0		17.0		15.0	
EVALUATION SUMMARY	Not Recommended		Not Recommended		Recommended to be Carried Forward		Not Recommended	

The recommended solution for the Ward to Downtown Pedestrian Bridge is **Alternative 3 – Construct a Modified Structure on South Side of GJR Rail Bridge.**

Overall Recommended Solution



Pros:

- ✓ Active transportation on Macdonell Bridge improves connectivity to Downtown and within the Cycling Network.
- ✓ Addresses trespassing/safety issues on GJR Rail Bridge.
- ✓ Supports *City Policy (OPA 43)* to increase the number of crossings over the Speed River into the downtown core.
- ✓ Provides direct link to proposed trail through St. Patrick's Ward between Macdonell Street and Huron Street.

Cons:

- ✗ Removal of Designated Heritage structure (Allan's Bridge).
- ✗ Potential for minor encroachment into private cultural heritage property.

Capital Cost (structures only): \$13.3M

Macdonell Street Intersections

- Macdonell St, Woolwich St, and Wellington St E Intersection (West)
- Macdonell St, Arthur St N and Elizabeth St (East)
- Traffic operation, active transportation and safety improvements through intersection reconfiguration to be confirmed *following confirmation of recommendations for Macdonell, Allan's and Ward structures.*



Existing Macdonell Street Intersections Layout



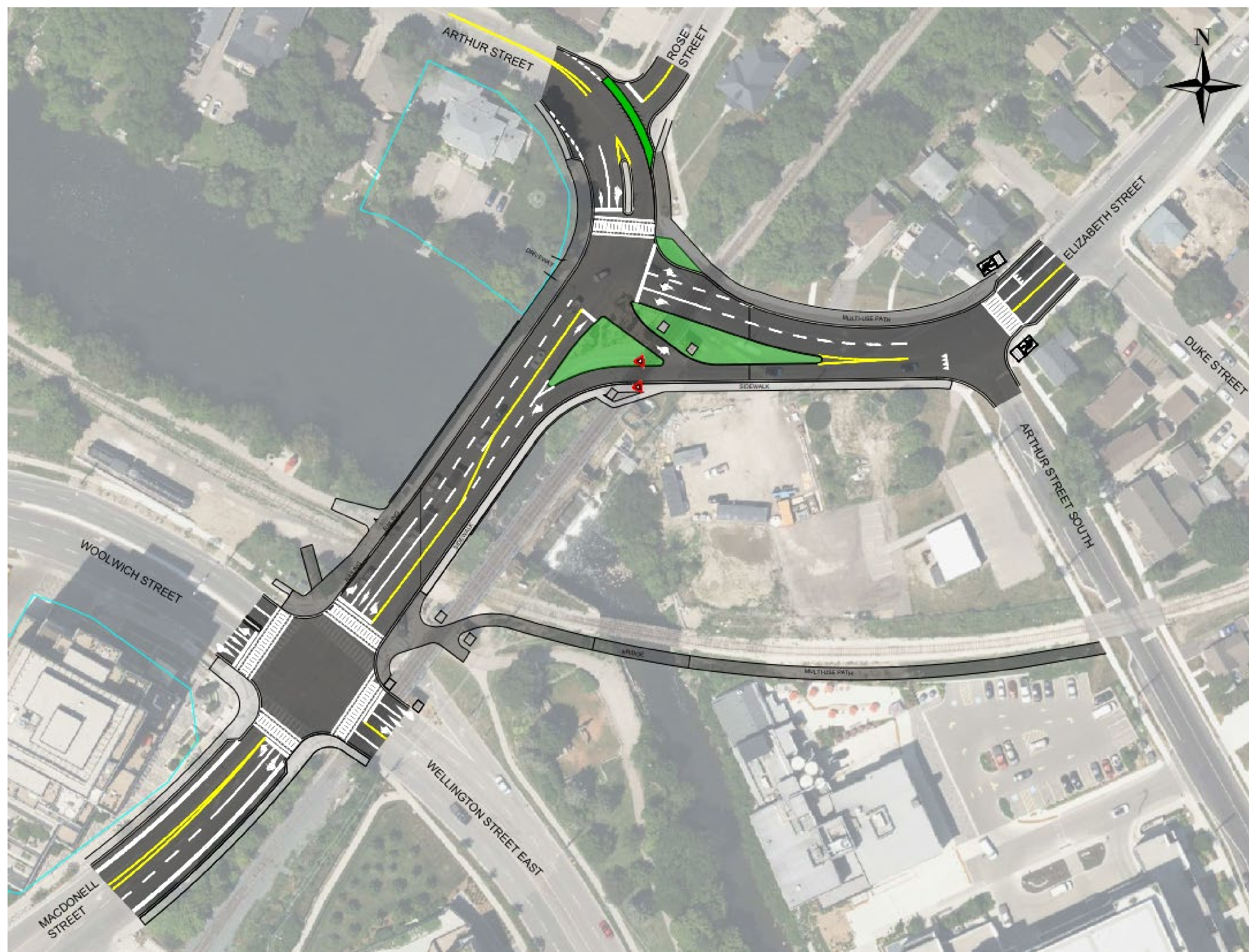
Macdonell St, Woolwich St, and Wellington St E Intersection



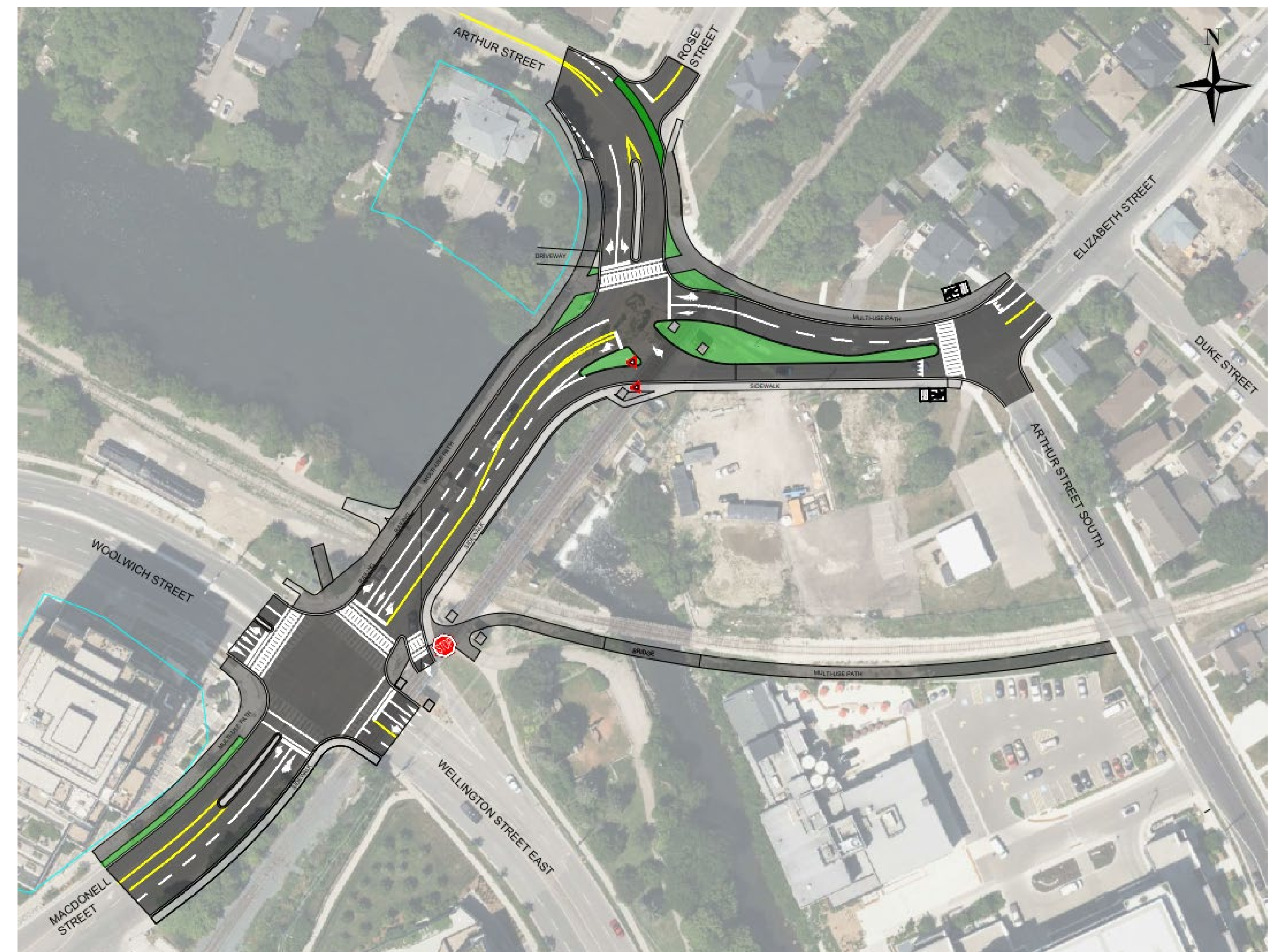
Macdonell St, Arthur St N and Elizabeth St Intersection

Preliminary Macdonell Street Intersections*

These intersections will be further reviewed and presented to the public once the EA for the Macdonell & Allan's structures has been finalized.



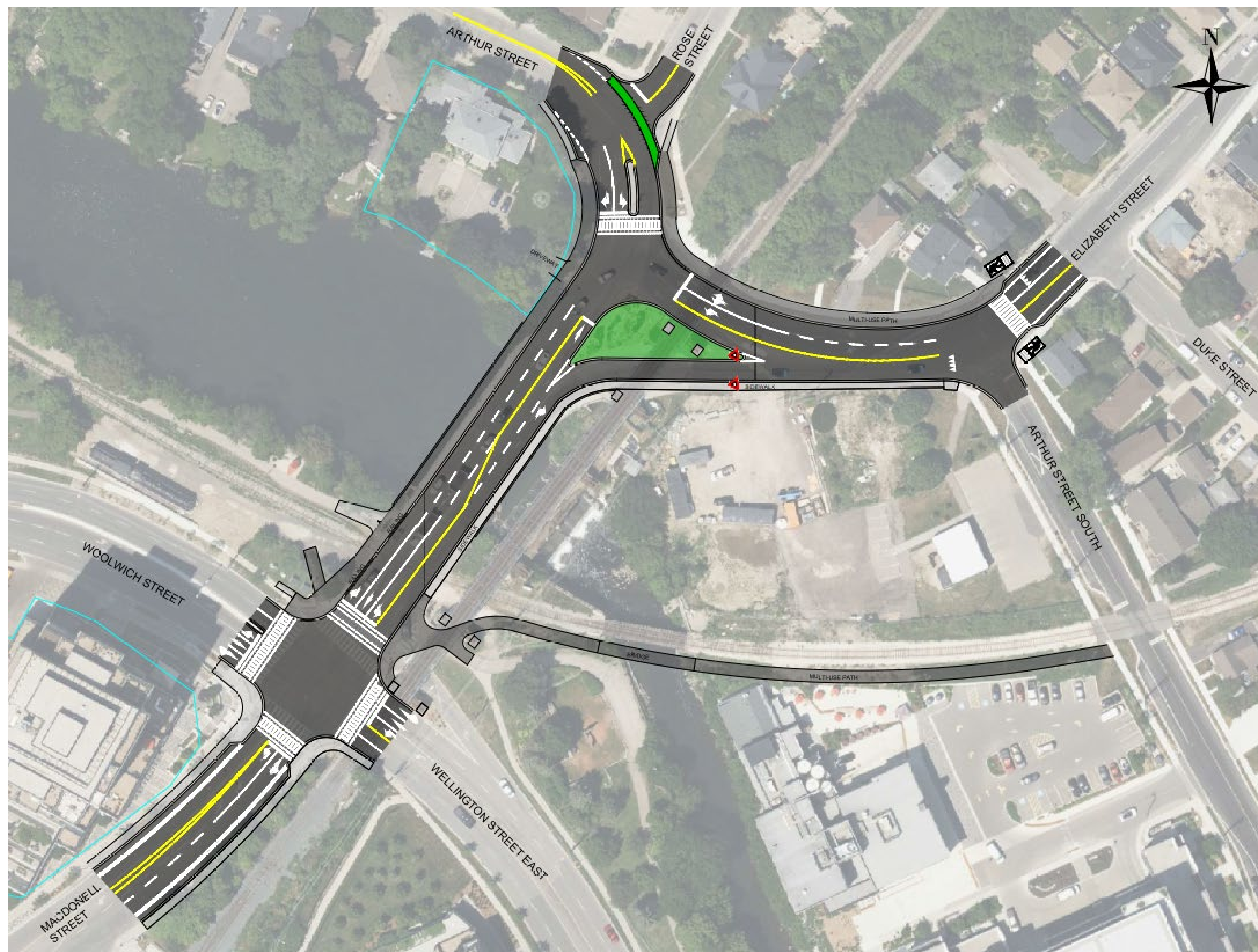
Macdonell Street Intersection Concept 1



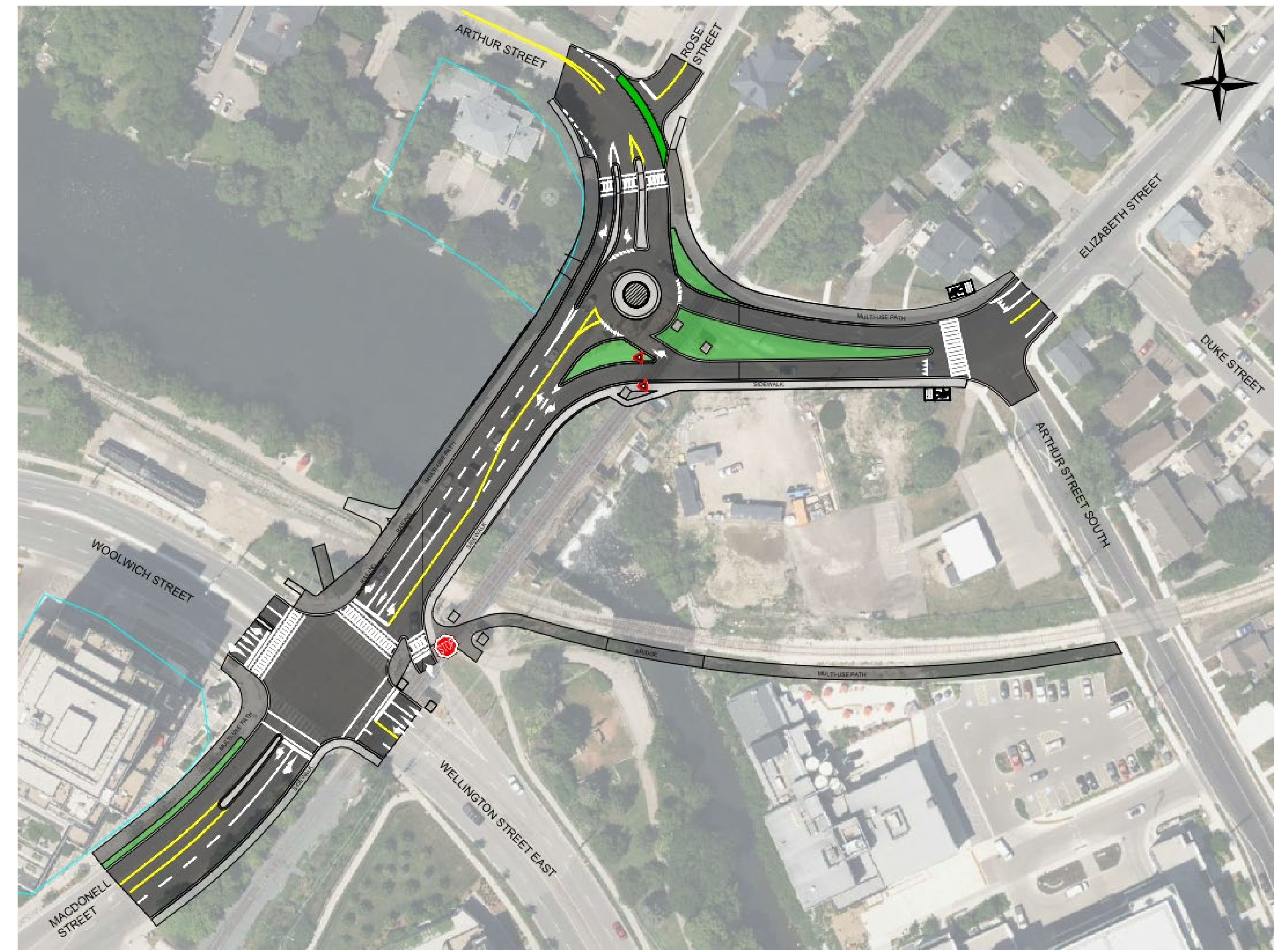
Macdonell Street Intersection Concept 2

Preliminary Macdonell Street Intersections*

These intersections will be further reviewed and presented to the public once the EA for the Macdonell & Allan's structures has been finalized.



Macdonell Street Intersection Concept 3



Macdonell Street Intersection Concept 4

Next Steps

1. Consult with additional interest-holders and technical agencies, as required, and complete necessary studies.
2. Confirm the preferred solution(s) in consideration of feedback received.
3. Prepare a Project File Report documenting the Class EA planning and design process followed.
4. Obtain Council approval on the study recommendations.
5. Submit Project File Report for 30-day public review.

Remember to provide your comments on the sheets available, or online, at haveyoursay.guelph.ca/downtownproject by January 12, 2025.



APPENDIX 12-5

Comments Received from Public



Mila Khatri

From: Andrew McGregor
Sent: August 30, 2021 9:56 AM
To: [REDACTED]
Cc: [REDACTED] Steven Di Pietro; Connor MacIsaac
Subject: RE: Macdonell and Allan Structures

Hello [REDACTED]

Thanks for the comments and interest in the project. Your comments will be circulated to the team and considered further as we proceed with the project. We will add you to the study contact list and notify you of project related activities and consultation events. Feel free to let us know if you have any additional concerns.

Kind regards,



Andrew McGregor, MCIP, RPP

SENIOR PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4211 | m 905 964 4056

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



SUMMER HOURS: RVA celebrates the summer season from June 4th to September 3rd. Our offices will be closed at 2 PM each Friday.

From: [REDACTED]
Sent: August 28, 2021 5:19 PM
To: Andrew McGregor <AMcGregor@rvanderson.com>
Cc: [REDACTED]
Subject: Macdonell and Allan Structures

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Andrew

[REDACTED] live at and own the 16 Arthur St N (unit 101).

I received the notice regarding the Environmental Assessment of the Allan's Bridge area a few days ago.

Any changes or construction in the area will have an major impact on us, our tenants, and our property. Bridge construction, dam replacement and road changes will be a major disruption during the construction and beyond. The construction period will be long for all these items.

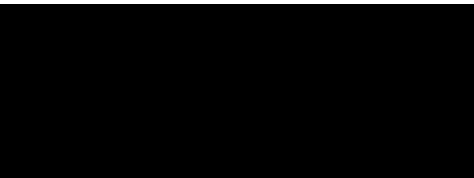
The work on the rail bridge is an example of the impact of change. The noise and traffic issues have been continuous all summer and will continue for some time. The changes in this assessment are much larger.

We would like early input on the assessment and continuous updates as the work moves along.

We don't want to make changes to our property and then find that these changes are damaged by future construction.

We look forward to being on your email list for updates.

Thank you for your time,



Mila Khatri

From: Andrew McGregor
Sent: October 31, 2022 4:53 PM
To: [REDACTED]
Cc: Steven Di Pietro; Connor MacIsaac
Subject: RE: Macdonell and Allan Renewal

Hi [REDACTED]

Thanks for the interest in the study. All of the materials to be presented will be placed on the project website haveyoursay.guelph.ca/downtownproject. The team is available to answer your questions throughout the study. We'll add your name to the mailing list as requested and be sure that you're notified of all study activities. We are still in the early planning stages but all feasible options will be considered for the noted intersection, including a roundabout configuration. However these won't be presented for comment until the 2nd Open House, anticipated to be held in early 2023.

Thanks again and kind regards,



Andrew McGregor, MCIP, RPP

SENIOR PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4211 | m 905 964 4056

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



From: [REDACTED]
Sent: October 20, 2022 10:00 PM
To: David.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: Macdonell and Allan Renewal

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hello David and Andrew,

I live near the Allan bridge and will love to hear more about its renewal and that of the intersection as a whole. Unfortunately, it's unlikely I'll be able to make the Open House. At least I'd like to be added to the mailing list.

One specific interest of mine: Will a 3-way roundabout be considered for the Eastern end of Macdonell? It seems well suited for one and the current traffic lights at that intersection are a long delay for all involved.

Best regards,
[REDACTED]

Mila Khatri

From: [REDACTED]
Sent: November 4, 2022 7:37 AM
To: [REDACTED]
Cc: [REDACTED] tracy.suerich@guelph.ca; Connor MacIsaac
Subject: Re: The Port City of Guelph

Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

[REDACTED]
We thoroughly enjoyed your presentation last night. So many wonderful photos that tell the Speed River story over time. Lots of wonderful memories.

Please get involved in the Allan Bridge EA. The City planners need to know the whole story before they attempt to draft the next chapter.

Respectfully yours,

[REDACTED]
Sent from my iPhone

On Nov 3, 2022, at 5:16 AM, [REDACTED]

[REDACTED]
I was at an EA preliminary meeting at City Hall last night. Among other things they floated a number of options at the site of the Allan Bridge. Mostly the options presented were very mechanical, artistic and engineering based solutions with little knowledge apparent or consideration of river history or possible land and river use going forward.

Before 1826 it was possible to paddle a canoe, without a portage, from the Speed River rapids and small waterfall at the site of the Allan Bridge all the way to Lake Erie. The journey can still be made by canoe, portaging around several dams, in about the same time as it takes to drive to Florida. Fish of a wide variety of species could make the same journey in both directions at that time. This became impossible for fish in 1827 when Guelph became a Port.

We have done a lot of things to our rivers in the last 200 years to protect us from the Americans and then get our trade goods to them. Most of those things we no longer need or use.

Before we add more THINGS on top of the THINGS of the past, we should understand what we have traded away at the rivers expense.

Let's make sure that we are not hoarding history. This is a time for new beginnings.

The Canada Company and John Galt had one clear objective and very limited technology to pull it off. We can do better.

I'm looking forward to your talk at the "Carnegie Library" this evening.

All the best from an old steel guy,
[REDACTED]



Sent from my iPhone

Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: January 5, 2023 10:42 AM
To: Andrew McGregor; Connor MacIsaac
Cc: Stacey Laughlin; David Di Pietro; Steven Di Pietro
Subject: Fwd: Downtown Renewal - Wyndham EA + Macdonell Bridge/Allan Dam Structures EA

Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Andrew and Connor,

Please see below for additional EA comments.

- Reg

From: Stacey Laughlin <Stacey.Laughlin@guelph.ca>
Sent: Thursday, January 5, 2023, 9:54 a.m.
To: Reg Russwurm <Reg.Russwurm@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Steven Di Pietro <Steven.DiPietro@guelph.ca>
Subject: FW: Downtown Renewal - Wyndham EA + Macdonell Bridge/Allan Dam Structures EA

Happy New Year all! I hope you had a fun and relaxing holiday season.

Please see the comments below from David Douglas.

Thanks,
Stacey

Stacey Laughlin, MCIP, RPP | Downtown Revitalization Advisor
Office of the Chief Administrative Officer
City of Guelph
519-822-1260 extension 2327
mobile 519-321-9601
stacey.laughlin@guelph.ca

From: [REDACTED]
Sent: Thursday, December 29, 2022 6:33 PM
To: Stacey Laughlin <Stacey.Laughlin@guelph.ca>
Subject: Re: Downtown Renewal - Wyndham EA + Macdonell Bridge/Allan Dam Structures EA

[EXTERNAL EMAIL] Do not click links or attachments unless you recognize the sender and know the content is safe.

Stacey:

Thank you for returning my recent call and for the e-mail.

Through this e-mail I would ask you to lodge my input to the Wyndham Street and the Macdonell Bridge/Allan's Dam EA projects, and forward these comments to your colleagues in the City.

I have already submitted some input through the City's *Have Your Say* facility on the former and now dilapidated Allans Dam Bridge.

SUBMISSION

Preamble

My preamble to the Wyndham Street project and the Macdonell Bridge/Adam's Mill dam (spillway) structures project, is essentially the same for both.

That is, they both must be approached in an integrated, comprehensive manner. The City has publicly committed to taking an integrated approach to all aspects of city building. City Council and Administration recognize the complex interconnections between all facets of urban design, land use planning, transportation planning, environmental planning and management, heritage and cultural planning, and all other dimensions of developing the urban community.

The "turf and territory" of professional practice fiefdoms are antithetical to modern urban development and management. "Silos" in whatever quarter are counterproductive, costly, and wasteful.

We all know that integration is easier said than done. And enabling legislative and regulatory institutions (e.g. from the Provincial government) do not always facilitate taking a holistic, interconnected and integrated approach to urban projects. But the onus is on all of us to pursue this objective.

I have worked through various Provincial and Federal EA frameworks over the years, and they are a very mixed blessing.

However, all of this does not excuse us from explicitly situating what look like discrete projects (e.g. bridge (re)building) into the larger whole, to ensure that all pertinent factors in design and build are incorporated, and that all possible benefits, direct and indirect, are gained from the initiative.

WYNDHAM STREET

The lead design and land use planning question for Wyndham Street is not to be found in the question in the EA on the City's website - to determine "the function of the road".

The formative question is "What is the function of Wyndham Street in a revitalized and vibrant Downtown Guelph?" Once the multi-functional specifications are explicitly articulated (e.g. cultural, social, economic) then its role as a street and as a movement channel or road, can be set out. The City does get to "streetscape" later on in its site.

For example, what are the operational requirements for a commercial business (e.g. frontage parking, loading/unloading zones, retail frontage for customers)? Or, what would Guelph residents require to honour, acknowledge, and manifest through activities, the cultural meaning(s) of this street? And so on.

As acknowledged by the City, the future functions of St. George's Square is a pivotal consideration here. Its primary function should be a city centre function, a symbolic, physical and functional centre of the growing urban community. Most thriving and healthy cities have such a centrepiece. They have huge symbolic value, are creators of meaning, engender pride, embed memories, and a sense of place. All of the canons of urban planning and design speak to pivotal and evocative centres of gravity of the urban realm. Every city where we spend significant amounts of time and money to visit, is characterized by these centerpieces. The renewal of our Downtown provides a singular opportunity, and responsibility, to get this right. To ensure that this space speaks to what Guelph is, where it has come from, and where it is going. It should, by design, be iconic.

Given the above, the circulation facets of this centrepiece must fit into, and in no way compromise its pivotal and primary function in city building. Automobile traffic, is necessary for the other functions of the Downtown and its constituent streets. But it must not be the determining factor in the design and re-building of St. George's Square. Alternative transportation modes are no less important, and are becoming more important. But, again, these are subsidiary factors in the design and re-building of the urban community's central place.

Besides the physical symbols and representations, such as sculptures, water features, ice features, lighting, flag poles and banners, trees and flower beds, surface treatments, and other facets of the design, the scale of the place and how it manifests itself to the pedestrian, the onlooker, and others is critically important. The importance of an all weather plaza should be a central consideration. Facilitating different types of performance and events spaces should be integral to the design and re-build.

All of this suggests the primacy of scale, edges, visual impact and presence.

Traffic junctions, automobile, van and small truck flows and ease of movement, while important, must be secondary considerations here. This situates the junctions of the adjoining streets (e.g. Wyndham, Quebec, Douglas) and means that selective flow-through should be permissible, but that the pedestrian access and use of the civic plaza should be the central design consideration. The junctions and traffic circulation around the plaza should facilitate through-traffic flows for very limited and specific periods of time. What would in conventional traffic engineering terms be complete junctions, with all conventional turning movements provision, should, for most of the time, be turnaround spaces. allowing for the normal up and down traffic of all streets, but with a minimum of restricted flow-through (i.e. around the plaza into other adjoining streets) traffic.

Limited off-peak delivery traffic should be accommodated with flow-through provisions, and increased access to the rear of Wyndham Street businesses (e.g. through expanded provision off Douglas Street laneways, though the redeveloped Baker District).

This proposed re-design of St. George's Square might be expressed as a cross between the City's *Option 5* and *Option 6*.

In terms of traffic infrastructure for Wyndham Street itself, these considerations suggest a maximum of two lanes for automobile and related local circulation, two lanes (preferably together) for cyclists, angled automobile parking, and expanded sidewalks for pedestrians.

MACDONELL BRIDGE

I have provided my input on the old decrepit and unused part of the joint infrastructure here, under the Adams Dam Bridge and Sluiceway/Spillway submission, under separate cover.

In that I have stressed, as above, the imperative of seeing this set of infrastructures (current Macdonell Bridge, Spillway/Dam, and unused, decrepit Adams Bridge) in the context of Downtown revitalization, and in terms of the site's location in Guelph itself, as a historical city, and as a growing urban community. This is not only an engineering project. It is not solely a transportation planning project. It is not a discrete traffic engineering project. These interrelated elements on the site have to be addressed, and solutions are required. But, the complex site is in fact a challenge in *place making*, as an integral and very important element in overall city building.

The founding of the City of Guelph (1827), the periodic presence of Indigenous people well before any European interest, the early river-based industrial activities here, the intersections of railway interests and their investments, and several other facets of this place's importance in the Guelph story means that this has meanings and values well beyond the current imperatives of transportation planning and management.

Add to these considerations the multifunctional site's role as a gateway to the Downtown. Then add in its role as something of a "bookend" to a rejuvenated and re-designed Macdonell Street (boulevard), with the hill-top Basilica as the complementary "bookend". Then add in the opportunity to re-connect community and river, and the emphasis shifts from "getting over the river" to "getting to the river".

These multifaceted considerations, very much the stuff of urban planning, mean that we now have a confluence of formative perspectives here, that raise the imperative of place making as *the* agenda for the Macdonell "bridge". And that is how this matter should be approached.

The resolution of automobile movement issues and challenges (throughflow), the resolution of turning movements and related matters (traffic engineering and pedestrian safety), and the associated choices in structural engineering (e.g. re-building the bridge platform, reconfiguration of the dam/spillway), and all related matters must respond to and directly reflect the multifunctional dimensions of this unique place in Guelph's Downtown, and in the city as a whole.

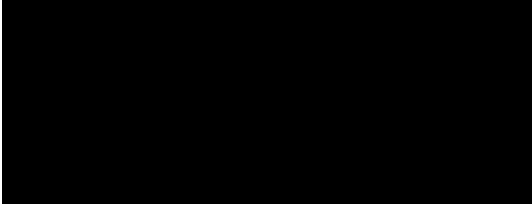
The amount of motorized traffic to be accommodated here, the mix of traffic types, the pedestrian opportunities here, the river viewing and enjoyment activities, the bridge and street lighting profile, the presence of historical and cultural installations, the planting, the integration of the bridge itself with the re-designed, treed urban boulevard (Macdonell Street), and so on, will all manifest the urban place making that characterized this unique place in our city.

For the Macdonell Bridge proper all of this suggests a complete replacement of the present structure, and the redesign of a bridge that serves its multifunctional potentials, as suggested above.

Likewise, the sluiceway and spillway should be refurbished to both ensure their functions as water course management entities, likely with GRCVA oversight, and as recreation/tourist facilities. The latter might involve relocating the spillway (weir) so that it has a greater visual impact (e.g. 1-200 metres upstream) and using the refurbished Allan Dam bridge itself as a viewing site and as a supplementary pedestrian way between Elizabeth Street and Wellington Street East.

Any questions on this submission, please contact me.

Thank you for this opportunity and for your attention in these matters.



From: Stacey Laughlin <Stacey.Laughlin@guelph.ca>

Sent: Thursday, December 15, 2022 1:36 PM

To: [REDACTED]

Cc: Reg Russwurm <Reg.Russwurm@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Steven Di Pietro <Steven.DiPietro@guelph.ca>

Subject: Downtown Renewal - Wyndham EA + Macdonell Bridge/Allan Dam Structures EA

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Hi [REDACTED]

Thank you for reaching out and thank you for filling out the survey that's currently open on Have Your Say. If you'd like to provide additional comments on the Wyndham Street EA or the Macdonell Bridge/Allan Dam structures EA, the Program Manager for this work is Reg Russwurm. The Project Manager for the Wyndham EA is David Di Pietro and the Project Manager for the Macdonell Bridge/Allan Dam structures EA is Steven Di Pietro. I've copied all of them on this email so that you can provide any comments you may have.

Thanks,
Stacey

Stacey Laughlin, MCIP, RPP | Downtown Revitalization Advisor
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Mila Khatri

From: Susan Hall <shall@lura.ca>
Sent: January 8, 2025 8:34 AM
To: Andrew McGregor; Mila Khatri; Susan Jacob
Subject: FW: Allans Dam - Spillway and Sluiceway

Follow Up Flag: Follow up
Flag Status: Flagged

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Hi RVA team

Please add this to the comments on the EA record.

Thanks,
Susan

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: January 8, 2025 8:32 AM
To: Susan Hall <shall@lura.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Tracy Suerich <Tracy.Suerich@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: FW: Allans Dam - Spillway and Sluiceway

Susan,

Please add this email to the EA Record of comments. I've also encouraged the writer to fill out the HYS survey.

- Reg

From: [REDACTED]
Sent: Tuesday, January 7, 2025 5:49 PM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>; Andrew Miller <Andrew.Miller@guelph.ca>
Subject: Allans Dam - Spillway and Sluiceway

[EXTERNAL EMAIL] This email originates outside the City of Guelph. Do not click links or attachments unless you recognize the sender and know the content is safe.

Dear Reg and Andrew,

I hope you are having a fantastic 2025 so far.

I am reaching out regarding the Downtown Infrastructure Renewal EA underway with focus on the Allan's Dam Spillway and Sluiceway. As a nearby resident, I frequently run and walk by this area and always

think about how gross the water under the Heffernan Street Bridge. I look forward to the times of year when the dam is let out and a meandering channel forms in the riverbed and the water goes from murky brown to clear.

In learning about the EA underway, I was disappointed to read that the Sluiceway and Spillway are not being removed. I think this is a huge missed opportunity for our river. The removal could facilitate creation of over 4 acres of riparian habitat. This area could be filled with beautiful native plants, pollinators, and birds people could see from many different vantage points. Reducing the volume of stalled water could support a cooler waterway as this is labeled a cold water habitat in our official plan. The cooler water may have significant impacts to water characteristics downstream.

I fear that the current study over emphasizes the property values and fear of change instead of the natural benefits. I ask that the removal of the dam features be reconsidered, and that language representing the natural heritage benefits be more plainly stated and heavily weighted in the evaluation of the sluiceway and spillway removal.

Please do not take this decision lightly, this could be one of the most valuable changes to our downtown river for a long time!!

Best,

A black rectangular redaction box covering the signature of the sender.

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Mila Khatri

From: Andrew McGregor
Sent: February 13, 2025 1:37 PM
To: Mila Khatri
Subject: FW: Addition studies needed on Allan's Dam
Attachments: Allan's Dam Recommendations RSMP.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Fyi

Andrew McGregor, MCIP, RPP

Associate, Senior Planner Transportation



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From: Andrew Miller <Andrew.Miller@guelph.ca>
Sent: February 13, 2025 1:05 PM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>
Subject: FW: Addition studies needed on Allan's Dam

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Reg,

Please see below email from [REDACTED] with the University of Guelph. The comments made seem valuable for consideration of the rehabilitation option of the Allan's Dam and Sluiceway. I understand that we've had some challenging interactions with [REDACTED] in the past, particularly in the Clair-Maltby Secondary Plan, so I'm looking for direction on how you'd like to reply before either Andrew or I respond.

Kind regards,

Andrew Miller, P.Eng, PMP | Project Engineer
D&C, Infrastructure, Development, & Environment Services
City of Guelph

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[@cityofguelph](https://www.facebook.com/cityofguelph)

From: [REDACTED]

Sent: Thursday, February 13, 2025 12:56 PM

To: Andrew Miller <Andrew.Miller@guelph.ca>; amcgregor@rvanderson.com

Subject: Addition studies needed on Allan's Dam

[EXTERNAL EMAIL] This email originates outside the City of Guelph. Do not click links or attachments unless you recognize the sender and know the content is safe.

Greetings:

I have observed that the description provided in the Macdonell and Allan Structures Class Environmental Assessment of the functional purpose for the Allan's Dam, Spillway and Sluiceway is incomplete and misleading. This submission contains suggestions for improving this description and recommendations for additional studies need before a final decision is made on the best future option for the Allan's Dam.

The statement that "these structures work together to limit or release water flow along the river as needed" suggests that the Allan Dam controls a significant volume of reservoir storage and that the storage volume is manipulated to store water and reduce downstream peak flowrates during high-flow conditions and to supplement downstream flowrates during extended dry-weather periods.

The suggestion that the Allan's Dam reservoir has a large enough active storage that it can be used to regulate flowrates in the Speed River is false. It is true that the Guelph Lake Reservoir has a large enough active storage volume to be used to diminish downstream flood peaks and augment dry-period flows in the Speed River. However, the Allan's Dam active storage volume is less than 1% of the Guelph Lake volume and is much too small to be used for flowrate modification.

The operational function of the Allan's Dam Structures is, instead, to provide a large ponded-water surface in the Speed River valley for the months from May through October. This ponded-water surface forms when the gate in the Sluiceway is closed (it takes a day or so to form) and remains until the Sluiceway gate is raised.

You have correctly noted that this operational function for the Allan's Dam produces intrinsic heritage value, recreation and aesthetic value and opine that these values are sufficient to justify retention of Allan's Dam. The 1993 River System Management Master Plan reached the same conclusions (see attached abstract) but attached a set of observations about Allan's Dam that have not been considered in the present study so far.

I draw specific attention to two aspects that require more study. The first is the observation that the recreational and aesthetic values of the ponded water are now limited to half the year because the sluiceway gate must be kept raised from October to May to minimize the flood hazard created when the sluiceway gate is closed. The result, as noted in the RSMP is an unattractive and detrimental viewscape

for half the year. I would add the personal comment that the recreational value of Allan's Pond seems to be declining over time as recreational patterns shift.

The decline in heritage, recreational and aesthetic values because of the seasonality of the ponded condition should be explicitly taken into account in any decision on the retention of Allan's pond.

The other aspect requiring further study is the impact of Allan's Dam on the location of the floodplain boundary in the stretch of the Speed River upstream of the dam to Eramosa Road. The RSMP dealt with flood hazards associated with the dam when the sluiceway gate is closed but did not consider the effect of the dam on the flood line elevation upstream of the dam during the Regional Storm.

What is needed is a study by the GRCA of the drop in elevation and change in position of the Regional Storm flood line for the reach of the Speed River between the dam and Eramosa Road that would be created and shown in GRCA mapping if the dam structure is removed. The increased value of land removed from the floodplain could then be assessed and a judgement made about whether this increased value justified the loss of heritage, recreational and aesthetic values.

Your report should also acknowledge that retention of the dam creates annual operational costs that include maintenance and testing of the gate structure on the sluiceway and maintenance and testing of an emergency response system that assures the raising of the sluiceway gate in response to a flood warning system alarm during summer months when the gate is normally closed.

Best regards



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APPENDIX 12-6

Comments Received from Technical Agencies



APPENDIX 12-6-1

City Natural Environment Staff



Mila Khatri

From: Jason Elliott <Jason.Elliott@guelph.ca>
Sent: February 22, 2023 3:15 PM
To: Courtney Beneteau
Cc: Steven Di Pietro; Jennifer Juste; Connor MacIsaac; David OSullivan; Andrew McGregor; Paul Mikoda; Natasha Welch
Subject: Re: PN0061 Wyndham Street Class EA: Long List to Short List Workshop - Information Package (RVA 215632.01)

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Hi Courtney,

Thanks for the opportunity to provide my thoughts.

My concerns with Table 4.1 Natural Environment Goals rankings centre around the interplay with the Macdonell Bridge alternatives, the planned [Ward to Downtown pedestrian bridge](#) immediately adjacent to the Allans Dam Bridge, Policy 4.1.3.5.12 of the [Official Plan](#) (reproduced below), and general opportunities to improve the health of the river.

4.1.3.5.12: The City will continue to investigate the feasibility of removing/modifying structural barriers to fish passage in the Speed and Eramosa Rivers and their tributaries in order to permit natural stream processes, improve fish habitat and the restoration of natural stream morphology

As accommodation of all modes of travel including active transportation will be considered during Phase 3 for the proposed short-listed alternatives for the MacDonell Bridge, Alternatives 2 and 3 for the Allan's Dam bridge would result in at least two and possibly three active transportation bridges across the river in very close proximity. While this stretch of the river is severely impacted such that three bridges would not represent much additional degradation, it would go against general principles of minimizing watercourse crossings and prevent an opportunity to improve the riparian zone of the river in this area through some native naturalization that could be realized if the Allans Bridge was removed.

Further, as the existing Allans Bridge is supported with an in-river pier, Alternative 2 and perhaps Alternative 3 would prevent it from being removed from the river. This has interplay with OP Policy 4.1.3.5.12. Depending on the preferred alternative for the Allans Dam Spillway the positive impacts of the pier's removal may be small with respect to the health of the river (i.e. if the spillway remains) but if the Allans Bridge and the Allans Spillway are being assessed separately, this must be considered separately. Certainly, it would be a concern if maintaining the Allans Bridge and pier affected the assessment for the Sluiceway (i.e. it caused the alternatives where it is removed to score lower).

With the above in mind, I don't agree with ranking Alternatives 1-3 as somewhat aligning with natural environment goals as that would mean the goals are to maintain or slightly exacerbate a severely impacted stretch of the river. I assume that the proposed rankings were mainly focused on negative impacts to the river that may result from the alternatives (i.e. construction impacts) given the description of the evaluation criteria in Table 6-1 that references "existing" natural features and functions. However, it doesn't appear that

cumulative negative impacts have been taken into account and EAs should also assess positive impacts and against policy (these latter two should be included in the evaluation criteria). As the three-rank system doesn't allow for much nuance, I think that the rankings for Alternatives 1-3 should be "Does not align with Natural Environment Goals" and the ranking for Alternative 4 should be "Fully Aligns with Natural Environmental Goals".

As an aside, if you haven't already, be sure to review and consider the information in the Guelph Pedestrian Bridges Ward to Downtown Scoped EIS in this EA. It can be found on the project website that using the link I provided above. Among other useful data and information, note that that project will include invasive species management and naturalization plantings.

My concerns with Table 5.1 Natural Environment Goals rankings centre on Policy 4.1.3.5.12. That policy and Table 6.1 make it clear that the most relevant natural environmental goals are the removal of the spillway and sluiceway (positive impacts to the existing natural features and functions) and addressing climate change (increased vegetation for carbon storage). This is reflected in the proposed ranking for Alternative 4 (removal of the barriers in the river and the significant amount of additional vegetation that would result in the restored upstream floodplain). However, as Alternatives 1 and 2 would maintain those structures, I think that those rankings should be "Does not align with Natural Environment Goals". I am not clear how Alternative 3 is possible or what the design may look like, but it also appears to represent a significant positive impact aligned with Policy 4.1.3.5.12 that would also result in a significant amount of additional vegetation in the upstream floodplain. While these positive impacts may be slightly less than Alternative 4, due to the coarseness of the three-rank system, I think it should also be ranked as 'fully aligns'.

Finally, as I indicated in my previous email, it appears that the Active Transportation Underpass from Alternative 5b (proposed to be removed from consideration) could be incorporated into Alternative 4. Perhaps it could also be incorporated into Alternative 3? If so, that would increase the amount those alternatives address climate change. As such, I suggest that this should be considered prior to evaluating the short list.

Please let me know if I can provide any clarification or if you would like to discuss further. Looking forward to the workshops tomorrow.

Jason

Jason Elliott, Environmental Planner
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From: Courtney Beneteau <cbeneteau@rvanderson.com>
Sent: February 21, 2023 1:39 PM

To: Jason Elliott <Jason.Elliott@guelph.ca>

Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; Jennifer Juste <Jennifer.Juste@guelph.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Andrew McGregor <AMcGregor@rvanderson.com>; Paul Mikoda <pmikoda@rvanderson.com>; Natasha Welch <nwelch@rvanderson.com>

Subject: RE: PN0061 Wyndham Street Class EA: Long List to Short List Workshop - Information Package (RVA 215632.01)

Hi Jason,

On behalf of Andrew and myself, thank you for your comments. We would definitely like to better understand your concerns with the natural environment evaluations, and appreciate your offer to provide additional information. Would you be able to provide your comments on the tables so that we can review, and have time to discuss if necessary, prior to the workshop on Thursday?

Thanks again,
Courtney

Courtney Beneteau, M.Sc., CAN-CISEC (she / her)

Fisheries Biologist



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From: Jason Elliott <Jason.Elliott@guelph.ca>

Sent: Thursday, February 16, 2023 4:52 PM

To: Andrew McGregor <AMcGregor@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; Jennifer Juste <Jennifer.Juste@guelph.ca>

Subject: Re: PN0061 Wyndham Street Class EA: Long List to Short List Workshop - Information Package (RVA 215632.01)

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Hi Andrew / Connor,

I have yet to review the material for Wyndham St. but I just reviewed the Macdonell and Allan Structures materials and decided to chime in right away with potential concerns as asked. While I support the proposed short lists (with the caveat regarding Active Transportation below), I don't support some of the Natural Environment Goals preliminary review rankings for the Allans Dam Bridge (Table 4.1) and Sluiceway/Spillway (Table 5.1). As I support the short lists, I am not sure how important this is for the workshop, but I wanted you to be aware of this. If you would like more information on where and how I have disagreement, I am happy to provide it. Certainly, we should work through this prior to the evaluation of the short list.

I also note for the Sluiceway/Spillway that the Active Transportation Underpass from Alternative 5b (proposed to be removed from consideration) could be incorporated into Alternative 4 so that piece is not lost.

Thanks,

Jason

Jason Elliott, Environmental Planner
Planning and Building Services
City of Guelph
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TTY 519-826-9771
jason.elliott@guelph.ca

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From: Andrew McGregor <AMcGregor@rvanderson.com>
Sent: February 16, 2023 2:57 PM
To: Connor MacIsaac <cmaclsaac@rvanderson.com>; Timea Filer <Timea.Filer@guelph.ca>; Leanne Warren <Leanne.Warren@guelph.ca>; Stacey Laughlin <Stacey.Laughlin@guelph.ca>; Jason Elliott <Jason.Elliott@guelph.ca>; Dave Beaton <Dave.Beaton@guelph.ca>; Rob Reid <Rob.Reid@guelph.ca>; Jennifer Juste <Jennifer.Juste@guelph.ca>; Gwen Zhang <Gwen.Zhang@guelph.ca>; Robin Gerus <Robin.Gerus@guelph.ca>; Rory Templeton <Rory.Templeton@guelph.ca>; Kyle Gagne <Kyle.Gagne@guelph.ca>; Terry Dooling <Terry.Dooling@guelph.ca>; David deGroot <David.deGroot@guelph.ca>; Jamie Zettle <Jamie.Zettle@guelph.ca>; Laura Catalano-Bragues <Laura.Bragues@guelph.ca>; Stephen Robinson <Stephen.Robinson@guelph.ca>; Alex Jaworiwsky <Alex.Jaworiwsky@guelph.ca>; Christine Chapman <Christine.Chapman@guelph.ca>; Tracy Suerich <Tracy.Suerich@guelph.ca>
Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; Matthew Di Maria <MDiMaria@rvanderson.com>; Stanley Pijl <SPijl@rvanderson.com>; Susan Hall <shall@lura.ca>; David OSullivan <dosullivan@rvanderson.com>; Natasha Welch <nwelch@rvanderson.com>; Melissa Gallina <mgallina@lura.ca>; Reg Russwurm <Reg.Russwurm@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>
Subject: RE: PN0061 Wyndham Street Class EA: Long List to Short List Workshop - Information Package (RVA 215632.01)

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Hello All,

Following up on Connor's email below, attached please find attached a similar package pertaining to the Macdonell and Allan Structures Class EA workshop, to be held later the same day, 1 p.m. to 3 p.m.

Thanks and kind regards,

Andrew McGregor, MCIP, RPP

Senior Planner / Project Manager, EA & Approvals



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43 Church Street, Suite 104, St. Catharines ON L2R 7E1
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From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: Wednesday, February 15, 2023 1:56 PM

To: timea.filer@guelph.ca; Leanne Warren <leanne.warren@guelph.ca>; Stacey Laughlin <Stacey.Laughlin@guelph.ca>; Jason Elliott <jason.elliott@guelph.ca>; dave.beaton@guelph.ca; Rob Reid <rob.reid@guelph.ca>; jennifer.juste@guelph.ca; Gwen Zhang <Gwen.Zhang@guelph.ca>; Robin Gerus <robin.gerus@guelph.ca>; Rory Templeton <rory.templeton@guelph.ca>; Kyle Gagne <kyle.gagne@guelph.ca>; terry.dooling@guelph.ca; David deGroot <david.degroot@guelph.ca>; Jamie Zettle <jamie.zettle@guelph.ca>; Laura Catalano-Bragues <Laura.Bragues@guelph.ca>; stephen.robinson@guelph.ca; alex.jaworiwsky@guelph.ca; Christine Chapman <christine.chapman@guelph.ca>; Tracy Suerich <Tracy.Suerich@guelph.ca>

Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; Matthew Di Maria <MDiMaria@rvanderson.com>; Stanley Pijl <SPijl@rvanderson.com>; Susan Hall <shall@lura.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Natasha Welch <nwelch@rvanderson.com>; Melissa Gallina <mgallina@lura.ca>; Reg.Russwurm@guelph.ca; David Di Pietro <David.DiPietro@guelph.ca>

Subject: RE: PN0061 Wyndham Street Class EA: Long List to Short List Workshop - Information Package (RVA 215632.01)

Hello,

Thank you to those of you who have RSVP'd to participate in the Wyndham Street Municipal Class EA workshop scheduled next Thursday from 10 a.m. to 12 noon, in Meeting Room C at City Hall. The purpose of the workshop is to pair down the "long list" of alternative solutions presented at the PIC, to a "short-list" of a maximum of 4 options to move forward for a robust evaluation. If you have not RSVP'd to the meeting, please ensure that you do so ASAP to allow the project team to plan appropriately.

To achieve the objectives of the workshop as listed in the attached agenda (including obtaining general acceptance of the options to be carried forward for a robust evaluation), it is expected that staff review the attached supplementary information over the next week, prior to attending the workshop. The attached technical memorandum includes details on the "long list" of options presented at Open House #1 within the context of higher-order planning documents and other City priorities/drivers/goals, as well as a preliminary recommendation (for discussion Thursday) on the "short-list" of options to move forward for a future robust evaluation. Please note that a full evaluation will not be completed during this meeting.

A high-level summary of each option is also included in the attached slide deck. Please contact the project team in advance of the workshop if you have any questions or concerns regarding any of the materials, or the workshop in general. Thank you in advance for your participation and preparation for this workshop as your input is invaluable to the project team in developing recommendations for this critical corridor.

Best,

Connor MacIsaac, ENV SP, EPT

Environmental Planner, EA & Approvals



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43 Church Street, Suite 104, St. Catharines ON L2R 7E1
t 905 685 5049 ext. 4218

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-----Original Appointment-----

From: Connor MacIsaac

Sent: January 31, 2023 1:54 PM

To: Connor MacIsaac; Andrew McGregor; Susan Hall; Matthew Di Maria; Stanley Pijl; Reg.Russwurm; David Di Pietro; timea.filer@guelph.ca; Leanne Warren; Stacey Laughlin; Jason Elliott; dave.beaton@guelph.ca; Rob Reid; jennifer.juste@guelph.ca; Gwen Zhang; Robin Gerus; Rory Templeton; Kyle Gagne; terry.dooling@guelph.ca; David deGroot; Jamie Zettle; stephen.robinson@guelph.ca; alex.jaworiwsky@guelph.ca; Christine Chapman; Tracy Suerich
Cc: Steven Di Pietro; David OSullivan; Natasha Welch; Melissa Gallina; Laura Catalano-Bragues
Subject: PN0061 Wyndham Street Class EA: Long List to Short List Workshop (RVA 215632.01)
When: February 23, 2023 10:00 AM-12:00 PM (UTC-05:00) Eastern Time (US & Canada).
Where: Guelph City Hall; - Meeting Room C

Workshop to pair down the "long list" of alternative solutions presented at the PIC, to a "short-list" of 3-4 options to move forward for a robust evaluation for the Wyndham Street Municipal Class EA.

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Internal Memo



Date December 6, 2024

To **Andrew Miller, Project Engineer**

From Leah Lefler, Environmental Planner

Service Area Infrastructure, Development and Environment

Department Planning

Subject Macdonell and Allan's Structures Schedule 'B' Class EA – Evaluation of Alternative Solutions Technical Memorandum - Draft

Environmental planning staff have reviewed the Technical Memorandum prepared by R.V. Anderson Associates Limited, dated November 1, 2024 on the Macdonell and Allan's Structure Schedule 'B' Class EA – Evaluation of Alternative Solutions. The following comments are offered:

1. In section 3.3.3 Alternative 3 – Remove Spillway and Sluiceway, it is unclear what the word "significant" means and what significance is based on to evaluate impacts to the hydraulic elevation upstream. The consultant is encouraged to update the Technical Memorandum to provide a clear description of what the assessment of significance is based on in this context. Further, please provide the hydraulic evaluation to environmental planning for review.
2. Environmental planning staff are not supportive of any alternative that is inconsistent with Official Plan policy. For example, section 3.3.4 Alternative 4 – Remove Spillway and Sluiceway and Build New Dam Upstream with an Active Transportation Underpass includes the creation of a new dam. Official Plan policy provides the direction to remove barriers and restore rivers, not impact them further. Official Plan policy also requires infrastructure projects to demonstrate no negative impact and a net ecological gain to the natural heritage system.
3. Please clarify if Alternative 3 – Construct a Simplified Bridge Adjacent to the South Side of the GJR Bridge (Section 3.4.3) would require or necessitate the removal of Allan's Bridge.
4. In Section 4.0 Summary of Existing Conditions, please confirm if a future conditions assessment was completed to determine impacts to hydraulic conditions. If a future conditions assessment has not been completed, please clarify how impacts to hydraulics has been evaluated following from the removal of the dam.
5. In Section 4.2 Environmental Impact Study, it appears that the existing conditions memo is based on a desktop review and that a comprehensive field program will be completed to identify impacts and mitigation measures for the preferred alternative. Typically, the field program and data collected during the field program would be used to inform the selection of the preferred alternative.

Further, environmental planning staff are typically involved with scoping the Terms of Reference for Environmental Impact Studies prior to their completion.

6. In Section 4.2.4 Designated Natural Areas, please note that the study area also includes fish habitat.
7. The Cultural Heritage Resource Assessment section takes a very Eurocentric view of cultural value. The Evaluation Criteria provided in Table 5.1 includes Cultural Heritage / Archaeological criteria described as follows: Will the alternative affect archaeological, cultural heritage resources or Indigenous communities. In light of the recent Council approved Indigenous Relations Framework, consideration should be given to expanding the assessment to include the significance of the Speed River to Indigenous Peoples.
8. The Hydraulic Analysis described in Section 4.7 refers to impacts to the water level elevation of the Speed River and significant public scrutiny. Environmental planning staff are concerned with the bias presented in the technical memo and the lack of technical data to support the claims of significant impact. For example, to what extent will the water level elevation be impacted upstream? For how many months of the year? These questions should be evaluated and assessed if this is a criterion being used to influence the outcome of the EA. Further, the text appears to accept that public perspective is limited to the unfavorable impacts to private property and recreation opportunities resulting from reduced water elevation. Public perspectives on restoring river systems and ecological enhancements appear to be entirely lacking.
9. In Section 5.4 Allan's Dam Spillway and Sluiceway, please confirm if leaving the spillway and sluiceway as is without investing in repairs or restoration of the structures was considered as an option. If these structures continue to be repaired or restored will there ever be justification for their removal? In other words, does rehabilitating them mean that future EAs will also determine not to remove them. If this is the case, it seems that the only way for in-stream barriers to be removed would be if the structure fails and falls apart.
10. Environmental planning staff have the following comments on the evaluation of criteria shown in Table 5.4 Evaluation of Alternative Solutions for Allan's Dam Spillway and Sluiceway:
 - a. Under alternative 3. Remove Sluiceway and Spillway, the assessment of the impact on the hydraulic function of the river should be reassessed to consider and be transparent about what "significant" is based on. For example, hydraulic function should be assessed in terms of flood hazard, water quality, stream stage/flow, ecological function, etc.
 - b. Under alternative 3. Remove Sluiceway and Spillway, assessment of impacts to property and/or recreational value should be evaluated in greater detail. For example, how much of a difference and for what period of time are water levels impacted? What is the social benefit of removing the sluiceway and spillway? (e.g., increased urban forest canopy, increased water quality, etc.).
 - c. Under alternative 3. Remove Sluiceway and Spillway, the evaluation of natural environment and climate change is summarized as follows: "extensive impacts (and extensive restoration opportunity) to the aquatic

and terrestrial environments anticipating from removal of the sluiceway and spillway". Environmental planning staff are unclear on how the scoring has been applied. Extensive restoration opportunity for this option is ranked the same as other alternatives that are assessed as "minor anticipated impacts to the aquatic environment". Additional discussion on this aspect is strongly encouraged.

- d. Consideration should be given to evaluating construction impact separately from long-term impact when evaluating environmental impact.
- 11. The alternative "recommended to be carried forward" is 2. Rehabilitate Sluiceway and Spillway. The cost to implement the alternative to rehabilitate the sluiceway and spillway is \$280K whereas the cost to remove the sluiceway and spillway is \$200K. What is the justification for spending \$80K more on rehabilitating these structures?
- 12. In general, the Technical Memorandum appears to prioritize avoidance of water level changes and the improvement of connectivity to the downtown core. This presents a bias that deprioritizes the removal of in-stream barriers and the restoration of the natural heritage system. Environmental planning staff are concerned with this bias and the deprioritization of the natural environment.

Environmental planning staff would be happy to meet with you and the consultant team to review and discuss the above comments.

Sincerely,

Leah Lefler

Environmental Planner
Planning and Building Services

City of Guelph

519-822-1260 extension 2362

leah.lefler@guelph.ca

Copy: Jane Gurney, Ryan Hamelin, Karen Reis – Environmental Planning
Reg Russwurm, Steven Di Pietro – Engineering

January 28, 2025

City of Guelph
Infrastructure, Development and Environment, Planning
1 Carden St
Guelph, ON N1H 3A1

Attention: Leah Lefler
Environmental Planner

Dear Ms. Lefler:

Re: Macdonell and Allan's Structures Municipal Class Environmental Assessment
Response to Comments Provided on the Evaluation of Alternative Solutions Technical Memorandum

RVA has received the City's Environmental Planning staff's internal memo dated December 6, 2024, providing comments on the Evaluation of Alternative Solutions Technical Memorandum. We have reviewed your comments and provided the following responses:

1. In Section 3.3 Alternative 3 – Remove Spillway and Sluiceway, it is unclear what the word “significant” means and what significance is based on to evaluate impacts to the hydraulic elevation upstream. The consultant is encouraged to update the Technical Memorandum to provide a clear description of what the assessment of significance is based on in this context. Further, please provide the hydraulic evaluation to environmental planning for review.

Text will be modified to remove the term “significant”, as the hydraulic impacts have not been fully quantified.

As the Allan's Dam Sluiceway/pillway was not modeled as a separate control structure within GRCA's HEC-RAS Model at the time of the preparation of this Memo, the hydraulic function of the dam on the Speed River could not be fully quantified. As such, the exact hydraulic impacts of removing or modifying the structure have not been determined. However, as the dam structure has a large influence on the elevation of the Speed River, upstream and downstream, it is anticipated that removal of the structure would result in impacts to the water level elevation of the Speed River upstream and likely come with significant public scrutiny.

2. Environmental planning staff are not supportive of any alternative that is inconsistent with Official Plan policy. For example, section 3.3.4 Alternative 4 – Remove Spillway and Sluiceway and Build New Dam Upstream with an Active Transportation Underpass includes the creation of a new dam. Official Plan policy provides the direction to remove barriers and restore rivers, not impact them further. Official Plan policy also requires infrastructure projects to demonstrate no negative impact and a net ecological gain to the natural heritage

system.

Your comment has been noted. A statement regarding the OP's direction to remove barriers and not impact rivers further will be incorporated into the evaluation.

3. Please clarify if Alternative 3 – Construct a Simplified Bridge Adjacent to the South Side of the GJR Bridge (Section 3.4.3) would require or necessitate the removal of Allan's Bridge.

Constructing a Simplified Bridge Adjacent to the South Side of the GJR Bridge does not require or necessitate the removal of Allan's Bridge, but it would further reduce the need to maintain the Allan's bridge to accommodate pedestrians and cyclists.

4. In Section 4.0 Summary of Existing Conditions, please confirm if a future conditions assessment was completed to determine impacts to hydraulic conditions. If a future conditions assessment has not been completed, please clarify how impacts to hydraulics has been evaluated following from the removal of the dam.

As the Allans Dam Sluiceway / Spillway was not modelled as a separate control structure within GRCA's HEC-RAS Model at the time of the preparation of this Memo, the hydraulic function of the dam on the Speed River could not be fully quantified. As such, the exact hydraulic impacts of removing or modifying the structure have not been determined. However, as the dam structure has a large influence on the elevation of the Speed River, upstream and downstream, it is anticipated that removal of the structure would result in impacts to the water level elevation of the Speed River upstream. The Macdonell and Allan's Structures Class EA Hydraulic Analysis Technical Memorandum has been enclosed.

5. In Section 4.2 Environmental Impact Study, it appears that the existing conditions memo is based on a desktop review and that a comprehensive field program will be completed to identify impacts and mitigation measures for the preferred alternative. Typically, the field program and data collected during the field program would be used to inform the selection of the preferred alternative. Further, environmental planning staff are typically involved with scoping the Terms of Reference for Environmental Impact Studies prior to their completion.

The Natural Environment Existing Conditions Memo (November 30, 2021) was prepared to document the existing information available through desktop study to identify data gaps prior to field investigations. The field program was proposed in this document (Section 4.7) for discussion/comment/scoping with the City. On June 10, 2022, the City environmental planner (Jason Elliot) provided an internal memo to the City engineer (Steven Di Pietro) commenting on the Nat Env EC Memo (RVA Nov 2021). In that memo he acknowledged that the Nat Env EC Memo provided background review and was also functioning as an EIS Terms of Reference. He noted that the field program must be conducted prior to the selection of the preferred alternative, so that all significant and sensitive features could be considered in the evaluation. On August 11, 2022, field investigations, as noted in the Nat Env EC Memo (TOR), were conducted, except for the tree inventory (which will be conducted at 60% design). On October 3, 2022, a phone conference was held between RVA environmental planning and ecology groups, and the City engineer and environmental planner, to discuss the evaluation of alternatives and necessity of an EIS vs evaluation table or memo. The City

environmental planner (JE) was primarily focused on ensuring that all alternatives were appropriately considered, especially the dam removal option. JE confirmed that the final environmental report did not need to be a formal EIS (which he noted was usually done for development) but instead could be a natural heritage report/memo or table, and that it could be prepared at the detailed design stage, provided that the information needed to evaluate the options was provided before (i.e., during evaluation and preferred selection). The results of the field findings, background information, and general research regarding dam removal impacts, were compiled and referenced in the alternative evaluation tables used to prepare the Evaluation of Alternative Solutions Technical Memorandum.

6. In Section 4.2.4 Designated Natural Areas, please note that the study area also includes fish habitat.

This section will be revised to include fish habitat.

7. The Cultural Heritage Resource Assessment section takes a very Eurocentric view of cultural value. The Evaluation Criteria provided in Table 5.1 includes Cultural Heritage / Archaeological criteria described as follows: Will the alternative affect archaeological, cultural heritage resources or Indigenous communities. In light of the recent Council approved Indigenous Relations Framework, consideration should be given to expanding the assessment to include the significance of the Speed River to Indigenous Peoples.

Wording in the evaluation table will be revised to include "significance of the Speed River to Indigenous Peoples".

8. The Hydraulic Analysis described in Section 4.7 refers to impacts to the water level elevation of the Speed River and significant public scrutiny. Environmental planning staff are concerned with the bias presented in the technical memo and the lack of technical data to support the claims of significant impact. For example, to what extent will the water level elevation be impacted upstream? For how many months of the year? These questions should be evaluated and assessed if this is a criterion being used to influence the outcome of the EA. Further, the text appears to accept that public perspective is limited to the unfavorable impacts to private property and recreation opportunities resulting from reduced water elevation. Public perspectives on restoring river systems and ecological enhancements appear to be entirely lacking.

The level of analysis undertaken was based on information available to the project team at the time of the assessment and was determined by the team to be acceptable for the Schedule B level of detail. Should the City wish, a more detailed Hydraulic Analyses could be undertaken with newer data from NPCA. It should be noted however, that other criteria (e.g. cost, heritage significance, recreational use of the river, etc) factored into the team's recommendation for rehabilitation, as opposed to removal. As such, additional information from a more detailed hydraulic analysis may not result in a change in the City's recommended option. RVA will proceed with a more detailed analysis following the City's approval.

9. In Section 5.4 Allan's Dam Spillway and Sluiceway, please confirm if leaving the spillway and sluiceway as is without investing in repairs or restoration of the structures was considered as an option. If these structures continue to be repaired or restored will there ever be justification for their removal? In other words, does rehabilitating them mean that future EAs will also determine not to remove them. If this is the case, it seems that the only way for in-stream barriers to be removed would be if the structure fails and falls apart.

The Do-nothing option was considered and evaluated against the other options, in accordance with the Municipal Class EA Planning requirements, but not recommended.

10. Environmental planning staff have the following comments on the evaluation of criteria shown in Table 5.4 Evaluation of Alternative Solutions for Allan's Dam Spillway and Sluiceway:

- a. Under alternative 3. Remove Sluiceway and Spillway, the assessment of the impact on the hydraulic function of the river should be reassessed to consider and be transparent about what "significant" is based on. For example, hydraulic function should be assessed in terms of flood hazard, water quality, stream stage/flow, ecological function, etc.

The evaluation tables presented in the Tech Memo were summarized for readability and ease of review, with many details hidden or "rolled up". The detailed, expanded versions of the evaluation tables were provided in **Appendix 3**, which show how each of the options compared against detailed sub-criteria. It should be noted that long term water quality improvements and restored connectivity to habitats were identified as positive impacts under Removal Option (Alternative 3) and ranked higher than the Rehabilitation Option (Alternative 2). Overall, however, when compared against **all** the Criteria/Sub-criteria, the Rehabilitation Option ranked highest.

- b. Under alternative 3. Remove Sluiceway and Spillway, assessment of impacts to property and/or recreational value should be evaluated in greater detail. For example, how much of a difference and for what period of time are water levels impacted? What is the social benefit of removing the sluiceway and spillway? (e.g., increased urban forest canopy, increased water quality, etc.).

The evaluation tables presented in the Tech Memo were summarized for readability and ease of review, with many details hidden or "rolled up". Detailed, expanded versions of the evaluation tables were provided in **Appendix 3**, which show how each of the options compared against detailed sub-criteria. It should be noted that both Connectivity and Ecological Linkages and Form and Function of River criteria both ranked higher in the long-term water quality improvements and restored connectivity to habitats was identified as a positive impact under the Removal Option (Alternative 3) and ranked higher than the Rehabilitation Option (Alternative 2). Overall, however, when compared against **all** the Criteria/Sub-criteria, the Rehabilitation Option ranked higher than the Rehabilitation Option.

- c. Under alternative 3. Remove Sluiceway and Spillway, the evaluation of natural environment and climate change is summarized as follows: "extensive impacts (and

extensive restoration opportunity) to the aquatic and terrestrial environments anticipating from removal of the sluiceway and spillway". Environmental planning staff are unclear on how the scoring has been applied. Extensive restoration opportunity for this option is ranked the same as other alternatives that are assessed as "minor anticipated impacts to the aquatic environment". Additional discussion on this aspect is strongly encouraged.

As noted in our response to 10 a and b, the evaluation tables presented in the Tech Memo were summarized for readability and ease of review, with many details hidden or "rolled up". Detailed, expanded versions of the evaluation tables were provided in [Appendix 3](#), which show how each of the options compared against detailed sub-criteria.

- d. Consideration should be given to evaluating construction impact separately from long-term impact when evaluating environmental impact.

The expanded version of the evaluation table ([Appendix 3](#)) shows that both short and long-term impacts were considered under the Natural Environment & Climate Change Criteria, with each of the criteria receiving the same weighting value.

- 11. The alternative "recommended to be carried forward" is 2. Rehabilitate Sluiceway and Spillway. The cost to implement the alternative to rehabilitate the sluiceway and spillway is \$280K whereas the cost to remove the sluiceway and spillway is \$200K. What is the justification for spending \$80K more on rehabilitating these structures?

The cost of each option was revised to \$415K (Rehabilitation Option) and \$2M (Removal Option) prior to presenting to the public at the second Open House on December 9, 2024.

- 12. In general, the Technical Memorandum appears to prioritize avoidance of water level changes and the improvement of connectivity to the downtown core. This presents a bias that deprioritizes the removal of in-stream barriers and the restoration of the natural heritage system. Environmental planning staff are concerned with this bias and the deprioritization of the natural environment.

The recommendation of the Rehabilitation Option (Alternative 2) over Do Nothing (Alternative 1) and Removal of the Sluiceway and Spillway (Alternatives 3 & 4) was based on a comparative evaluation against **all** key criteria as shown in the evaluation table. Although the comparative ranking numbers are subjective, no weighting factors were applied that could alter the relative importance of the selected criteria.

Yours very truly,

Andrew McGregor, MCIP, RPP
R.V. ANDERSON ASSOCIATES LIMITED

Enclosed: Hydraulic Analysis Technical Memorandum

APPENDIX 12-6-2
Emerge Guelph



Mila Khatri

From: Andrew McGregor
Sent: February 6, 2025 12:18 PM
To: Mila Khatri
Subject: FW: Guelph Downtown Renewal: Allan's Dam Spillway and Sluiceway

Follow Up Flag: Follow up
Flag Status: Flagged

Fyi

Andrew McGregor, MCIP, RPP

Senior Planner / Project Manager, EA & Approvals



R.V. Anderson Associates Limited
43 Church Street, Suite 104, St. Catharines ON L2R 7E1
t 905 685 5049 ext. 4211 | m 905 964 4056

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Susan Hall <shall@lura.ca>
Sent: Thursday, February 6, 2025 9:42 AM
To: Andrew McGregor <AMcGregor@rvanderson.com>; Andrew Miller <Andrew.Miller@guelp.ca>
Subject: Fw: Guelph Downtown Renewal: Allan's Dam Spillway and Sluiceway

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Hi Andrew and Andrew

I saw Evan Ferrari at another City open house yesterday. He mentioned he had sent in some questions about the sluiceway and spillway.

Are you able to get back to him on the questions below?

Thanks Susan

Get [Outlook for iOS](#)

From: Evan Ferrari <evan@emergeguelp.ca>
Sent: Wednesday, February 5, 2025 9:42 PM
To: Susan Hall <shall@lura.ca>
Subject: Fwd: Guelph Downtown Renewal: Allan's Dam Spillway and Sluiceway

Hi Susan,

It was good seeing you again tonight.

Here are the questions that I sent to Andrew and Andrew.

Thanks

Evan

----- Forwarded message -----

From: **Evan Ferrari** <evan@emergeguelph.ca>

Date: Mon, Jan 13, 2025 at 8:14 AM

Subject: Guelph Downtown Renewal: Allan's Dam Spillway and Sluiceway

To: Andrew Miller <andrew.miller@guelph.ca>, Andrew McGregor <AMcGregor@rvanderson.com>

Hi,

I have questions regarding the Allan's Dam Spillway and Sluiceway section of the Downtown Renewal project.

Page 14 of the [information boards](#) provided Evaluation of Allan's Dam Sluiceway and Spillway Alternatives.

Can you provide more information related to these questions?

- 3. Remove Sluiceway and Spillway

Structural / Technical

Significant impact on hydraulic function of the river.

- How would the floodplain, GRCA regulation mapping and City of Guelph planning constraints change with dam and sluiceway removal?
- What strengths and weaknesses would occur to the hydraulic function of the river?

Social Environment

Potential for impacts on property values and enjoyment of property by altering water levels. Major impacts to public recreation uses of the river. Could allow for creation of a cycling underpass.

- What potential impacts would occur on property values and insurance implications if the removal of the dam would reduce the amount of property and buildings in the floodplain and GRCA regulation area?

Natural Environment and Climate Change

Removal will impact the aquatic and terrestrial environments. Provides opportunity for restoring NHS and significant valleylands.

- What impacts will this have on the aquatic habitat given that the top draw nature of the existing dam may be contributing to warmer water temperatures in the impoundment area and downstream?
- What are the microclimate implications of restoring the NHS and significant valleylands vs the status quo?
- How many properties could see positive climate adaptation impacts of the floodplain, GRCA regulation area and City of Guelph planning constraints by removing the dam and sluiceway?

I look forward to hearing from you on this.

Thanks

Evan

--

Evan Ferrari (he/him) | Executive Director
Mobile - 519-836-8068
Office - 519-763-2652

eMERGE
42 Carden Street
Guelph, ON
N1H 3A2

evan@emergeguelph.ca

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--

Evan Ferrari (he/him) | Executive Director
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Office - 519-763-2652

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www.emergeguelph.ca

APPENDIX 12-6-3

Guelph Coalition of Active Transportation



Mila Khatri

From: Steven Di Pietro <Steven.DiPietro@guelph.ca>
Sent: September 7, 2021 9:04 AM
To: Connor MacIsaac
Cc: Andrew McGregor
Subject: FW: Macdonell and Allan structures EA

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FYI – please add this contact to the stakeholder register

From: Mike Darmon <mike_darmon@gcat.ca>
Sent: Tuesday, September 7, 2021 8:44 AM
To: AMcGregor@vanderson.com; Steven Di Pietro <Steven.DiPietro@guelph.ca>
Subject: Macdonell and Allan structures EA

[EXTERNAL EMAIL] Do not click links or attachments unless you recognize the sender and know the content is safe.



Good Morning Steven
Please add me to your the study email list for the Macdonell and Allan structures EA
Thanks
Mike Darmon
President GCAT
519 710 7717

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APPENDIX 12-6-4
Guelph Hiking Club



Mila Khatri

From: Steven Di Pietro <Steven.DiPietro@guelph.ca>
Sent: August 30, 2021 10:50 AM
To: 'John Fisher'
Cc: Andrew McGregor; Connor MacIsaac
Subject: RE: Macdonell and Allan Structures EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi John,

For information on the timing of the GJR Bridge Replacement please connect with Les Petroczi (cc'd).

The construction tender for the Ward to Downtown Bridge project is planned to be released in January 2022 with construction completed by the end of the year.

Regards,

Steven Di Pietro, P. Eng, Project Engineer
Design and Construction, Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2348
Mobile 226-820-0154
steven.dipietro@guelph.ca

guelph.ca

From: John Fisher <jhnfsshr62@gmail.com>
Sent: Monday, August 30, 2021 10:45 AM
To: Steven Di Pietro <Steven.DiPietro@guelph.ca>
Subject: Fwd: Macdonell and Allan Structures EA

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Try this one

----- Forwarded message -----

From: **John Fisher** <jhnfsshr62@gmail.com>

Date: Mon, Aug 30, 2021 at 10:30 AM

Subject: Macdonell and Allan Structures EA

To: <Stephen.dipietro@guelph.ca>, <AMcGregor@rvanderson.com>

Cc: David J Douglas <djdougla@uoguelph.ca>, Mike McBurney <McBurney23@gmail.com>

Hi Stephen,

The scope of the EA appears to be restricted to 3 structures and geographically limited. Could you please advise as to the status of the GJR rail bridge replacement and the addition of the Ward/downtown pedestrian bridge.

What is the timing of these projects?

Thanks

john fisher (President Guelph Hiking Trail Club).

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APPENDIX 12-6-5

Guelph Heritage Committee



Mila Khatri

From: Steven Di Pietro <Steven.DiPietro@guelph.ca>
Sent: November 9, 2021 9:04 AM
To: Connor MacIsaac
Cc: 'pbrianskerrett@gmail.com'
Subject: FW: Macdonell and Allan Structures Municipal Class Environmental Assessment

Categories: Resident Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Hi Connor,

Can you please add Brian to the stakeholder list for the MacDonell Structures EA?

Thanks,

Steven Di Pietro, P. Eng, Project Engineer
Design and Construction, Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2348
Mobile 226-820-0154
steven.dipietro@guelph.ca

guelph.ca

From: pbrianskerrett@gmail.com <pbrianskerrett@gmail.com>
Sent: Tuesday, November 9, 2021 8:35 AM
To: Steven Di Pietro <Steven.DiPietro@guelph.ca>
Subject: Macdonell and Allan Structures Municipal Class Environmental Assessment

[EXTERNAL EMAIL] Do not click links or attachments unless you recognize the sender and know the content is safe.

Hi Steven,

I saw the notice of the EA on the city website, and understand I'll need to keep an eye on the page for the open house dates.

I was just hoping that if you compile an email list for people who wish to get updates on the project, you could add me to that list.

Thanks!

Brian

P. Brian Skerrett

Chair

Heritage Guelph

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APPENDIX 12-6-6

Grand River Conservation Authority



Mila Khatri

From: Ben Kissner <bkissner@grandriver.ca>
Sent: April 6, 2022 2:21 PM
To: Connor MacIsaac
Cc: Andrew McGregor; David OSullivan; Stewart Dickson
Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

Categories: Technical Agency Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Connor,

I had followed up with my colleagues about Speed river modeling and then neglected to pass the information along to you. The speed River updates won't be finalized until the end of this year at best.

For now you will need to go ahead with the information we have on file and if there is an opportunity to include the updates in the future we can advise at that time.

Regards,
Ben

Ben Kissner, M.Sc., MCIP, RPP
Resource Planner
Grand River Conservation Authority

400 Clyde Road, PO Box 729
Cambridge, ON N1R 5W6
Office: 519-621-2763 ext. 2237
Toll-free: 1-866-900-4722
Fax: 519-621-4844

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From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: April 6, 2022 2:16 PM
To: Ben Kissner <bkissner@grandriver.ca>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Stewart Dickson <sdickson@rvanderson.com>
Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

Ben,

Thank you for this information and offering time for a call. Ae will review to determine whether a coordination call is required at this time.

I had also left a message earlier this week regarding the GRCA updating the Speed River model. Is this still an active project for the GRCA? If so, what is the status / timeline for this? It would be great to utilize the updated model during our study if possible.

Thanks,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



From: Ben Kissner <bkissner@grandriver.ca>

Sent: April 6, 2022 1:54 PM

To: Diego Paez <dpaez@rvanderson.com>

Cc: Andrew McGregor <AMcGregor@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Connor MacIsaac <cmacisaac@rvanderson.com>; Stewart Dickson <sdickson@rvanderson.com>

Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good afternoon,

I have received the following from the Engineer I had been working with in regard to this project:

The flows are not constant along the entire reach, as there are a number of spill locations along the way. The flows provided are appropriate for modeling the bridge. I would recommend truncating the model to analyze the impact of the regulated flows on pre and post conditions of the Macdonell Bridge.

However, as mentioned previously, we would also like to see the impact of the pre and post geometry with the unregulated flows included in the HECRAS model.

The Regional storm flow can be modeled unregulated through this reach. I do not have data for the regulated flows in the Regional event.

I am happy to have a phone call with the consultants if required to discuss further.

Let me know if you would like to discuss the matter with her and I will be able to assist in coordinating a meeting.

Regards,
Ben

Ben Kissner, M.Sc., MCIP, RPP
Resource Planner

Grand River Conservation Authority

400 Clyde Road, PO Box 729
Cambridge, ON N1R 5W6
Office: 519-621-2763 ext. 2237
Toll-free: 1-866-900-4722
Fax: 519-621-4844

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From: Diego Paez <dpaez@rvanderson.com>

Sent: March 29, 2022 3:39 PM

To: Ben Kissner <bkissner@grandriver.ca>

Cc: Andrew McGregor <AMcGregor@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Connor MacIsaac <cmacIsaac@rvanderson.com>; Stewart Dickson <sdickson@rvanderson.com>

Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

Hi Ben,

Thank you for the new information. I just have two following up questions:

1. Can we assume that the flows that you have provided are constant along the full length of the Speed River (from the dam to the discharging point at the Eramosa River), and specially, that those are the flows reaching the section at Macdonell St.?
2. Can you also provide the flows for the Regional Storm?

Thanks again for your support.

Regards,
Diego

From: Ben Kissner <bkissner@grandriver.ca>

Sent: March 29, 2022 10:59 AM

To: Diego Paez <dpaez@rvanderson.com>

Cc: Andrew McGregor <AMcGregor@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Connor MacIsaac <cmacIsaac@rvanderson.com>; Stewart Dickson <sdickson@rvanderson.com>

Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

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Good morning,

I have received the following that should help address your request below:

Please see below table of regulated and deregulated flows for the Speed River. I recommend that you run both regulated and deregulated scenarios in their model and confirm that their proposed design does not exacerbate flood conditions in both scenarios.

Flood Frequency Flows (m3/s)

Return Period	1.25	1.5	2	5	10	20	25	50	100	200
Deregulated	44	57	72	107	129	150	157	176	196	215
Regulated	34	40	47	65	76	87	91	102	112	123

And regarding the status of the Speed River modeling, I believe it is still in progress at this time.

Regards,
Ben

Ben Kissner, M.Sc., MCIP, RPP
Resource Planner
Grand River Conservation Authority

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Fax: 519-621-4844

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From: Diego Paez <dpaez@rvanderson.com>
Sent: March 28, 2022 4:42 PM
To: Ben Kissner <bkissner@grandriver.ca>
Cc: Andrew McGregor <AMcGregor@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>; Stewart Dickson <sdickson@rvanderson.com>
Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

Hi Ben,

Thank you for the information provided last week. Based on the discussions at the last meeting and our meeting notes, we are also looking for the following items:

1. The regulated flows for different return periods including the Regional Storm. Right now the model provided includes unregulated flows, which we believe are much higher than the regulated ones. Can you give us an update on the status of this information request?
2. An updated model of the Speed River. We were told it was being completed at the time of the meeting. Has there been any progress with this?

Thanks for your help and time. Please note that I am only supporting Andrew and Connor temporarily, so we would appreciate if you make sure to CC them in your responses.

Regards,
Diego

From: Andrew McGregor <AMcGregor@rvanderson.com>
Sent: March 23, 2022 3:16 PM
To: Diego Paez <dpaez@rvanderson.com>; David OSullivan <dosullivan@rvanderson.com>; Connor MacIsaac <cmaclsaac@rvanderson.com>
Subject: Fwd: Macdonell EA (Guelph) - DTM from GRCA

Fyi, additional info from GRCA.

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From: Ben Kissner <bkissner@grandriver.ca>
Sent: Wednesday, March 23, 2022 2:38:28 PM
To: Andrew McGregor <AMcGregor@rvanderson.com>
Cc: Nick Palomba <NPalomba@rvanderson.com>
Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good afternoon,

I have been provided with some supplemental information pertaining to structures, please see attached.

Regards,
Ben

Ben Kissner, M.Sc., MCIP, RPP
Resource Planner
Grand River Conservation Authority

400 Clyde Road, PO Box 729
Cambridge, ON N1R 5W6
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Toll-free: 1-866-900-4722
Fax: 519-621-4844

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From: Zoë Green <zgreen@grandriver.ca>
Sent: March 22, 2022 6:08 PM
To: Andrew McGregor <AMcGregor@rvanderson.com>
Cc: Ben Kissner <bkissner@grandriver.ca>; Nick Palomba <NPalomba@rvanderson.com>
Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

Hi Andrew, here's the link to download the data bundle (data, metadata, license).

https://data.grandriver.ca/downloads/share/GRCA_DTM_RVAnderson-McdonnellEA.zip

Zoe

From: Andrew McGregor <AMcGregor@rvanderson.com>
Sent: March 22, 2022 2:16 PM
To: Zoë Green <zgreen@grandriver.ca>
Cc: Ben Kissner <bkissner@grandriver.ca>; Nick Palomba <NPalomba@rvanderson.com>
Subject: RE: Macdonell EA (Guelph) - DTM from GRCA

Hi Zoe,

Please find our signed agreement attached.

Kind regards,



Andrew McGregor, MCIP, RPP

SENIOR PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4211 | m 905 964 4056

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



Vacation Alert: Please note that I will be away on vacation March 14 - March 18, 2022.

From: Zoë Green <zgreen@grandriver.ca>
Sent: March 17, 2022 10:45 AM
To: Andrew McGregor <AMcGregor@rvanderson.com>
Cc: Ben Kissner <bkissner@grandriver.ca>
Subject: Macdonell EA (Guelph) - DTM from GRCA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Andrew, we've received your request for a high resolution topo-bathy DTM surface for use with the Macdonell and Allan Structures Municipal Class EA you're working on for the City of Guelph. Please return a signed copy of the attached data license to my attention. Data and documentation to follow.

The data will be provided in NAD83 CSRS v6 (2010), CGVD2013.

Thank you

Zoë Green

Enterprise Geodata Specialist

Grand River Conservation Authority

400 Clyde Road, PO Box 729

Cambridge, ON N1R 5W6

Office: 519-621-2763 ext. 2283

Toll-free: 1-866-900-4722

Email: zgreen@grandriver.ca

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Mila Khatri

From: Ben Kissner <bkissner@grandriver.ca>
Sent: November 17, 2022 3:48 PM
To: Connor MacIsaac
Cc: David Di Pietro; Andrew McGregor
Subject: RE: City of Guelph Downtown Renewal - Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies - Notice of Open House

Categories: Technical Agency Comments

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Good afternoon,

At this time GRCA does not have any objection to the proposed works outlined in the PIC slides. Please continue to circulate this office on the project as it moves forwards so that we can contribute where necessary.

Regards,
Ben

Ben Kissner, M.Sc., MCIP, RPP
Resource Planner
Grand River Conservation Authority

400 Clyde Road, PO Box 729
Cambridge, ON N1R 5W6
Office: 519-621-2763 ext. 2237
Toll-free: 1-866-900-4722
Fax: 519-621-4844

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From: Connor MacIsaac <cmaisaac@rvanderson.com>
Sent: October 13, 2022 10:28 AM
Cc: David Di Pietro <David.DiPietro@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: City of Guelph Downtown Renewal - Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies - Notice of Open House

Dear Sir/Madam,

On behalf of the City of Guelph, you are invited to join us in-person or online for the first Open House for the Macdonell and Allan Structures, and Wyndham Street Municipal Class Environmental Assessment Studies, being undertaken as part of the Downtown Renewal project.

This is a hybrid Open House where you can choose to attend in-person or online. The purpose of the open house is to share introductory material about the Environmental Assessments (EAs), answer your questions, and receive your

feedback. Materials will be available through the City's Have Your Say platform at haveyoursay.guelph.ca/downtown-renewal from October 26 to November 16, 2022. Refer to the attached notice for more detail.

Open House #1:

Date: Wednesday, November 2, 2022

Time: 6:30pm to 8:30pm

In-person Location: Guelph City Hall (1 Carden Street, Guelph ON, N1H 3A1)

Online location: Zoom – visit haveyoursay.guelph.ca/downtownproject for the drop-in meeting link

If you would like to be removed from the project mailing list, please let us know by responding to this email.

Kind Regards,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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APPENDIX 12-6-7

Ministry of Citizenship and Multiculturalism



September 9, 2021

EMAIL ONLY

Andrew McGregor, MCIP, RPP
Senior Planner, EA and Approvals
R.V. Anderson Associates Limited
AMcGregor@rvanderson.com

MHSTCI File : 0014949
Proponent : City of Guelph
Subject : Notice of Study Commencement – Schedule C – Municipal Class EA
Project : Macdonell and Allan Structures Municipal Class Environmental
Assessment
Location : City of Guelph

Dear Andrew McGregor:

Thank you for providing the Ministry of Heritage, Sport, Tourism and Cultural Industries (MHSTCI) with the Notice of Study Commencement for the above-referenced project. MHSTCI's interest in this environmental assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage, which includes:

- archaeological resources (including land and marine)
- built heritage resources (including bridges and monuments)
- cultural heritage landscapes

Project Summary

In response, and as part of the broader Downtown Infrastructure Revitalization Program, the City of Guelph has initiated a Municipal Class EA (Class EA) for improvements and modifications to the Macdonell and Allan structures. The project is being completed as a Schedule "C" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 and 2015).

Identifying Cultural Heritage Resources

While some cultural heritage resources may have already been formally identified, others may be identified through screening and evaluation. Indigenous communities may have knowledge that can contribute to the identification of cultural heritage resources, and we suggest that any engagement with Indigenous communities includes a discussion about known or potential cultural heritage resources that are of value to these communities. Municipal Heritage Committees, historical societies and other local heritage organizations may also have knowledge that contributes to the identification of cultural heritage resources.

Municipal Heritage Bridges: Cultural, Heritage & Archaeological Resources Assessment Checklist

Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources. The Municipal Engineers Association provides screening criteria for work on bridges that falls under the Municipal Class EA with a [checklist](#) and [background material](#) available online, developed in coordination with MHSTCI.

Part A – Municipal Class EA Activity Selection

Please use the [checklist](#) and [background material](#) to determine the Municipal Class EA schedule (A, A+, B or C) for the project. Completing the remainder of this checklist determines what technical cultural heritage studies may be required.

Part B - Cultural Heritage Assessment

If Part B of the checklist determines that the bridge or study area warrants the preparation of a Cultural Heritage Evaluation Report (CHER), and the undertaking of a Heritage Impact Assessment (HIA), our ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. CHERs and HIAs are to be prepared by qualified consultants. Please send HIAs to MHSTCI for review and make copies available to local organizations or individuals who have expressed an interest in cultural heritage.

Part C – Heritage Assessment

If Part C of the checklist determines that the CHER has identified heritage features on the project and recommends that a Heritage Impact Assessment (HIA) be undertaken, our Ministry's [Info Sheet #5: Heritage Impact Assessments and Conservation Plans](#) outlines the scope of HIAs. CHERs and HIAs are to be prepared by qualified consultants. Please send HIAs to MHSTCI for review and make copies available to local organizations or individuals who have expressed an interest in cultural heritage.

Part D – Archaeological Resources Assessment

If Part D of the checklist establishes that an archaeological assessment is required, it is to be conducted by an archaeologist licenced under the *Ontario Heritage Act (OHA)*, who is responsible for submitting the report directly to MHSTCI for review. MHSTCI archaeological sites data are available at archaeology@ontario.ca.

After completing the checklist, please update MHSTCI on the project Class EA schedule and whether any technical cultural heritage studies will be completed for the project. Please provide all technical heritage studies to MHSTCI before issuing a Notice of Completion or commencing any of work on site.

Environmental Assessment Reporting

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. If the screening has identified no known or potential cultural heritage resources, or no impacts to these resources, please include the completed checklists and supporting documentation in the EA report or file.

Thank you for consulting MHSTCI on this project. Please continue to do so through the EA process, and contact me for any questions or clarification.

Sincerely,

Joseph Harvey
Heritage Planner
Heritage Planning Unit
joseph.harvey@Ontario.ca

Copied to: Steven Di Pietro, Engineering and Transportation Services, City of Guelph
Connor MacIsaac, Junior Environmental Planner, R.V. Anderson Associates Limited

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. MHSTCI makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MHSTCI be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Please notify MHSTCI if archaeological resources are impacted by EA project work. All activities impacting archaeological resources must cease immediately, and a licensed archaeologist is required to carry out an archaeological assessment in accordance with the Ontario Heritage Act and the Standards and Guidelines for Consultant Archaeologists.

If human remains are encountered, all activities must cease immediately and the local police as well as the Registrar, Burials of the Ministry of Government and Consumer Services (416-326-8800) must be contacted. In situations where human remains are associated with archaeological resources, MHSTCI should also be notified to ensure that the site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

Ministry of Tourism, Culture and Sport

Programs and Services Branch
400 University Ave, 5th Flr
Toronto, ON M7A 2R9
Tel: 613.242.3743

Ministère du Tourisme, Culture et Sport

Direction des programmes et des services
400, av. University, 5e étage
Toronto, ON M7A 2R9
Tél: 613.242.3743



September 20, 2022

EMAIL ONLY

Connor MacIsaac.
Junior Environmental Planner
R.V. Anderson Associates Limited
43 Church Street, Suite 104,
St. Catharines, ON L2R 7E1
cmacIsaac@rvanderson.com

MTCS File : 0014949
Proponent : City of Guelph
Subject : Cultural Heritage Evaluation Report
Project : Macdonell and Allan Structures Municipal Class Environmental Assessment
Location : City of Guelph

Dear Connor MacIsaac:

Thank you for providing the Ministry of Tourism, Culture and Sport (MTCS) with the draft Cultural Heritage Evaluation Report (CHER) for the above-referenced project for our review and comment.

MTCS's interest in this Environmental Assessment (EA) project relates to its mandate of conserving Ontario's cultural heritage, which includes archaeological resources, built heritage resources and cultural heritage landscapes. Under the EA process, the proponent is required to determine a project's potential impact on cultural heritage resources.

Project Summary

In response, and as part of the broader Downtown Infrastructure Revitalization Program, the City of Guelph has initiated a Municipal Class EA (Class EA) for improvements and modifications to the Macdonell and Allan structures. The project is being completed as a Schedule "C" project in accordance with the Municipal Class Environmental Assessment (October 2000, amended in 2007, 2011 and 2015).

Project Comments

We have reviewed the CHER, dated March 2022, and prepared by ASI Archaeological and Cultural Heritage Services, and find that it is overall consistent with the requirements, guidance and standards of the MCEA and with best practice guidance prepared by MTCS.

The Macdonell Bridge does not retain cultural heritage value, therefore a Heritage Impact Assessment is not required for the structure. The Allan Bridge and Allan Spillway were found to be of cultural heritage value or interest. Therefore, a HIA shall be undertaken by a qualified person. MTCS recommends that the HIA be prepared as a part of preliminary design prior to issuing a notice of completion.

However, we do have the following comments and observations to support documentation around cultural heritage due diligence.

Section 1.2 (Legislation and Policy Context)

This section should include a reference to Ontario Regulation 160/02 (Standards for Bridges) and the Ontario Heritage Bridge Guidelines: For Provincial Owned Bridges (MTO 2008). A copy of the completed Municipal Class EA's associated checklist for municipal bridges ([Municipal Heritage Bridges Cultural, Heritage and Archaeological Resources Assessment Checklist Revised April 11, 2014](#)) should be appended to the final draft of the CHER.

Please advise whether archaeological assessment(s) have been or will be undertaken for this project. If so, please provided us with the Project Information Form (PIF) Numbers.

Environmental Assessment Reporting

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects.

Thank you for consulting MTCS on this project. We look forward to receiving the final copy of the CHER for our records and reviewing the draft HIAs. If you have any questions or require clarification, please do not hesitate to contact me.

Sincerely,

Joseph Harvey
Heritage Planner
joseph.harvey@Ontario.ca

Copied to: Steven Di Pietro, Engineering and Transportation Services, City of Guelph
Andrew McGregor, Senior Planner, EA and Approvals, R.V. Anderson Associates Limited

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. Ministry of Tourism, Culture and Sport (MTCS) makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MTCS be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Should previously undocumented archaeological resources be discovered, they may be a new archaeological site and therefore subject to Section 48(1) of the *Ontario Heritage Act*. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out an archaeological assessment, in compliance with Section 48(1) of the *Ontario Heritage Act*.

The Funeral, Burial and Cremation Services Act, 2002, S.O. 2002, c.33 requires that any person discovering human remains must cease all activities immediately and notify the police or coroner. If the coroner does not suspect foul play in the disposition of the remains, in accordance with Ontario Regulation 30/11 the coroner shall notify the Registrar, Ontario Ministry of Public and Business Service Delivery, which administers provisions of that Act related to burial sites. In situations where human remains are associated with archaeological resources, MTCS should also be notified (at archaeology@ontario.ca) to ensure that the archaeological site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

From: [Harvey, Joseph \(MCM\)](#)
Sent on: October 26, 2022 3:02:48 PM
To: [Connor MacIsaac](#)
CC: [Steven.DiPietro@guelph.ca](#); [Andrew McGregor](#); [Del Villar Cuicas, Joan \(MECP\)](#)
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

Categories: Technical Agency Comments

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Hi Connor,

Thanks for providing us with these updates.

Please note that the responsibility for administration of the *Ontario Heritage Act* and matters related to cultural heritage has been recently transferred from the Ministry of Tourism, Culture and Sport to the Ministry of Citizenship and Multiculturalism (MCM). Individual staff roles and contact information remain unchanged. Please continue to send any notices, reports and/or documentation to both Karla Barboza and Joseph Harvey.

We have the following comments with regards to the documentation of cultural heritage due diligence:

Archaeological Resources

Thank you for providing us with the PIF number for the Stage 1 archaeological assessment (P383-0297-2021). Please note that archaeological concerns have not been addressed until reports have been entered into the Ontario Public Register of Archaeological Reports where those reports recommend that:

- the archaeological assessment of the project area is complete and
- all archaeological sites identified by the assessment are either of no further cultural heritage value or interest (as per Section 48(3) of the Ontario Heritage Act) or that mitigation of impacts has been accomplished through an avoidance and protection strategy.

Built Heritage Resources and Cultural Heritage Landscapes

MCM continues to recommend that an HIA be completed during the planning phase of the EA.

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. An HIA will determine if any built heritage resources and/or cultural heritage landscapes (including those previously identified and those found as part of the site assessment) are impacted by a specific proposed development or site alteration. It can also demonstrate how those resources will be conserved in the context of redevelopment or site alteration. Mitigative or avoidance measures or alternative development or site alteration approaches may be recommended. The HIA will also inform the selection of the preferred alternative.

Please do not hesitate to contact me with any questions or concerns.

Thanks,

Joseph Harvey | Heritage Planner

Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit
Ministry of Citizenship and Multiculturalism
613.242.3743

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: October 14, 2022 4:24 PM
To: Harvey, Joseph (MTCS) <Joseph.Harvey@ontario.ca>
Cc: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>; Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hi Joseph,


Our cultural heritage consultant ASI has provided us with the following advice with regards to completing the HIA, prior to issuing the Class EA Notice of Completion, during the functional / preliminary design stage as recommended by the MTCS....

We would advise that preparing the HIA for the bridge and spillway during preliminary design would be much too early, because there would not be enough details about the undertaking and precisely how the heritage resources may be impacted. It should also be noted that we will be updating the CH report which will include a high level review of potential direct/indirect impacts to these heritage resources and make recommendations, which of course will be to avoid negative impacts to the bridge and spillway and where that is not possible, to complete an HIA early in detailed design to help inform the process.

We understand the importance of ascertaining enough cultural heritage information at the time of the EA to be able to evaluate the alternatives against each other accurately, and develop mitigation measures (which will include the preparation of an HIA as required). The scores assigned to the cultural heritage section will be defensible with the ability to refer to information collected during the CHER.

As requested, an update on the archaeological assessment for this undertaking is as follows: ASI has completed a draft Stage 1 Archaeological Assessment Report, which is currently under review by City of Guelph staff. It is expected that the draft report be provided to relevant Indigenous communities, prior to being submitted directly to the MTCS by the archaeologist (ASI) for review as required. The PIF for this undertaking is: P383-0297-2021

Best,

 **Connor MacIsaac, ENV SP, EPT**
ENVIRONMENTAL PLANNER, EA & APPROVALS
t 905 685 5049 ext. 4218
a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Harvey, Joseph (MTCS) <Joseph.Harvey@ontario.ca>
Sent: September 20, 2022 1:57 PM
To: Connor MacIsaac <cmacisaac@rvanderson.com>
Cc: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>; Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Connor MacIsaac,

Please find attached MTCS comments on the Cultural Heritage Evaluation Report prepared in support of the above referenced undertaking. Please do not hesitate to contact me with any questions or concerns.

Regards,


Joseph Harvey | Heritage Planner
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
Ministry of Tourism, Culture, and Sport
613.242.3743
Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmaclsaac@rvanderson.com>
Sent: August 15, 2022 2:54 PM
To: Harvey, Joseph (MTCS) <Joseph.Harvey@ontario.ca>
Cc: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>; Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>; Laura Wickett <lwickett@asiheritage.ca>
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218
a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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Mila Khatri

From: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>
Sent: November 4, 2022 9:43 AM
To: Connor MacIsaac
Cc: Barboza, Karla (MCM)
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

Categories: Technical Agency Comments

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Good morning Connor,

Thanks for the invitation. Please ensure my team lead Karla Barboza (see cc'd) is included in the meeting invitation. We should be available to meet Monday or Tuesday (9:30 to noon or 3 to 4pm) or Thursday (2 to 4pm).

Thanks,

Joseph Harvey | Heritage Planner
Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit
Ministry of Citizenship and Multiculturalism
613.242.3743
Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmaclsaac@rvanderson.com>
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To: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>
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Based on your comments we believe that we are both on the same page that cultural heritage information needs to be considered in the evaluation of alternatives, and that as part of the EA, the project team should develop mitigation or avoidance measures. We are hoping that a brief meeting will clarify our proposed approach to meet the Ministry's goals.

If you could provide a few times in the next couple weeks that would work on your end to chat, that would be great.

Best,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

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a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>

Sent: October 26, 2022 3:03 PM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>; Del Villar Cuicas, Joan (MECP) <Joan.DelVillarCuicas@ontario.ca>

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We have the following comments with regards to the documentation of cultural heritage due diligence:

Archaeological Resources

Thank you for providing us with the PIF number for the Stage 1 archaeological assessment (P383-0297-2021). Please note that archaeological concerns have not been addressed until reports have been entered into the Ontario Public Register of Archaeological Reports where those reports recommend that:

- the archaeological assessment of the project area is complete and
- all archaeological sites identified by the assessment are either of no further cultural heritage value or interest (as per Section 48(3) of the Ontario Heritage Act) or that mitigation of impacts has been accomplished through an avoidance and protection strategy.

Built Heritage Resources and Cultural Heritage Landscapes

MCM continues to recommend that an HIA be completed during the planning phase of the EA.

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. An HIA will determine if any built heritage resources and/or cultural heritage landscapes (including

those previously identified and those found as part of the site assessment) are impacted by a specific proposed development or site alteration. It can also demonstrate how those resources will be conserved in the context of redevelopment or site alteration. Mitigative or avoidance measures or alternative development or site alteration approaches may be recommended. The HIA will also inform the selection of the preferred alternative.

Please do not hesitate to contact me with any questions or concerns.

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Hi Joseph,

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We would advise that preparing the HIA for the bridge and spillway during preliminary design would be much too early, because there would not be enough details about the undertaking and precisely how the heritage resources may be impacted. It should also be noted that we will be updating the CH report which will include a high level review of potential direct/indirect impacts to these heritage resources and make recommendations, which of course will be to avoid negative impacts to the bridge and spillway and where that is not possible, to complete an HIA early in detailed design to help inform the process.

We understand the importance of ascertaining enough cultural heritage information at the time of the EA to be able to evaluate the alternatives against each other accurately, and develop mitigation measures (which will include the preparation of an HIA as required). The scores assigned to the cultural heritage section will be defensible with the ability to refer to information collected during the CHER.

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Connor MacIsaac,

Please find attached MTCS comments on the Cultural Heritage Evaluation Report prepared in support of the above referenced undertaking. Please do not hesitate to contact me with any questions or concerns.

Regards,

Joseph Harvey | Heritage Planner

Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit

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Cc: Steven.DiPietro@guelph.ca; Barboza, Karla (MCM); Andrew McGregor; Del Villar Cuicas, Joan (MECP); Laura Wickett; Igraves@asiheritage.ca
Subject: FW: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

Categories: Technical Agency Comments

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Hi Connor et al.,

Thanks for setting up a meeting with us this morning. We found it very helpful.

Further to our discussion, this is our understanding of the proposed approach:

- A Cultural Heritage Evaluation Report (CHER) has been completed as part of the Macdonell and Allan Structures Municipal Class Environmental Assessment. The CHER determined that the Allan Bridge and Allan Spillway are of cultural heritage value or interest (CHVI). ASI will make some revisions based on feedback as appropriate.
- A Cultural Heritage Assessment Report (CHRA), which is also known as a “Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment”, is being undertaken as part of the City of Guelph Downtown Renewal Project.
 - This report will include a preliminary impact assessment section which will address potential impacts to the Allan Bridge and Allan Spillway, among other properties identified through the study.
 - The discussion of impacts for these structures will reflect the eight conservation options provided in Section 4.3 of the [Ontario Heritage Bridge Guidelines](#) (OHBG) – which ranks alternatives from minimum to maximum intervention as most to least preferred. The demolition or removal of a bridge should be considered a last resort after all other alternatives have been considered.
 - The CHRA will be completed before the notice of completion for the Macdonell and Allan Structures Municipal Class Environmental Assessment and the CHRA’s preliminary impact assessment will inform the selection of a preferred alternative for the Macdonell and Allan Structures Municipal Class Environmental Assessment.
- A Heritage Impact Assessment (HIA) for Allan Bridge and Allan Spillway will be completed during the detailed design phase of the Macdonell and Allan Structures Municipal Class Environmental Assessment.

Please let us know if approach described above aligns with your recommendation/understanding. Please feel free to provide any additional information as appropriate.

Thanks,

Joseph Harvey | Heritage Planner

Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit

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Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

Categories: Technical Agency Comments

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Hi Connor et al.,

Thank you for your prompt reply.

We agree that the conservation options provided in the OHGB do not need to be applied to the Allan Spillway, however given the Allan Spillway's close proximity to the Allan Bridge we recommend that best practices and conservation mitigation measures are applied.

Thanks,

Joseph Harvey | Heritage Planner
Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit
Ministry of Citizenship and Multiculturalism
613.242.3743
Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmaisaac@rvanderson.com>
Sent: November 9, 2022 9:44 AM
To: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>
Cc: Steven.DiPietro@guelph.ca; Barboza, Karla (MCM) <Karla.Barboza@ontario.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Del Villar Cuicas, Joan (MECP) <Joan.DelVillarCuicas@ontario.ca>; Laura Wickett <lwickett@asiheritage.ca>; lgraves@asiheritage.ca
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hi Joseph,

Thank you to yourself and Karla for the meeting and thank you also for sending this detailed summary with outcomes from our discussion, it is equally as helpful.

Our team has reviewed, and everything looks good to us, there's just one point we've added in red below.

Best,



Connor MacIsaac, ENV SP, EPt

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>

Sent: November 8, 2022 2:49 PM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Steven.DiPietro@guelph.ca; Barboza, Karla (MCM) <Karla.Barboza@ontario.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Del Villar Cuicas, Joan (MECP) <Joan.DelVillarCuicas@ontario.ca>; Laura Wickett <lwickett@asiheritage.ca>; lgraves@asiheritage.ca

Subject: FW: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hi Connor et al.,

Thanks for setting up a meeting with us this morning. We found it very helpful.

Further to our discussion, this is our understanding of the proposed approach:

- A Cultural Heritage Evaluation Report (CHER) has been completed as part of the Macdonell and Allan Structures Municipal Class Environmental Assessment. The CHER determined that the Allan Bridge and Allan Spillway are of cultural heritage value or interest (CHVI). ASI will make some revisions based on feedback as appropriate.
- A Cultural Heritage Assessment Report (CHRA), which is also known as a “Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment”, is being undertaken as part of the City of Guelph Downtown Renewal Project.
 - This report will include a preliminary impact assessment section which will address potential impacts to the Allan Bridge and Allan Spillway, among other properties identified through the study.
 - The discussion of impacts for these structures will reflect the eight conservation options provided in Section 4.3 of the [Ontario Heritage Bridge Guidelines](#) (OHBG) – which ranks alternatives from minimum to maximum intervention as most to least preferred. The demolition or removal of a bridge should be considered a last resort after all other alternatives have been considered. **This will be conducted for the Allan Bridge, however it won't be practical to apply bridge conservation options to the spillway, because it is not a bridge structure and the conservation options in the OHBG are quite specific to bridges.**
 - The CHRA will be completed before the notice of completion for the Macdonell and Allan Structures Municipal Class Environmental Assessment and the CHRA's preliminary impact assessment will inform the selection of a preferred alternative for the Macdonell and Allan Structures Municipal Class Environmental Assessment.

- A Heritage Impact Assessment (HIA) for Allan Bridge and Allan Spillway will be completed during the detailed design phase of the Macdonell and Allan Structures Municipal Class Environmental Assessment.

Please let us know if approach described above aligns with your recommendation/understanding. Please feel free to provide any additional information as appropriate.

Thanks,

Joseph Harvey | Heritage Planner
Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit
Ministry of Citizenship and Multiculturalism
613.242.3743
Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: November 3, 2022 3:37 PM
To: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>
Cc: Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>; Del Villar Cuicas, Joan (MECP) <Joan.DelVillarCuicas@ontario.ca>; Laura Wickett <lwickett@asiheritage.ca>; Lindsay Graves <lgraves@asiheritage.ca>
Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hi Joseph,

Thank you for your comments. We are hoping to setup a meeting to discuss the Ministry's comments with yourself and the City of Guelph Project Manager, RVA, and ASI cultural heritage specialists.

Based on your comments we believe that we are both on the same page that cultural heritage information needs to be considered in the evaluation of alternatives, and that as part of the EA, the project team should develop mitigation or avoidance measures. We are hoping that a brief meeting will clarify our proposed approach to meet the Ministry's goals.

If you could provide a few times in the next couple weeks that would work on your end to chat, that would be great.

Best,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Harvey, Joseph (MCM) <Joseph.Harvey@ontario.ca>

Sent: October 26, 2022 3:03 PM

To: Connor MacIsaac <cmacisaac@rvanderson.com>

Cc: Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>; Del Villar Cuicas, Joan (MECP)

<Joan.DelVillarCuicas@ontario.ca>

Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hi Connor,

Thanks for providing us with these updates.

Please note that the responsibility for administration of the *Ontario Heritage Act* and matters related to cultural heritage has been recently transferred from the Ministry of Tourism, Culture and Sport to the Ministry of Citizenship and Multiculturalism (MCM). Individual staff roles and contact information remain unchanged. Please continue to send any notices, reports and/or documentation to both Karla Barboza and Joseph Harvey.

We have the following comments with regards to the documentation of cultural heritage due diligence:

Archaeological Resources

Thank you for providing us with the PIF number for the Stage 1 archaeological assessment (P383-0297-2021). Please note that archaeological concerns have not been addressed until reports have been entered into the Ontario Public Register of Archaeological Reports where those reports recommend that:

- the archaeological assessment of the project area is complete and
- all archaeological sites identified by the assessment are either of no further cultural heritage value or interest (as per Section 48(3) of the Ontario Heritage Act) or that mitigation of impacts has been accomplished through an avoidance and protection strategy.

Built Heritage Resources and Cultural Heritage Landscapes

MCM continues to recommend that an HIA be completed during the planning phase of the EA.

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects. An HIA will determine if any built heritage resources and/or cultural heritage landscapes (including those previously identified and those found as part of the site assessment) are impacted by a specific proposed development or site alteration. It can also demonstrate how those resources will be conserved in the context of redevelopment or site alteration. Mitigative or avoidance measures or alternative development or site alteration approaches may be recommended. The HIA will also inform the selection of the preferred alternative.

Please do not hesitate to contact me with any questions or concerns.

Thanks,

Joseph Harvey | Heritage Planner
Inclusion and Heritage Division | Heritage Branch | Heritage Planning Unit
Ministry of Citizenship and Multiculturalism
613.242.3743
Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: October 14, 2022 4:24 PM

To: Harvey, Joseph (MTCS) <Joseph.Harvey@ontario.ca>

Cc: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>; Steven.DiPietro@guelph.ca; Andrew McGregor
<AMcGregor@rvanderson.com>

Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hi Joseph,

Our cultural heritage consultant ASI has provided us with the following advice with regards to completing the HIA, prior to issuing the Class EA Notice of Completion, during the functional / preliminary design stage as recommended by the MTCS....

We would advise that preparing the HIA for the bridge and spillway during preliminary design would be much too early, because there would not be enough details about the undertaking and precisely how the heritage resources may be impacted. It should also be noted that we will be updating the CH report which will include a high level review of potential direct/indirect impacts to these heritage resources and make recommendations, which of course will be to avoid negative impacts to the bridge and spillway and where that is not possible, to complete an HIA early in detailed design to help inform the process.

We understand the importance of ascertaining enough cultural heritage information at the time of the EA to be able to evaluate the alternatives against each other accurately, and develop mitigation measures (which will include the preparation of an HIA as required). The scores assigned to the cultural heritage section will be defensible with the ability to refer to information collected during the CHER.

As requested, an update on the archaeological assessment for this undertaking is as follows: ASI has completed a draft Stage 1 Archaeological Assessment Report, which is currently under review by City of Guelph staff. It is expected that the draft report be provided to relevant Indigenous communities, prior to being submitted directly to the MTCS by the archaeologist (ASI) for review as required. The PIF for this undertaking is: P383-0297-2021

Best,



Connor MacIsaac, ENV SP, EPT

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



From: Harvey, Joseph (MTCS) <Joseph.Harvey@ontario.ca>

Sent: September 20, 2022 1:57 PM

To: Connor MacIsaac <cmaclsaac@rvanderson.com>

Cc: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>; Steven.DiPietro@guelph.ca; Andrew McGregor
<AMcGregor@rvanderson.com>

Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Connor MacIsaac,

Please find attached MTCS comments on the Cultural Heritage Evaluation Report prepared in support of the above referenced undertaking. Please do not hesitate to contact me with any questions or concerns.

Regards,

Joseph Harvey | Heritage Planner

Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit

Ministry of Tourism, Culture, and Sport

613.242.3743

Joseph.Harvey@ontario.ca

From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: August 15, 2022 2:54 PM

To: Harvey, Joseph (MTCS) <Joseph.Harvey@ontario.ca>


Cc: Barboza, Karla (MTCS) <Karla.Barboza@ontario.ca>; Steven.DiPietro@guelph.ca; Andrew McGregor <AMcGregor@rvanderson.com>; Laura Wickett <lwickett@asiheritage.ca>

Subject: RE: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Hello Joseph,

Thank you for your response. As part of this study, a Stage 1 Archaeological Assessment is being undertaken for the study area. The Stage 1 Archaeological Report will be submitted directly to the MHSTCI by the archaeologist (ASI) for review as required. Furthermore, based on completion of the MHSTCI checklist a Cultural Heritage Resource Assessment and subsequently, a Cultural Heritage Evaluation Report are being completed by ASI for the study area / study structures.

As requested, please use this  [link](#) to access the draft Cultural Heritage Reports completed for the study, for your review and comment. The link is specific to you, however, you can download and share the documents internally. Alternatively, please let me know if any other staff members require access, and I can provide them access to the link. The report has been reviewed by the City of Guelph's Heritage Planner and will be included in the filing of the Environmental Study Report.

To give you a brief overview, the draft CHRA includes the broader Downtown Guelph Project study area and is limited to a desktop review (to be updated with field work and an impact assessment once a preferred design has been determined), while the CHER is specific to the Macdonell and Allan Bridge Class EA study area. In short, the CHER found that the Macdonell Bridge does not retain cultural heritage value, and that both the Allan Bridge and Allan Spillway were found to retain cultural heritage value. Because the CHER has found that the Macdonell bridge has no heritage value, there would be no specific recommendations pertaining to the bridge itself. Heritage Impact Assessments, identifying specific mitigation measures for the Allan Bridge and Allan Spillway should be undertaken during detailed design, once all direct and indirect impacts are confirmed.

Thanks,



Connor MacIsaac, ENV SP, EPt

ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

SUMMER HOURS: RVA celebrates the summer season from June 3rd to September 2nd. Our offices will be closed each Friday. We will remain available to address any urgent or project related matters.

From: Harvey, Joseph (MHSTCI) <Joseph.Harvey@ontario.ca>
Sent: September 9, 2021 12:24 PM
To: Andrew McGregor <AMcGregor@rvanderson.com>
Cc: Barboza, Karla (MHSTCI) <Karla.Barboza@ontario.ca>; Connor MacIsaac <cmaclsaac@rvanderson.com>;
Steven.DiPietro@guelph.ca
Subject: File 0014949: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment

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Andrew McGregor,

Please find attached MHSTCI's initial advice on the above referenced undertaking. Do not hesitate to contact me with any questions or concerns.

Regards,

Joseph Harvey | Heritage Planner (A)
Heritage, Tourism and Culture Division | Programs and Services Branch | Heritage Planning Unit
Ministry of Heritage, Sport, Tourism and Culture Industries
613.242.3743
Joseph.Harvey@ontario.ca

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**Ministry of Citizenship
and Multiculturalism**

Heritage Planning Unit
Heritage Branch
Citizenship, Inclusion and
Heritage Division
5th Flr, 400 University Ave
Tel.: 437 240 2379

**Ministère des Affaires civiques
et du Multiculturalisme**

Unité de la planification relative au
patrimoine
Direction du patrimoine
Division des affaires civiques, de
l'inclusion et du patrimoine
Tél.: 437 240 2379



April 23, 2025

EMAIL ONLY

Mila Khatri
Environmental Planner, Transportation
R.V. Anderson Associates Limited
400 Palladium Way, Suite 200
Burlington, ON L7M 0W7
mkhatri@rvanderson.com

MCM File : 0014512
Proponent : City of Guelph
Subject : Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment
Project : Capital Implementation Plan - Downtown Infrastructure Revitalization Program
Location : City of Guelph

Dear Mila Khatri:

Thank you for providing the Ministry of Citizenship and Multiculturalism (MCM) with the Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment for the City of Guelph Downtown Infrastructure Revitalization Program for our review and comment.

MCM's interest in this project relates to its mandate of conserving Ontario's cultural heritage which includes archaeological resources, built heritage resources and cultural heritage landscapes.

Under an environmental assessment process, the proponent is required to determine a project's potential impact on cultural heritage resources.

The Cultural Heritage Report: Existing Conditions and Preliminary Impact Assessment (dated June 2021 (updated March 2022 and March 2025) and prepared by Archaeological Services Inc.) was completed as part of the Downtown Infrastructure Revitalization Program. The Downtown Infrastructure Revitalization Program includes three components: the Capital Implementation Plan, Wyndham Street Municipal Class Environmental Assessment (Schedule A+), and Macdonnell and Allan Structures Municipal Class EA.

The purpose of the Cultural Heritage Report is to present an inventory of known and potential built heritage resources (BHRs) and cultural heritage landscapes (CHLs), identify existing

conditions of the study area, provide a preliminary impact assessment, and propose appropriate mitigation measures for minimizing and avoiding negative impacts.

The Cultural Heritage Report identified 259 known and potential BHRs and CHLs and 7 commemorative features in the Guelph Downtown Infrastructure Revitalization study area and made several recommendations – general and specific for each component of the program.

On September 22, 2022, MCM provided initial comments on for the draft Cultural Heritage Report: Desktop Results (dated June 2021 and prepared by ASI) for the Wyndham Street MCEA (one of the components of the for the City of Guelph Downtown Infrastructure Revitalization Program) prepared by ASI. It is our understanding that the 2025 Cultural Heritage Report superseded the 2022 report. MCM acknowledges that the previous comments provided as part of the draft have been mostly addressed by the submitted CHR report and only a few minor revisions are still required.

We have reviewed the above referenced Cultural Heritage Report and find that the report overall is consistent with the requirements, guidance and standards of the Municipal Class EA and with best practice guidance prepared by MCM.

However, we recommend that that the term ‘adjacent’ included in the Glossary be revised to align with the definition from the Provincial Planning Statement, 2024 which came into effect October 20, 2024:

- **Adjacent** – The first paragraph should be revised to align with the PPS 2024. Keep the definition from the City’s Official Plan.

All technical cultural heritage studies and their recommendations are to be addressed and incorporated into EA projects.

Thank you for consulting MCM on this project and please continue to do so throughout the EA process. If you have any questions or require clarification, please do not hesitate to contact me.

Sincerely,

Anastasia Abrazhevich
Heritage Planner
anastasia.abrazhevich@ontario.ca

Copied to: Steven Di Pietro, Engineering and Transportation Services, City of Guelph
Andrew McGregor, Senior Planner, EA and Approvals, R.V. Anderson Associates Limited
Karla Barboza, Team Lead – Heritage Planning Unit, MCM

It is the sole responsibility of proponents to ensure that any information and documentation submitted as part of their EA report or file is accurate. The Ministry of Citizenship and Multiculturalism (MCM) makes no representation or warranty as to the completeness, accuracy or quality of the any checklists, reports or supporting documentation submitted as part of the EA process, and in no way shall MCM be liable for any harm, damages, costs, expenses, losses, claims or actions that may result if any checklists, reports or supporting documents are discovered to be inaccurate, incomplete, misleading or fraudulent.

Should previously undocumented archaeological resources be discovered, they may be a new archaeological site and therefore subject to Section 48(1) of the *Ontario Heritage Act*. The proponent or person discovering the archaeological resources must cease alteration of the site immediately and engage a licensed consultant archaeologist to carry out an archaeological assessment, in compliance with Section 48(1) of the *Ontario Heritage Act*.

The *Funeral, Burial and Cremation Services Act, 2002, S.O. 2002, c.33* requires that any person discovering human remains must cease all activities immediately and notify the police or coroner. If the coroner does not suspect foul play in the disposition of the remains, in accordance with *Ontario Regulation 30/11* the coroner shall notify the Registrar, Ontario Ministry of Public and Business Service Delivery and Procurement, which administers provisions of that Act related to burial sites. In situations where human remains are associated with archaeological resources, the Ministry of Citizenship and Multiculturalism should also be notified (at archaeology@ontario.ca) to ensure that the archaeological site is not subject to unlicensed alterations which would be a contravention of the *Ontario Heritage Act*.

APPENDIX 12-6-8

Ministry of the Environment, Conservation, and
Parks



Ministry of the Environment,
Conservation and Parks

Environmental Assessment Branch

1st Floor
135 St. Clair Avenue W
Toronto [ON M4V 1P5](#)
Tel.: 416 314-8001
Fax.: 416 314-8452

Ministère de l'Environnement, de la
Protection de la nature et des Parcs

*Direction des évaluations
environnementales*

Rez-de-chaussée
135, avenue St. Clair Ouest
Toronto [ON M4V 1P5](#)
Tél. : 416 314-8001
Télec. : 416 314-8452



August 24, 2021

Steven Di Prieto
City of Guelph

Andrew McGregor
R.V. Anderson Associates Limited

Re: **Improvements and modifications to the Macdonell and Allan structures
Schedule C Municipal Class Environmental Assessment
Response to Notice of Commencement**

Dear Steven Di Prieto and Andrew McGregor,

This letter is in response to the Notice of Commencement for the above noted project. The Ministry of the Environment, Conservation and Parks (MECP) acknowledges that the City of Guelph has indicated that the study is following the approved environmental planning process for a Schedule C project under the Municipal Class Environmental Assessment (Class EA).

The **updated (February 2021)** attached "Areas of Interest" document provides guidance regarding the ministry's interests with respect to the Class EA process. Please address all areas of interest in the EA documentation at an appropriate level for the EA study. Proponents who address all the applicable areas of interest can minimize potential delays to the project schedule. **Further information is provided at the end of the Areas of Interest document relating to recent changes to the Environmental Assessment Act through Bill 197, Covid-19 Economic Recovery Act 2020.**

The Crown has a legal duty to consult Aboriginal communities when it has knowledge, real or constructive, of the existence or potential existence of an Aboriginal or treaty right and contemplates conduct that may adversely impact that right. Before authorizing this project, the Crown must ensure that its duty to consult has been fulfilled, where such a duty is triggered. Although the duty to consult with Aboriginal peoples is a duty of the Crown, the Crown may delegate procedural aspects of this duty to project proponents while retaining oversight of the consultation process.

The proposed project may have the potential to affect Aboriginal or treaty rights protected under Section 35 of Canada's *Constitution Act* 1982. Where the Crown's duty to consult is triggered in relation to the proposed project, **the MECP is delegating the procedural aspects of rights-based consultation to the proponent through this letter.** The Crown intends to rely on the delegated consultation process in discharging its duty to consult and maintains the right to participate in the consultation process as it sees fit.

Based on information provided to date and the Crown's preliminary assessment the proponent is required to consult with the following communities who have been identified as potentially affected by the proposed project:

- Mississaugas of the Credit First Nation
- Six Nations of the Grand River (both Elected Council and Haudenosaunee Confederacy Chiefs Council)

Steps that the proponent may need to take in relation to Aboriginal consultation for the proposed project are outlined in the "[Code of Practice for Consultation in Ontario's Environmental Assessment Process](#)". Additional information related to Ontario's Environmental Assessment Act is available online at: www.ontario.ca/environmentalassessments.

Please also refer to the attached document "A Proponent's Introduction to the Delegation of Procedural Aspects of consultation with Aboriginal Communities" for further information, including the MECP's expectations for EA report documentation related to consultation with communities.

The proponent must contact the Director of Environmental Assessment Branch (EABDirector@ontario.ca) under the following circumstances subsequent to initial discussions with the communities identified by MECP:

- Aboriginal or treaty rights impacts are identified to you by the communities
- You have reason to believe that your proposed project may adversely affect an Aboriginal or treaty right
- Consultation with Indigenous communities or other stakeholders has reached an impasse
- A Part II Order request is expected on the basis of impacts to Aboriginal or treaty rights

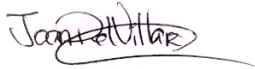
The MECP will then assess the extent of any Crown duty to consult for the circumstances and will consider whether additional steps should be taken, including what role you will be asked to play should additional steps and activities be required.

A draft copy of the report should be sent directly to me prior to the filing of the final report, allowing a minimum of 30 days for the ministry's technical reviewers to provide comments.

Please also ensure a copy of the final notice is sent to the ministry's West Central Region EA notification email account (eanotification.wcregion@ontario.ca) after the draft report is reviewed and finalized.

Should you or any members of your project team have any questions regarding the material above, please contact me at joan.delvillarcuicas@ontario.ca or 365-889-1180.

Yours truly,

A handwritten signature in dark ink, appearing to read "Joan Del Villar C", with a stylized flourish at the end.

Joan Del Villar C
Regional Environmental Planner – West Central Region

cc Katy Potter, Supervisor, Environmental Assessment Services, MECP
 Jeff Burdon, Guelph District Manager, MECP
 Clarissa Whitelaw, Guelph District Supervisor, MECP
 Stephanie Ferraro, Guelph District Supervisor, MECP

Attach: Areas of Interest
 A Proponent's Introduction to the Delegation of Procedural Aspects of Consultation with
 Aboriginal Communities

AREAS OF INTEREST (v. February 2021)

It is suggested that you check off each section after you have considered / addressed it.

☐ Planning and Policy

- Projects located in MECP Central Region are subject to [A Place to Grow: Growth Plan for the Greater Golden Horseshoe \(2020\)](#). Parts of the study area may also be subject to the [Oak Ridges Moraine Conservation Plan \(2017\)](#), [Niagara Escarpment Plan \(2017\)](#), [Greenbelt Plan \(2017\)](#) or [Lake Simcoe Protection Plan \(2014\)](#). Applicable plans and the applicable policies should be identified in the report, and the proponent should describe how the proposed project adheres to the relevant policies in these plans.
- The [Provincial Policy Statement \(2020\)](#) contains policies that protect Ontario's natural heritage and water resources. Applicable policies should be referenced in the report, and the proponent should describe how the proposed project is consistent with these policies.
- In addition to the provincial planning and policy level, the report should also discuss the planning context at the municipal and federal levels, as appropriate.

☐ Source Water Protection

The *Clean Water Act*, 2006 (CWA) aims to protect existing and future sources of drinking water. To achieve this, several types of vulnerable areas have been delineated around surface water intakes and wellheads for every municipal residential drinking water system that is located in a source protection area. These vulnerable areas are known as a Wellhead Protection Areas (WHPAs) and surface water Intake Protection Zones (IPZs). Other vulnerable areas that have been delineated under the CWA include Highly Vulnerable Aquifers (HVAs), Significant Groundwater Recharge Areas (SGRAs), Event-based modelling areas (EBAs), and Issues Contributing Areas (ICAs). Source protection plans have been developed that include policies to address existing and future risks to sources of municipal drinking water within these vulnerable areas.

Projects that are subject to the Environmental Assessment Act that fall under a Class EA, or one of the Regulations, have the potential to impact sources of drinking water if they occur in designated vulnerable areas or in the vicinity of other at-risk drinking water systems (i.e. systems that are not municipal residential systems). MEA Class EA projects may include activities that, if located in a vulnerable area, could be a threat to sources of drinking water (i.e. have the potential to adversely affect the quality or quantity of drinking water sources) and the activity could therefore be subject to policies in a source protection plan. Where an activity poses a risk to drinking water, policies in the local source protection plan may impact how or where that activity is undertaken. Policies may prohibit certain activities, or they may require risk management measures for these activities. Municipal Official Plans, planning decisions, Class EA projects (where the project includes an activity that is a threat to drinking water) and prescribed instruments must conform with policies that address significant risks to drinking water and must have regard for policies that address moderate or low risks.

- In October 2015, the MEA Parent Class EA document was amended to include reference to the Clean Water Act (Section A.2.10.6) and indicates that proponents undertaking a Municipal Class EA project must identify early in their process whether a project is or could potentially be occurring with a vulnerable area. **Given this requirement, please include a section in the report on source water protection.**
 - The proponent should identify the source protection area and should clearly document how the proximity of the project to sources of drinking water (municipal or other) and any delineated vulnerable areas was considered and assessed. Specifically, the report should

discuss whether or not the project is located in a vulnerable area and provide applicable details about the area.

- If located in a vulnerable area, proponents should document whether any project activities are prescribed drinking water threats and thus pose a risk to drinking water (this should be consulted on with the appropriate Source Protection Authority). Where an activity poses a risk to drinking water, the proponent must document and discuss in the report how the project adheres to or has regard to applicable policies in the local source protection plan. This section should then be used to inform and be reflected in other sections of the report, such as the identification of net positive/negative effects of alternatives, mitigation measures, evaluation of alternatives etc.
- While most source protection plans focused on including policies for significant drinking water threats in the WHPAs and IPZs it should be noted that even though source protection plan policies may not apply in HVAs, these are areas where aquifers are sensitive and at risk to impacts and within these areas, activities may impact the quality of sources of drinking water for systems other than municipal residential systems.
- In order to determine if this project is occurring within a vulnerable area, proponents can use this mapping tool: <http://www.applications.ene.gov.on.ca/swp/en/index.php>. Note that various layers (including WHPAs, WHPA-Q1 and WHPA-Q2, IPZs, HVAs, SGRAs, EBAs, ICAs) can be turned on through the “Map Legend” bar on the left. The mapping tool will also provide a link to the appropriate source protection plan in order to identify what policies may be applicable in the vulnerable area.
- For further information on the maps or source protection plan policies which may relate to their project, proponents must contact the appropriate source protection authority. **Please consult with the local source protection authority to discuss potential impacts on drinking water. Please document the results of that consultation within the report and include all communication documents/correspondence.**

More Information

For more information on the *Clean Water Act*, source protection areas and plans, including specific information on the vulnerable areas and drinking water threats, please refer to [Conservation Ontario's website](#) where you will also find links to the local source protection plan/assessment report.

A list of the prescribed drinking water threats can be found in [section 1.1 of Ontario Regulation 287/07](#) made under the *Clean Water Act*. In addition to prescribed drinking water threats, some source protection plans may include policies to address additional “local” threat activities, as approved by the MECP.

☐ **Climate Change**

The document "[Considering Climate Change in the Environmental Assessment Process](#)" (Guide) is now a part of the Environmental Assessment program's Guides and Codes of Practice. The Guide sets out the MECP's expectation for considering climate change in the preparation, execution and documentation of environmental assessment studies and processes. The guide provides examples, approaches, resources, and references to assist proponents with consideration of climate change in EA. Proponents should review this Guide in detail.

• **The MECP expects proponents of Class EA projects to:**

1. Consider during the assessment of alternative solutions and alternative designs, the following:
 - a. the project's expected production of greenhouse gas emissions and impacts on carbon sinks (climate change mitigation); and
 - b. resilience or vulnerability of the undertaking to changing climatic conditions (climate change adaptation).
2. Include a discrete section in the report detailing how climate change was considered in the EA.

How climate change is considered can be qualitative or quantitative in nature and should be scaled to the project's level of environmental effect. In all instances, both a project's impacts on climate change (mitigation) and impacts of climate change on a project (adaptation) should be considered.

- The MECP has also prepared another guide to support provincial land use planning direction related to the completion of energy and emission plans. The "[Community Emissions Reduction Planning: A Guide for Municipalities](#)" document is designed to educate stakeholders on the municipal opportunities to reduce energy and greenhouse gas emissions, and to provide guidance on methods and techniques to incorporate consideration of energy and greenhouse gas emissions into municipal activities of all types. We encourage you to review the Guide for information.

☐ **Air Quality, Dust and Noise**

- If there are sensitive receptors in the surrounding area of this project, a quantitative air quality/odour impact assessment will be useful to evaluate alternatives, determine impacts and identify appropriate mitigation measures. The scope of the assessment can be determined based on the potential effects of the proposed alternatives, and typically includes source and receptor characterization and a quantification of local air quality impacts on the sensitive receptors and the environment in the study area. The assessment will compare to all applicable standards or guidelines for all contaminants of concern. **Please contact this office for further consultation on the level of Air Quality Impact Assessment required for this project if not already advised.**
- If a quantitative Air Quality Impact Assessment is not required for the project, the MECP expects that the report contain a qualitative assessment which includes:
 - A discussion of local air quality including existing activities/sources that significantly impact local air quality and how the project may impact existing conditions;
 - A discussion of the nearby sensitive receptors and the project's potential air quality impacts on present and future sensitive receptors;
 - A discussion of local air quality impacts that could arise from this project during both construction and operation; and
 - A discussion of potential mitigation measures.
- As a common practice, "air quality" should be used an evaluation criterion for all road projects.
- Dust and noise control measures should be addressed and included in the construction plans to ensure that nearby residential and other sensitive land uses within the study area are not adversely affected during construction activities.
- The MECP recommends that non-chloride dust-suppressants be applied. For a comprehensive list of fugitive dust prevention and control measures that could be applied, refer to [Cheminfo Services Inc. Best Practices for the Reduction of Air Emissions from Construction and Demolition Activities](#) report prepared for Environment Canada. March 2005.
- The report should consider the potential impacts of increased noise levels during the operation of the completed project. The proponent should explore all potential measures to mitigate significant noise impacts during the assessment of alternatives.

☐ **Ecosystem Protection and Restoration**

- Any impacts to ecosystem form and function must be avoided where possible. The report should describe any proposed mitigation measures and how project planning will protect and enhance the local ecosystem.
- Natural heritage and hydrologic features should be identified and described in detail to assess potential impacts and to develop appropriate mitigation measures. The following sensitive environmental features may be located within or adjacent to the study area:

- Key Natural Heritage Features: Habitat of endangered species and threatened species, fish habitat, wetlands, areas of natural and scientific interest (ANSIs), significant valleylands, significant woodlands; significant wildlife habitat (including habitat of special concern species); sand barrens, savannahs, and tallgrass prairies; and alvars.
- Key Hydrologic Features: Permanent streams, intermittent streams, inland lakes and their littoral zones, seepage areas and springs, and wetlands.
- Other natural heritage features and areas such as: vegetation communities, rare species of flora or fauna, Environmentally Sensitive Areas, Environmentally Sensitive Policy Areas, federal and provincial parks and conservation reserves, Greenland systems etc.

We recommend consulting with the Ministry of Natural Resources and Forestry (MNRF), Fisheries and Oceans Canada (DFO) and your local conservation authority to determine if special measures or additional studies will be necessary to preserve and protect these sensitive features. In addition, you may consider the provisions of the Rouge Park Management Plan if applicable.

□ **Species at Risk**

- The Ministry of the Environment, Conservation and Parks has now assumed responsibility of Ontario's Species at Risk program. Information, standards, guidelines, reference materials and technical resources to assist you are found at <https://www.ontario.ca/page/species-risk>.
- The Client's Guide to Preliminary Screening for Species at Risk (Draft May 2019) has been attached to the covering email for your reference and use. Please review this document for next steps.
- For any questions related to subsequent permit requirements, please contact SAROntario@ontario.ca.

□ **Surface Water**

- The report must include enough information to demonstrate that there will be no negative impacts on the natural features or ecological functions of any watercourses within the study area. Measures should be included in the planning and design process to ensure that any impacts to watercourses from construction or operational activities (e.g. spills, erosion, pollution) are mitigated as part of the proposed undertaking.
- Additional stormwater runoff from new pavement can impact receiving watercourses and flood conditions. Quality and quantity control measures to treat stormwater runoff should be considered for all new impervious areas and, where possible, existing surfaces. The ministry's [Stormwater Management Planning and Design Manual \(2003\)](#) should be referenced in the report and utilized when designing stormwater control methods. **A Stormwater Management Plan should be prepared as part of the Class EA process** that includes:
 - Strategies to address potential water quantity and erosion impacts related to stormwater draining into streams or other sensitive environmental features, and to ensure that adequate (enhanced) water quality is maintained
 - Watershed information, drainage conditions, and other relevant background information
 - Future drainage conditions, stormwater management options, information on erosion and sediment control during construction, and other details of the proposed works
 - Information on maintenance and monitoring commitments.
- Ontario Regulation 60/08 under the *Ontario Water Resources Act* (OWRA) applies to the Lake Simcoe Basin, which encompasses Lake Simcoe and the lands from which surface water drains into Lake Simcoe. If the proposed sewage treatment plant is listed in Table 1 of the regulation, the report should describe how the proposed project and its mitigation measures are consistent with the requirements of this regulation and the OWRA.

- Any potential approval requirements for surface water taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, except for certain water taking activities that have been prescribed by the Water Taking EASR Regulation – O. Reg. 63/16. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information. Additionally, an Environmental Compliance Approval under the OWRA is required for municipal stormwater management works.

☐ **Groundwater**

- The status of, and potential impacts to any well water supplies should be addressed. If the project involves groundwater takings or changes to drainage patterns, the quantity and quality of groundwater may be affected due to drawdown effects or the redirection of existing contamination flows. In addition, project activities may infringe on existing wells such that they must be reconstructed or sealed and abandoned. Appropriate information to define existing groundwater conditions should be included in the report.
- If the potential construction or decommissioning of water wells is identified as an issue, the report should refer to Ontario Regulation 903, Wells, under the OWRA.
- Potential impacts to groundwater-dependent natural features should be addressed. Any changes to groundwater flow or quality from groundwater taking may interfere with the ecological processes of streams, wetlands or other surficial features. In addition, discharging contaminated or high volumes of groundwater to these features may have direct impacts on their function. Any potential effects should be identified, and appropriate mitigation measures should be recommended. The level of detail required will be dependent on the significance of the potential impacts.
- Any potential approval requirements for groundwater taking or discharge should be identified in the report. A Permit to Take Water (PTTW) under the OWRA will be required for any water takings that exceed 50,000 L/day, with the exception of certain water taking activities that have been prescribed by the Water Taking EASR Regulation – O. Reg. 63/16. These prescribed water-taking activities require registration in the EASR instead of a PTTW. Please review the [Water Taking User Guide for EASR](#) for more information.
- Consultation with the railroad authorities is necessary wherever there is a plan to use construction dewatering in the vicinity of railroad lines or where the zone of influence of the construction dewatering potentially intercepts railroad lines.

☐ **Excess Materials Management**

- In December 2019, MECP released a new regulation under the Environmental Protection Act, titled “[On-Site and Excess Soil Management](#)” (O. Reg. 406/19) to support improved management of excess construction soil. This regulation is a key step to support proper management of excess soils, ensuring valuable resources don’t go to waste and to provide clear rules on managing and reusing excess soil. New risk-based standards referenced by this regulation help to facilitate local beneficial reuse which in turn will reduce greenhouse gas emissions from soil transportation, while ensuring strong protection of human health and the environment. The new regulation is being phased in over time, with the first phase in effect on January 1, 2021. For more information, please visit <https://www.ontario.ca/page/handling-excess-soil>.

- The report should reference that activities involving the management of excess soil should be completed in accordance with O. Reg. 406/19 and the MECP's current guidance document titled "[Management of Excess Soil – A Guide for Best Management Practices](#)" (2014).
- All waste generated during construction must be disposed of in accordance with ministry requirements

□ **Contaminated Sites**

- Any current or historical waste disposal sites should be identified in the report. The status of these sites should be determined to confirm whether approval pursuant to Section 46 of the EPA may be required for land uses on former disposal sites. We recommend referring to the [MECP's D-4 guideline](#) for land use considerations near landfills and dumps.
 - Resources available may include regional/local municipal official plans and data; provincial data on [large landfill sites](#) and [small landfill sites](#); Environmental Compliance Approval information for waste disposal sites on [Access Environment](#).
- Other known contaminated sites (local, provincial, federal) in the study area should also be identified in the report (Note – information on federal contaminated sites is found on the Government of Canada's [website](#)).
- The location of any underground storage tanks should be investigated in the report. Measures should be identified to ensure the integrity of these tanks and to ensure an appropriate response in the event of a spill. The ministry's Spills Action Centre must be contacted in such an event.
- Since the removal or movement of soils may be required, appropriate tests to determine contaminant levels from previous land uses or dumping should be undertaken. If the soils are contaminated, you must determine how and where they are to be disposed of, consistent with *Part XV.1 of the Environmental Protection Act* (EPA) and Ontario Regulation 153/04, Records of Site Condition, which details the new requirements related to site assessment and clean up. Please contact the appropriate MECP District Office for further consultation if contaminated sites are present.

□ **Servicing, Utilities and Facilities**

- The report should identify any above or underground utilities in the study area such as transmission lines, telephone/internet, oil/gas etc. The owners should be consulted to discuss impacts to this infrastructure, including potential spills.
- The report should identify any servicing infrastructure in the study area such as wastewater, water, stormwater that may potentially be impacted by the project.
- Any facility that releases emissions to the atmosphere, discharges contaminants to ground or surface water, provides potable water supplies, or stores, transports or disposes of waste must have an Environmental Compliance Approval (ECA) before it can operate lawfully. Please consult with MECP's Environmental Permissions Branch to determine whether a new or amended ECA will be required for any proposed infrastructure.
- We recommend referring to the ministry's [environmental land use planning guides](#) to ensure that any potential land use conflicts are considered when planning for any infrastructure or facilities related to wastewater, pipelines, landfills or industrial uses.

□ **Mitigation and Monitoring**

- Contractors must be made aware of all environmental considerations so that all environmental standards and commitments for both construction and operation are met. Mitigation measures should be clearly referenced in the report and regularly monitored during the construction stage of the project. In addition, we encourage proponents to conduct post-construction monitoring to ensure all mitigation measures have been effective and are functioning properly.
- Design and construction reports and plans should be based on a best management approach that centres on the prevention of impacts, protection of the existing environment, and opportunities for rehabilitation and enhancement of any impacted areas.
- The proponent's construction and post-construction monitoring plans must be documented in the report, as outlined in Section A.2.5 and A.4.1 of the MEA Class EA parent document.

□ **Consultation**

- The report must demonstrate how the consultation provisions of the Class EA have been fulfilled, including documentation of all stakeholder consultation efforts undertaken during the planning process. This includes a discussion in the report that identifies concerns that were raised and **describes how they have been addressed by the proponent** throughout the planning process. The report should also include copies of comments submitted on the project by interested stakeholders, and the proponent's responses to these comments (as directed by the Class EA to include full documentation).
- Please include the full stakeholder distribution/consultation list in the documentation.

□ **Class EA Process**

- If this project is a Master Plan: there are several different approaches that can be used to conduct a Master Plan, examples of which are outlined in Appendix 4 of the Class EA. **The Master Plan should clearly indicate the selected approach for conducting the plan**, by identifying whether the levels of assessment, consultation and documentation are sufficient to fulfill the requirements for Schedule B or C projects. Please note that any Schedule B or C projects identified in the plan would be subject to Part II Order Requests under the Environmental Assessment Act, although the plan itself would not be. **Please include a description of the approach being undertaken (use Appendix 4 as a reference).**
- If this project is a Master Plan: Any identified projects should also include information on the MCEA schedule associated with the project.
- The report should provide clear and complete documentation of the planning process in order to allow for transparency in decision-making.
- The Class EA requires the consideration of the effects of each alternative on all aspects of the environment (including planning, natural, social, cultural, economic, technical). The report should include a level of detail (e.g. hydrogeological investigations, terrestrial and aquatic assessments, cultural heritage assessments) such that all potential impacts can be identified, and appropriate mitigation measures can be developed. Any supporting studies conducted during the Class EA process should be referenced and included as part of the report.
- Please include in the report a list of all subsequent permits or approvals that may be required for the implementation of the preferred alternative, including but not limited to, MECP's PTTW, EASR Registrations and ECAs, conservation authority permits, species at risk permits, MTO permits and approvals under the *Impact Assessment Act*, 2019.

- Ministry guidelines and other information related to the issues above are available at <http://www.ontario.ca/environment-and-energy/environment-and-energy>. We encourage you to review all the available guides and to reference any relevant information in the report.

Amendments to the EAA through the Covid-19 Economic Recovery Act, 2020

Once the EA Report is finalized, the proponent must issue a Notice of Completion providing a minimum 30-day period during which documentation may be reviewed and comment and input can be submitted to the proponent. The Notice of Completion must be sent to the appropriate MECP Regional Office email address (for projects in MECP Southwest Region, the email is eanotification.swregion@ontario.ca).

The public has the ability to request a higher level of assessment on a project if they are concerned about potential adverse impacts to constitutionally protected Aboriginal and treaty rights. In addition, the Minister may issue an order on his or her own initiative within a specified time period. The Director (of the Environmental Assessment Branch) will issue a Notice of Proposed Order to the proponent if the Minister is considering an order for the project within 30 days after the conclusion of the comment period on the Notice of Completion. At this time, the Director may request additional information from the proponent. Once the requested information has been received, the Minister will have 30 days within which to make a decision or impose conditions on your project.

Therefore, the proponent cannot proceed with the project until at least 30 days after the end of the comment period provided for in the Notice of Completion. Further, the proponent may not proceed after this time if:

- a Part II Order request has been submitted to the ministry regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, or
- the Director has issued a Notice of Proposed order regarding the project.

Please ensure that the Notice of Completion advises that outstanding concerns are to be directed to the proponent for a response, and that in the event there are outstanding concerns regarding potential adverse impacts to constitutionally protected Aboriginal and treaty rights, Part II Order requests on those matters should be addressed in writing to:

Minister Jeff Yurek
Ministry of Environment, Conservation and Parks
777 Bay Street, 5th Floor
Toronto ON M7A 2J3
minister.mecp@ontario.ca

and

Director, Environmental Assessment Branch
Ministry of Environment, Conservation and Parks
135 St. Clair Ave. W, 1st Floor
Toronto ON, M4V 1P5
EABDirector@ontario.ca

A PROPONENT'S INTRODUCTION TO THE DELEGATION OF PROCEDURAL ASPECTS OF CONSULTATION WITH ABORIGINAL COMMUNITIES

DEFINITIONS

The following definitions are specific to this document and may not apply in other contexts:

Aboriginal communities – the First Nation or Métis communities identified by the Crown for the purpose of consultation.

Consultation – the Crown's legal obligation to consult when the Crown has knowledge of an established or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. This is the type of consultation required pursuant to s. 35 of the *Constitution Act, 1982*. Note that this definition does not include consultation with Aboriginal communities for other reasons, such as regulatory requirements.

Crown – the Ontario Crown, acting through a particular ministry or ministries.

Procedural aspects of consultation – those portions of consultation related to the process of consultation, such as notifying an Aboriginal community about a project, providing information about the potential impacts of a project, responding to concerns raised by an Aboriginal community and proposing changes to the project to avoid negative impacts.

Proponent – the person or entity that wants to undertake a project and requires an Ontario Crown decision or approval for the project.

I. PURPOSE

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that may adversely impact that right. In outlining a framework for the duty to consult, the Supreme Court of Canada has stated that the Crown may delegate procedural aspects of consultation to third parties. This document provides general information about the Ontario Crown's approach to delegation of the procedural aspects of consultation to proponents.

This document is not intended to instruct a proponent about an individual project, and it does not constitute legal advice.

II. WHY IS IT NECESSARY TO CONSULT WITH ABORIGINAL COMMUNITIES?

The objective of the modern law of Aboriginal and treaty rights is the *reconciliation* of Aboriginal peoples and non-Aboriginal peoples and their respective rights, claims and interests. Consultation is an important component of the reconciliation process.

The Crown has a legal duty to consult Aboriginal communities when it has knowledge of an existing or asserted Aboriginal or treaty right and contemplates conduct that might adversely impact that right. For example, the Crown's duty to consult is triggered when it considers issuing a permit, authorization or approval for a project which has the potential to adversely impact an Aboriginal right, such as the right to hunt, fish, or trap in a particular area.

The scope of consultation required in particular circumstances ranges across a spectrum depending on both the nature of the asserted or established right and the seriousness of the potential adverse impacts on that right.

Depending on the particular circumstances, the Crown may also need to take steps to accommodate the potentially impacted Aboriginal or treaty right. For example, the Crown may be required to avoid or minimize the potential adverse impacts of the project.

III. THE CROWN'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

The Crown has the responsibility for ensuring that the duty to consult, and accommodate where appropriate, is met. However, the Crown may delegate the procedural aspects of consultation to a proponent.

There are different ways in which the Crown may delegate the procedural aspects of consultation to a proponent, including through a letter, a memorandum of understanding, legislation, regulation, policy and codes of practice.

If the Crown decides to delegate procedural aspects of consultation, the Crown will generally:

- Ensure that the delegation of procedural aspects of consultation and the responsibilities of the proponent are clearly communicated to the proponent;
- Identify which Aboriginal communities must be consulted;
- Provide contact information for the Aboriginal communities;
- Revise, as necessary, the list of Aboriginal communities to be consulted as new information becomes available and is assessed by the Crown;
- Assess the scope of consultation owed to the Aboriginal communities;
- Maintain appropriate oversight of the actions taken by the proponent in fulfilling the procedural aspects of consultation;
- Assess the adequacy of consultation that is undertaken and any accommodation that may be required;
- Provide a contact within any responsible ministry in case issues arise that require direction from the Crown; and
- Participate in the consultation process as necessary and as determined by the Crown.

IV. THE PROPONENT'S ROLE AND RESPONSIBILITIES IN THE DELEGATED CONSULTATION PROCESS

Where aspects of the consultation process have been delegated to a proponent, the Crown, in meeting its duty to consult, will rely on the proponent's consultation activities and documentation of those activities. The consultation process informs the Crown's decision of whether or not to approve a proposed project or activity.

A proponent's role and responsibilities will vary depending on a variety of factors including the extent of consultation required in the circumstance and the procedural aspects of consultation the Crown has delegated to it. Proponents are often in a better position than the Crown to discuss a project and its potential impacts with Aboriginal communities and to determine ways to avoid or minimize the adverse impacts of a project.

A proponent can raise issues or questions with the Crown at any time during the consultation process. If issues or concerns arise during the consultation that cannot be addressed by the proponent, the proponent should contact the Crown.

a) What might a proponent be required to do in carrying out the procedural aspects of consultation?

Where the Crown delegates procedural aspects of consultation, it is often the proponent's responsibility to provide notice of the proposed project to the identified Aboriginal communities. The notice should indicate that the Crown has delegated the procedural aspects of consultation to the proponent and should include the following information:

- a description of the proposed project or activity;
- mapping;
- proposed timelines;
- details regarding anticipated environmental and other impacts;
- details regarding opportunities to comment; and
- any changes to the proposed project that have been made for seasonal conditions or other factors, where relevant.

Proponents should provide enough information and time to allow Aboriginal communities to provide meaningful feedback regarding the potential impacts of the project. Depending on the nature of consultation required for a project, a proponent also may be required to:

- provide the Crown with copies of any consultation plans prepared and an opportunity to review and comment;
- ensure that any necessary follow-up discussions with Aboriginal communities take place in a timely manner, including to confirm receipt of information, share and update information and to address questions or concerns that may arise;
- as appropriate, discuss with Aboriginal communities potential mitigation measures and/or changes to the project in response to concerns raised by Aboriginal communities;
- use language that is accessible and not overly technical, and translate material into Aboriginal languages where requested or appropriate;
- bear the reasonable costs associated with the consultation process such as, but not limited to, meeting hall rental, meal costs, document translation(s), or to address technical & capacity issues;
- provide the Crown with all the details about potential impacts on established or asserted Aboriginal or treaty rights, how these concerns have been considered and addressed by the proponent and the Aboriginal communities and any steps taken to mitigate the potential impacts;
- provide the Crown with complete and accurate documentation from these meetings and communications; and
- notify the Crown immediately if an Aboriginal community not identified by the Crown approaches the proponent seeking consultation opportunities.

b) What documentation and reporting does the Crown need from the proponent?

Proponents should keep records of all communications with the Aboriginal communities involved in the consultation process and any information provided to these Aboriginal communities.

As the Crown is required to assess the adequacy of consultation, it needs documentation to satisfy itself that the proponent has fulfilled the procedural aspects of consultation delegated to it. The documentation required would typically include:

- the date of meetings, the agendas, any materials distributed, those in attendance and copies of any minutes prepared;
- the description of the proposed project that was shared at the meeting;
- any and all concerns or other feedback provided by the communities;
- any information that was shared by a community in relation to its asserted or established Aboriginal or treaty rights and any potential adverse impacts of the proposed activity, approval or disposition on such rights;
- any proposed project changes or mitigation measures that were discussed, and feedback from Aboriginal communities about the proposed changes and measures;
- any commitments made by the proponent in response to any concerns raised, and feedback from Aboriginal communities on those commitments;
- copies of correspondence to or from Aboriginal communities, and any materials distributed electronically or by mail;
- information regarding any financial assistance provided by the proponent to enable participation by Aboriginal communities in the consultation;
- periodic consultation progress reports or copies of meeting notes if requested by the Crown;
- a summary of how the delegated aspects of consultation were carried out and the results; and
- a summary of issues raised by the Aboriginal communities, how the issues were addressed and any outstanding issues.

In certain circumstances, the Crown may share and discuss the proponent's consultation record with an Aboriginal community to ensure that it is an accurate reflection of the consultation process.

c) Will the Crown require a proponent to provide information about its commercial arrangements with Aboriginal communities?

The Crown may require a proponent to share information about aspects of commercial arrangements between the proponent and Aboriginal communities where the arrangements:

- include elements that are directed at mitigating or otherwise addressing impacts of the project;
- include securing an Aboriginal community's support for the project; or
- may potentially affect the obligations of the Crown to the Aboriginal communities.

The proponent should make every reasonable effort to exempt the Crown from confidentiality provisions in commercial arrangements with Aboriginal communities to the extent necessary to allow this information to be shared with the Crown.

The Crown cannot guarantee that information shared with the Crown will remain confidential. Confidential commercial information should not be provided to the Crown as part of the consultation record if it is not relevant to the duty to consult or otherwise required to be submitted to the Crown as part of the regulatory process.

V. WHAT ARE THE ROLES AND RESPONSIBILITIES OF ABORIGINAL COMMUNITIES' IN THE CONSULTATION PROCESS?

Like the Crown, Aboriginal communities are expected to engage in consultation in good faith. This includes:

- responding to the consultation notice;
- engaging in the proposed consultation process;
- providing relevant documentation;
- clearly articulating the potential impacts of the proposed project on Aboriginal or treaty rights; and
- discussing ways to mitigate any adverse impacts.

Some Aboriginal communities have developed tools, such as consultation protocols, policies or processes that provide guidance on how they would prefer to be consulted. Although not legally binding, proponents are encouraged to respect these community processes where it is reasonable to do so. Please note that there is no obligation for a proponent to pay a fee to an Aboriginal community in order to enter into a consultation process.

To ensure that the Crown is aware of existing community consultation protocols, proponents should contact the relevant Crown ministry when presented with a consultation protocol by an Aboriginal community or anyone purporting to be a representative of an Aboriginal community.

VI. WHAT IF MORE THAN ONE PROVINCIAL CROWN MINISTRY IS INVOLVED IN APPROVING A PROPONENT'S PROJECT?

Depending on the project and the required permits or approvals, one or more ministries may delegate procedural aspects of the Crown's duty to consult to the proponent. The proponent may contact individual ministries for guidance related to the delegation of procedural aspects of consultation for ministry-specific permits/approvals required for the project in question. Proponents are encouraged to seek input from all involved Crown ministries sooner rather than later.

APPENDIX 12-6-9

Wood



Mila Khatri

From: Andrew McGregor
Sent: August 24, 2021 3:21 PM
To: Ian Panabaker; Steven Di Pietro
Cc: Connor MacIsaac
Subject: RE: Macdonell EA

Thanks Ian,

We'll add your name to the study circulation list as requested.

Kind regards,



Andrew McGregor, MCIP, RPP

SENIOR PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4211 | m 905 964 4056

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



rvanderson.com



SUMMER HOURS: RVA celebrates the summer season from June 4th to September 3rd. Our offices will be closed at 2 PM each Friday.

From: Ian Panabaker <ian.panabaker@wooddevelopment.ca>
Sent: August 24, 2021 1:48 PM
To: Steven Di Pietro <Steven.DiPietro@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: Macdonell EA

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hello Steven / Andrew

Please add me to the circulation list for communications on this project as you progress. We own major redevelopment lands at Elizabeth / Duke / Huron on the edge of the study area.

Thanks
IP

Ian Panabaker, CAHP, MRAIC
Vice President, Development
Office: 519-827-1900 x208 | Cell/Text: 226-821-1417
wooddevelopment.ca



WOOD DEVELOPMENT GROUP
Creating Space for Life

January 13, 2025

TO: CITY OF GUELPH EA TEAM:

Reg Russwurm, Manager, Design & Construction
Andrew Miller, Project Engineer
Stacey Laughlin, Downtown Revitalization Advisor

RE: **MacDonell & Allen's Structures EA – December 2024 Consultations**

We are submitting this memo to provide additional comments and questions having also submitted online survey format input.

Plant No. 2 – Local Traffic Walkabout – October 30, 2024

FYI -- As part of our own redevelopment project (wooddevelopment.ca/plant2) we hosted local citizens for two walkabouts in The Ward area to hear about local traffic and mobility issues. While we spent more time around our own site (Duke/Huron), consistently the public wanted to take us to Elizabeth/Arthur to point out the pedestrian issues. Not surprisingly they were:

- No ability to cross Elizabeth to get to adjacent Arthur St. N neighbourhood except for walking all the way to Woolwich intersection or all the way to Huron pedestrian signal.
- High level of conflicts with existing intersection design (MacDonell/Arthur/Elizabeth) in terms of poor visibility and inter-threading of cycling and vehicular travel.
- Poor pedestrian facilities in terms of minimum sidewalk widths, railway pylon blind-spots, general visibility etc. in relation to high traffic volumes and turning movements.

These points are provided for information on behalf of the residents we met that day as they relate to the EA area. The feedback supports the need to study and then implement upgrades in this area of the road and pedestrian network from current conditions.

COMMENTS ON DECEMBER 9th 2024 MACDONELL / ALLAN'S STRUCTURES EA

Page 7 - Ward to Downtown Pedestrian Bridge:

1. This is of course an important piece of infrastructure to address existing desire-lines and risky pedestrian activity over the existing GJR bridge. Whichever option gets this bridge built faster is the right decision.

2. The bridge crossing will be a vital connection throughout the year. Outside of the structure selection, there are questions about operational maintenance for the trail and bridge -- will there be ongoing winter maintenance built into the project?

Page 9 - MacDonell Bridge Alternatives

We note the constraints that place the Active Transportation (AT) only on the north side of the bridge. A basic principle to support the success of active transportation is to align routes with conventional traffic patterns. Our concern is depending on circuitous routing of eastbound AT movements to the 'Ward to Downtown' trail and/or a contra-flow facility rather than trying to align conventional routes with the roadway.

3. Is there an alternative Option 6: Replace + widen bridge to accommodate AT on both sides (a combination of Option 3 and 5)? If not, why not?

Page 11 - Allan's Bridge Alternatives

Related to the options around the reconstruction of MacDonell bridge, questions arise about the potential role that a refurbished Allan's Bridge structure could play in supporting East-West Active Transportation movements aligned with the road network.

If MacDonell Bridge can't be widened both sides to accommodate AT both sides, then can Allan's Bridge be used that way? How would this fit into the intersection and roadway designs, and does it make sense to consider rehabilitation (or replacement with a truss bridge?) for that broader use case?

Page 12 – Commemorating Allan's Bridge

The scope and questions about commemoration are probably larger than Allan's Bridge. One of the reasons the 'Ward to Downtown' bridge design was developed as it was was to celebrate and create a space to view this unique area. The confluence of movement systems, rivers, natural heritage, cultural heritage (the founding of Guelph), etc., means that larger landscape thinking should come to bear on all the various components. A Landscape Master Plan for the area could help guide each separate procurement and we would encourage the City to consider a study of this type, to include community engagement. (EA's can't do this kind of synthetic thinking)

Page 14 - Allan's Dam Sluiceway and Spillway Alternatives

We would suggest that the full implications of removal weren't clear:

4. What are the ecological benefits to restoring the natural flow of the Speed in this section? (given that there is generally a movement towards removing river controls where possible, although in this case the Speed is still controlled north and south of this segment)
5. Would it be acceptable to adjacent properties that the pond bed be restored as a meadow and potentially more usable greenspace in the downtown? (for many months of the year it's a mud-flat, not a pond)
6. It is noted in the evaluation grid that removal could allow consideration that a trail be introduced under the MacDonell bridge – shouldn't that benefit be more emphasised?

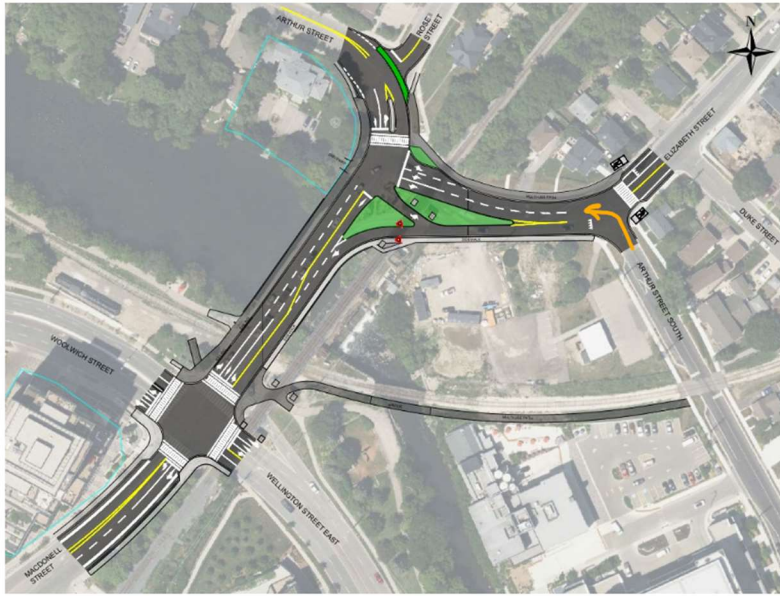
Pages 19-20 -- Preliminary MacDonell Street Intersections (not part of EA):

We appreciate that the intersection options were provided for comment outside of the EA scope. Some principles we wanted to emphasise:

- MacDonell and Elizabeth are the Arterial Roads and should connect and carry the primary flow. The connection of Arthur Street North, and Rose St. beyond, should be considered subordinate local street connections. (as in Concept 2)
- The intersection of Woolwich/MacDonell/Wellington, while playing an important vehicular role with a lot of turning priorities, is terrible for pedestrians and cyclists at the moment. There are more lanes and thru-lanes indicated on the drawings than exist now – these should be challenged and limited if possible. Many of these streets are now 40 km/hr yet the design of the roadway still reflects the 'downtown bypass' history of higher speeds and flow.

With regards to the diagrams:

7. Concepts show the MUP ending just east of the Elizabeth Street / Arthur Street intersection. Is the City's preference to continue this MUP further east along Elizabeth Street in future or continue to transition to bi-directional cycle lanes? This is part of the likely low-success rate adoption of awkward eastbound routes.
8. Concepts 1 & 3 show the pedestrian crossing on the east side while concepts 2 & 4 show the pedestrian crossing on the west. Locating the pedestrian crosswalk on the east side of the intersection is preferential to separate conflicts and provide capacity for northbound left vehicles and crossing pedestrians when the pedestrian crossing is activated.

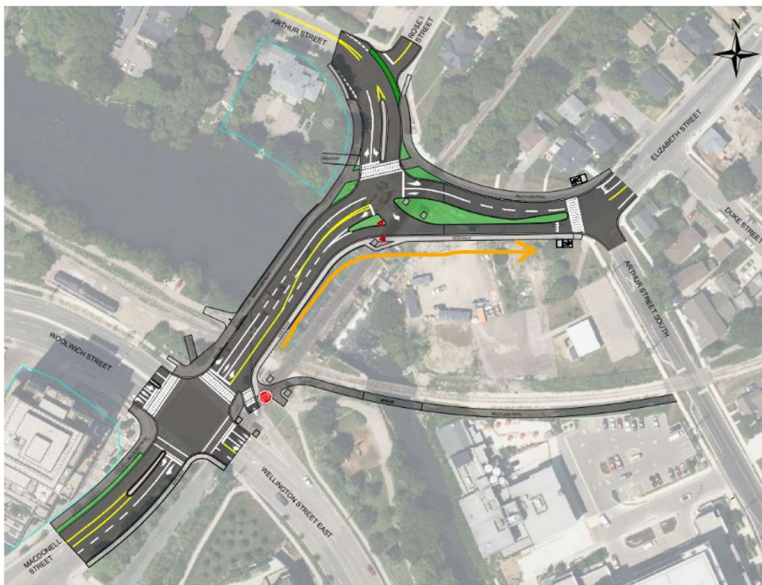


Notes:

This configuration puts too much weight on Arthur St./MacDonell

We like this position of the Elizabeth pedestrian crossing

9. Speeding along Elizabeth (north to eastbound traffic) is a safety concern for residents. Are there any design considerations or additional measures to mitigate this concern?



Notes:

This configuration correctly aligns MacDonell with Elizabeth Street.

Can Arthur Street be designed even tighter to imply its local role?

General concern about speeds and thru movement of EB vehicles

Elizabeth pedestrian crossing location not preferred.

Wellington Street / MacDonell Street Intersection:

10. Concept 2 and 4 features a right-turn 'smart channel' leading to the Speed River crossing. Given that this is a known truck route, have tractor-trailer turning movements been tested for this right turn into a single lane?
11. Equally, Concept 1 and 3 removes the 'smart channel' but leaves only one EB lane on MacDonell. Has this been tested with bus and tractor-trailer movements?

12. The existing north-south sidewalk on the west side of Wellington Street is quite narrow and squeezed at the railway abutment. Have any design considerations been made to widen it or enhance the pedestrian experience and safety? (given that currently there is only one SB vehicular lane – but its double-wide)
13. What on-street parking conditions along Wellington are being considered north and south of MacDonell Street? (There is currently some off-peak or special vehicle parking permitted related to RiverRun, as well, southbound Woolwich traffic is carried on one lane while adjacent built-form would benefit from on-street parking.
14. Are the three WB lanes on MacDonell Bridge really required (RH, Thru, LH) at the expense of an additional EB lane?

In general, the roadways and intersections, while carrying a lot of traffic and accommodating complex movements (including a railway crossing!) needs more pedestrian and active mobility rebalancing. While some of the speed limits in the area have been reduced, the detailing of the roadways need to start reflecting the urban character of the Downtown Secondary Plan.

Thank you allowing us to comment in more detail, should you have any questions about what's been provided we would be happy to clarify. We look forward to further opportunities to provide feedback and reviews as the work progresses.

Yours,



Ian Panabaker, CAHP, MRAIC
Vice President, Development, Wood Development Group

cc. Emily Ecker, BA Group

February 19, 2025

Sent via Email: ian.panabaker@wooddevelopment.ca

Ian Panabaker, CAHP, MRAIC
Vice President, Development
Wood Development Group
5068 Whitelaw Rd Unit 1
Guelph, ON N1H 6J3

Dear Mr. Panabaker:

RE: Guelph Macdonell and Allan's Structures Schedule 'B' Municipal Class EA
Response to Letter Dated January 13, 2025 RE: Macdonell & Allan's Structures
EA – December 2024 Consultations

On behalf of the City of Guelph, thank you for your letter dated January 13, 2025, regarding comments on the Macdonell and Allan's Structures Municipal Class EA. We have reviewed your comments and provide the following responses.

Ward to Downtown Bridge

1. We appreciate your expressed support in building a Ward to Downtown Bridge and have noted your comments.
2. Standard operations and maintenance costs have been included as part of the evaluation of alternatives for the Bridge. Costs associated with winter operations and maintenance will be determined upon Council approval of the proposed works.

Macdonell Bridge

3. The accommodation of active transportation (AT) along Macdonell Bridge via uni-directional cycle tracks along both sides of the bridge was assessed by the project team through the EA. Based on the evaluation, widening the bridge to accommodate AT on the north side was determined to be the preferred cross section, particularly when considered in conjunction with the additional Ward to Downtown Bridge crossing. Following assessment of various scenarios (AT on both sides; AT on north; and AT on south side), it was determined that a multi-use path (MUP) on the north side would best meet the needs of the City's Cycling Network, provide connections to nearby trails, reduce the degree of impact to nearby designated Cultural Heritage properties, and better tie-in with the Macdonell/Woolwich intersection. Furthermore, increasing the center-line shift to accommodate AT on both sides would result in significantly more property impacts and intersection traffic operations. Thus, AT/Multi-Use path on the north side only (and sidewalk on the south) was selected as the recommended and further refined

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prior to the alternatives being presented to the public.

Allan's Bridge

4. The alternative to rehabilitate Allan's Bridge to accommodate active transportation was considered in the evaluation (Alternative 3). By utilizing the bridge for active transportation, it would increase the complexity of the flow of pedestrians, cyclists, and traffic in the area including sightline issues with oncoming trains for bridge users. When considered in totality, i.e. with the recommendation to proceed with the new Ward to Downtown bridge, the costs associated with maintaining the Allan's bridge for AT purposes only was determined to be excessive.

Commemorating Allan's Bridge

5. Thank you for your comments regarding the commemoration of Allan's Bridge and suggestion for the City to undertake a Landscape Master Plan.

Allan's Dam Sluiceway and Spillway

6. The evaluation tables presented at the second Open House held on December 9, 2024, showed a summary of the evaluation for the Natural Environment criteria and sub-criteria. A detailed evaluation was completed in the background and it identified the following ecological benefits of removing the sluiceway and spillway: restoration of connectivity in the migration corridor, increase in littoral zone and wetland plants, increase in area of valley lands, restoration of the natural floodplain and sediment transport, and improved thermal regime.
7. Regarding the pond bed restoration to a meadow or greenspace, it is our opinion that the floodplains could be naturalized with wetland plants to enhance the riparian buffer. However, with the mud flats submerged for some of the year, the ability to sustain many plants is limited, so this would be a more feasible option if the dam/sluiceway/spillway were removed. We would not recommend infill and upland planting (like meadow) to increase greenspace for people, instead, the area would be excellently suited for naturalization and wildlife-use, with the added benefit of providing additional filtration of runoff before it enters the watercourse. However, to increase its potential as an amenity for people; trails, benches and lookouts could be incorporated to provide easily-accessible nature appreciation opportunities, specifically for birding.
8. Benefits of the trail under Macdonell Bridge, primarily around safety for users, was considered by the project team and factored into the City's evaluation. Based on our evaluation against various criteria, the City recommended rehabilitation of the spillway/sluiceway, as opposed to complete removal.

Ian Panabaker, CAHP, MRAIC

February 19, 2025

RE: Guelph Macdonell and Allan's Structures Schedule 'B' Municipal Class EA

Page 3 of 3

Preliminary Macdonell Street Intersections

Thank you for your comments regarding the Macdonell Street intersections; they will be taken into consideration when refining the intersection concepts. The intersections will be reconfigured to provide greater connectivity and improved safety for pedestrians and cyclists crossing the intersections and bridge. The Project Team has been working in coordination with City transportation staff, particularly staff working on the Cycling Network, to gather their feedback on how to best connect nearby trails and other cycling routes through these intersections.

Please let us know if you have any further questions or comments regarding the Macdonell and Allan's Structures Class EA.

Yours very truly,

Andrew Miller, P.Eng., PMP, Project Engineer
D&C, Infrastructure, Development, and Environment Services
City of Guelph

T **519-822-1260 x 3274**

E **Andrew.Miller@guelph.ca**

C Reg Russwurm, City of Guelph
 Stacey Laughlin, City of Guelph
 Andrew McGregor, R.V. Anderson Associates Limited
 Mila Khatri, R.V. Anderson Associates Limited

APPENDIX 12-7
Utility Coordination



APPENDIX 12-7-1

Alectra



Mila Khatri

From: Ian Bolton <Ian.Bolton@alectrautilities.com>
Sent: August 12, 2021 9:06 PM
To: Connor MacIsaac
Cc: Steven Di Pietro; David Di Pietro; Andrew McGregor; Reg.Russwurm@guelph.ca; Mark Jakubowski
Subject: RE: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation
Attachments: Allan's Bridge_Guelph.pdf; 1978061-RC22.tif; 1977007-RC22-A.tif; 1977007-RC22-C.tif; 1977007-RC22-D.tif; 1000083-SL22-D.tif

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Connor,
Attached is a clip from our City map of the requested area along with related construction drawings for your reference. I have also cc'd Mark Jakubowski from Alectra Utilities who would be involved if there are any relocations required of the distribution system.
Thanks

Ian



Ian Bolton, C.E.T.,
Supervisor, Planned Capital
[395 Southgate Drive, Guelph, ON N1G 4Y1](https://www.guelph.ca/395-Southgate-Drive-Guelph-ON-N1G-4Y1)

t 519.837.4717 | m 519.241.1447

[alectrautilities.com](https://www.alectrautilities.com)



Alectra is focused on delivering safe and reliable electricity to you during the COVID-19 pandemic. Our operations have been scaled back to ensure the safety of our employees and the public and to respond to emergencies as required. Our convenient online customer service portal is available to assist you with most of your Alectra account needs. Call waiting times may be longer than usual. Thank you for your patience

Alectra Utilities is a scent-free environment. If you will be visiting our offices, please do not wear or use scented products (perfume, cologne, after shave, lotions, shampoo, conditioner, hair spray, fabric softener, dryer sheets and scented laundry detergent).

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This message has been sent to you by Alectra Utilities, [395 Southgate Drive, Guelph, Ont N1G-4Y1.](#)”

From: Connor MacIsaac <cmacisaac@rvanderson.com>
Sent: Thursday, August 12, 2021 4:13 PM
Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Reg.Russwurm@guelph.ca
Subject: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation

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Dear Sir/Madam,

On behalf of the City of Guelph, please find attached the Notice of Study Commencement for the Macdonell and Allan Structures Municipal Class EA (Schedule C MCEA). The study will review opportunities for improvements and modifications to the Macdonell and Allan Structures and surrounding area in the Macdonell corridor at the Speed River to facilitate the City's proposed Downtown Infrastructure Revitalization Program.

At this point in the study, we are requesting any relevant background information from your utility company as it relates to the study area. Information that would be of interest includes:

- Markups of existing infrastructure and any planned expansion or repairs to existing infrastructure in the study; and
- Contact information and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process.

Please respond to this email with your comments, contact directly the project team members listed on the attached notice, or visit the project webpage [Macdonell and Allan Structures Municipal Class Environmental Assessment - City of Guelph](#) for further information on this project.

Best Regards,



Connor MacIsaac, EPT

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

📞 905 685 5049 ext. 4218

📍 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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APPENDIX 12-7-2

Bell



Mila Khatri

From: Ackerman, R. Neil <neil.ackerman1@bell.ca>
Sent: August 12, 2021 4:35 PM
To: Connor MacIsaac; Zuk, Andrew
Cc: Steven Di Pietro; David Di Pietro; Andrew McGregor; Reg.Russwurm@guelph.ca
Subject: RE: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation
Attachments: Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf
Categories: Technical Agency Comments

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hello Connor

There is 2 very large Bell ducts structures on the south side of the bridge containing local & city to city long haul networks.

Please send Andrew Zuk (best Bell point of contact) a base CAD plan for the bridge and he will have our mark up team add / mark up our structures.

I am not aware of any planned upgrades to happen 2021-2022 to add any additional cables or make any other changes affecting the bridge.

In 2023-2025 to meet Guelph residents demands Bell will be upgrading the north east side of Guelph and may require additional fiber feed cables placed through this pathway in the bridge.



Neil Ackerman
Implementation Manager, Network Provisioning

F1-575 Riverbend Drive
Kitchener, Ontario
N2K 3S3
P 519.568.5797
C 226.750.5389
neil.ackerman1@bell.ca

From: Connor MacIsaac <cmacisaac@rvanderson.com>

Sent: August-12-21 4:13 PM

Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Reg.Russwurm@guelph.ca

Subject: [EXT]City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation

Dear Sir/Madam,

On behalf of the City of Guelph, please find attached the Notice of Study Commencement for the Macdonell and Allan Structures Municipal Class EA (Schedule C MCEA). The study will review opportunities for improvements and modifications to the Macdonell and Allan Structures and surrounding area in the Macdonell corridor at the Speed River to facilitate the City's proposed Downtown Infrastructure Revitalization Program.

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Please respond to this email with your comments, contact directly the project team members listed on the attached notice, or visit the project webpage [Macdonell and Allan Structures Municipal Class Environmental Assessment - City of Guelph](#) for further information on this project.

Best Regards,



Connor MacIsaac, EPT

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

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APPENDIX 12-7-3

Enbridge Gas



Mila Khatri

From: Kevin Schimus <Kevin.Schimus@enbridge.com>
Sent: August 23, 2021 7:29 AM
To: Connor MacIsaac
Cc: Steven Di Pietro; David Di Pietro; Andrew McGregor; Reg.Russwurm@guelph.ca; Shawn Artt
Subject: RE: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation
Attachments: Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf; Enbridge Gas Macdonnell St, Guelph.pdf

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Hi Connor,

Please find attached PDF file showing information for Enbridge Gas plant locations in respect to the above-mentioned project, for engineering purposes only. The location of Enbridge Gas facilities on this drawing is approximate and is to be used for information purposes. It is understood that locates must be obtained through Ontario One Call Limited at 1-800-400-2255 to confirm location of our gas line prior to excavation. Enbridge Gas currently does not have any upgrade plans in this area.

Regards,

Kevin Schimus

Sr. Advisor, Construction and Project Management
Southeast Region Construction and Growth

Enbridge Gas Inc
Cell: 519-635-9488 | Kevin.Schimus@enbridge.com
603 Kumpf Drive, Waterloo, Ontario, N2V 1K3

enbridgegas.com
Safety. Integrity. Respect. Inclusion.

From: Connor MacIsaac <cmaisaac@rvanderson.com>
Sent: Thursday, August 12, 2021 4:13 PM
Cc: Steven Di Pietro <Steven.DiPietro@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Reg.Russwurm@guelph.ca
Subject: [External] City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation

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Dear Sir/Madam,

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- Contact information and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process.

Please respond to this email with your comments, contact directly the project team members listed on the attached notice, or visit the project webpage [Macdonell and Allan Structures Municipal Class Environmental Assessment - City of Guelph](#) for further information on this project.

Best Regards,



Connor MacIsaac, EPT

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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APPENDIX 12-7-4

Rogers



Markup Response Form

Application Date	May 7, 2025	Applicant:	R.V. Anderson Associates Limited
Date Returned:	May 16, 2025		
Rogers Ref. No.:	G251623	Applicant Ref. No.:	20251917978
Location / Municipality:	Macdonell St/Guelph		

Rogers Communications has reviewed your drawing(s) as requested.
Our comments follow below with an "X" indicating Rogers' stance on your proposed plan.

Markup Response is valid for 6 months from the date issued.

Please inform Rogers Communications a minimum of 6 - 12 months in advance of the proposed construction schedule in order to coordinate our plant relocation.

Contact Ontario One Call at 1-800-400-2255 or www.on1call.com at least 5 business days before beginning work to obtain utility locates.
Hand dig / Vac truck when crossing, or within 1.0m of existing Rogers plant.
Plant is to approximation.

Comments:

- | | | |
|-------------------------------------|---------------------------|--|
| <input checked="" type="checkbox"/> | Markup Only | Not for PUCC Approval |
| <input type="checkbox"/> | No Plant | Rogers Communications currently does not possess existing plant in the area indicated on your attached plans |
| <input checked="" type="checkbox"/> | For your Reference | Rogers Communications currently has existing plant as marked on your drawing. Our standard depth in this municipality is: 1m
Please ensure you maintain clearances of 0.3m vertically and 0.6m horizontally |
| <input type="checkbox"/> | No Conflict | Meets with Approval |
| <input type="checkbox"/> | CONFLICT | Your proposed construction appears to encroach within existing Rogers Communications plant. Please ensure you maintain clearances of 0.3 m vertically and 0.6 m horizontally. For hand dig maintain 0.6 m and for directional bore maintain 1.0 m horizontally. Please relocate your proposed construction to allow adequate clearance |

CAUTION NOTES:

- | | |
|-------------------------------------|--|
| <input type="checkbox"/> | Use vactruck and expose ducts, maintain minimum of 0.6m clearance |
| <input checked="" type="checkbox"/> | Rogers Communications has aerial plant in this area, as it is indicated on the attached plans |
| <input checked="" type="checkbox"/> | Fiber Optic Cable is present in the area of your proposed construction. Please obtain locates and maintain minimum 1.0m/1.0m clearance |
| <input type="checkbox"/> | Proposed Fiber Optic Cable in a joint use duct structure |
| <input checked="" type="checkbox"/> | Plant currently under construction |

APPENDIX 12-7-5

Telus



* Markups of existing infrastructure and any planned expansion or repairs to existing infrastructure in the study;

Best Regards,

Connor MacIsaac, EPT

junior environmental planner, ea & approvals

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1

<<https://can01.safelinks.protection.outlook.com/?url=https%3A%2F%2Fwww.linkedin.com%2Fcompany%2Fr-v-anderson-associates-limited&data=04%7C01%7CTelusUtilitymarkups%40telecon.ca%7Cd019b9247cdc4f77f94008d95dcd9d42%7Cbb329f08cad94614ad223bf29fdeb248%7C0%7C0%7C637643959999562318%7CUnknown%7CTWFpbGZsb3d8eyJWljiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6I1haWwiLCJXVCI6Mn0%3D%7C1000&sdata=SIRXqt%2FmCBhPz63iiM5wPrRJIjQ%2FCZBZoopZoXG%2FGok%3D&reserved=0>>

<<https://can01.safelinks.protection.outlook.com/?url=https%3A%2F%2Fwww.facebook.com%2FRVAndersonAssociates&data=04%7C01%7CTelusUtilitymarkups%40telecon.ca%7Cd019b9247cdc4f77f94008d95dcd9d42%7Cbb329f08cad94614ad223bf29fdeb248%7C0%7C0%7C637643959999562318%7CUnknown%7CTWFpbGZsb3d8eyJWljiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6I1haWwiLCJXVCI6Mn0%3D%7C1000&sdata=5c0zUMI8FAPhCQaWTwSRpJCxVQbuMYSXQstNFhDx0Og%3D&reserved=0>>

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<<https://can01.safelinks.protection.outlook.com/?url=https%3A%2F%2Freviews.canadastop100.com%2Ftop-employer-rv-anderson-associates&data=04%7C01%7CTelusUtilitymarkups%40telecon.ca%7Cd019b9247cdc4f77f94008d95dcd9d42%7Cbb329f08cad94614ad223bf29fdeb248%7C0%7C0%7C637643959999582232%7CUnknown%7CTWFpbGZsb3d8eyJWljiMC4wLjAwMDAiLCJQIjoiV2luMzliLCJBTiI6I1haWwiLCJXVCI6Mn0%3D%7C1000&sdata=AobE674SJnzKdV9GMVoEOKFWqvXb848T8R1G4Asg6NM%3D&reserved=0>>

AwMDAiLCJQIjoiV2luMzliLCJBtIl6Ik1haWwiLCJXVCI6Mn0%3D%7C1000&sdata=JdUvUZ5xObwxP2EqveLza6p4JaSxqJNMIwLgF%2B60K3U%3D&reserved=0>

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APPENDIX 12-7-6

Zayo



Mila Khatri

From: phil.arbeau@zayo.com on behalf of Utility Circulations <utility.circulations@zayo.com>
Sent: August 30, 2021 6:24 PM
To: Connor MacIsaac
Subject: Re: City of Guelph - Macdonell and Allan Structures Municipal Class Environmental Assessment - Utility Circulation
Attachments: Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good afternoon,

Zayo does have existing plant in the area indicated in your submission. Please maintain standard clearances and we have no objection. Thank you.

Phil Arbeau
Utility Circulations

On Thu, 12 Aug 2021 at 16:13, Connor MacIsaac <cmacIsaac@rvanderson.com> wrote:

Dear Sir/Madam,

On behalf of the City of Guelph, please find attached the Notice of Study Commencement for the **Macdonell and Allan Structures Municipal Class EA (Schedule C MCEA)**. The study will review opportunities for improvements and modifications to the Macdonell and Allan Structures and surrounding area in the Macdonell corridor at the Speed River to facilitate the City's proposed Downtown Infrastructure Revitalization Program.

At this point in the study, we are requesting any relevant background information from your utility company as it relates to the study area. Information that would be of interest includes:

- **Markups** of existing infrastructure and **any planned expansion or repairs** to existing infrastructure in the study; and
- **Contact information** and the identification of individuals that represent your agency or group that we should include as a primary contact throughout the study process.

Please respond to this email with your comments, contact directly the project team members listed on the attached notice, or visit the project webpage [Macdonell and Allan Structures Municipal Class Environmental Assessment - City of Guelph](#) for further information on this project.

Best Regards,



Connor MacIsaac, EPT

JUNIOR ENVIRONMENTAL PLANNER, EA & APPROVALS

t 905 685 5049 ext. 4218

a 43 Church Street, Suite 104, St. Catharines, ON L2R 7E1



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APPENDIX 12-8

Indigenous Community Consultation



APPENDIX 12-8-1

Haudenosaunee Confederacy Chiefs Council /
Haudenosaunee Development Institute



September 16, 2021

Attn: Haudenosaunee Confederacy Chiefs Council
Haudenosaunee Development Institute
16 Sunrise Court
Suite 600 Ohsweken, ON
P.O. Box 714
hdi2@bellnet.ca

Via E-mail only

RE: Haudenosaunee Confederacy Chiefs Council Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments

Dear Sir / Madam,

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Haudenosaunee, since time immemorial. We wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph Project to the Haudenosaunee Confederacy Chiefs Council and that this engagement will also help to grow our relationship by facilitating meaningful dialogue. Your participation will ensure your input is considered in the project as it moves forward.

About the Downtown Guelph Project

As you may know, Guelph's downtown has aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)
- The other EA will examine the Macdonell Street Bridge and Allan Dam structures across the Speed River to confirm required improvements and the preferred solution for each.

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By the end of 2022, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in early 2023. Construction will start no sooner than 2024.

The Downtown Guelph Project study area includes the Downtown Secondary Plan area north of the railway tracks as outlined in the image below.



About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

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Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and answer any questions you may have.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

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Next steps

We would be pleased to discuss the project in further detail together, and welcome the opportunity for HCCC representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, two virtual public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
Leslie Muñoz, City of Guelph
Tracey Ghdi, Haudenosaunee Confederacy Chiefs Council
Hohahes Leroy Hill, Haudenosaunee Confederacy Chiefs Council

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1 Carden St
Guelph, ON
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T 519-822-1260
TTY 519-826-9771

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Connor MacIsaac

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: September 17, 2021 8:52 AM
To: 'hdi2@bellnet.ca'
Cc: 'traceyghdi@gmail.com'; 'jocko@sixnations.ca'; Nick Palomba; Andrew McGregor; Connor MacIsaac; Leslie Muñoz; David Di Pietro
Subject: City of Guelph - Downtown Infrastructure Revitalization Program - Notice of EA Commencement
Attachments: HCCC-DowntownProjectNoticeLetter-signed.pdf; Notice of Commencement-WyndhamStreetClassEA_FINAL.pdf; Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate before Replying or Clicking on any links

Dear Sir / Madam,

Enclosed please find an introductory letter regarding the City of Guelph's Downtown Infrastructure Revitalization Program along with the Notices of Commencement for two Environmental Assessments being completed as part of this work.

Please do not hesitate to contact me if you have any questions.

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**
Engineering and Transportation Services, **Infrastructure, Development and Enterprise**
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

guelph.ca
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Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: May 27, 2022 3:19 PM
To: info@hdi.land
Cc: Nick Palomba; Andrew McGregor; Connor MacIsaac; Leslie Muñoz; David Di Pietro; Steven Di Pietro
Subject: Potentially TamperedThe Haudenosaunee Confederacy Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments
Attachments: HC-DowntownProjectNotice-Follow-up-vf.pdf; Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf; Notice of Commencement-WyndhamStreetClassEA_FINAL.pdf
Categories: Indigenous

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Dear Sir / Madam,

Please find attached a follow-up letter and Notice of Commencements for two Environmental Assessment in Downtown Guelph.

Sincerely,

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

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May 30, 2022

Attn: The Haudenosaunee Confederacy
c/o Haudenosaunee Development Institute
P.O. Box 714
Ohsweken, Ontario, N0A 1M0
info@hdi.land

Via E-mail & Hard Copy

**RE: The Haudenosaunee Confederacy Engagement for the Downtown Guelph Project
Municipal Class Environmental Assessments**

Dear Sir / Madam,

This letter is a follow up to our initial correspondence sent on August 30, 2021.

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Haudenosaunee, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph project to the Haudenosaunee Confederacy Chiefs Council and will also help to grow the relationship between the City of Guelph and HCCC by facilitating meaningful engagement and dialogue. Your participation will ensure your input is considered in the project as it moves forward.

About the Downtown Guelph Project

As you may know, Guelph's downtown is filled with aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)
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In 2023, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in 2023/2024. Construction will start no sooner than 2024.

About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers

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Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and any questions you may have about the HCCC interest and involvement in this project.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

Next steps

We would be pleased to discuss the project in further detail together, and welcome the opportunity for HCCC representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, three public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

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cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
Leslie Muñoz, City of Guelph

Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: November 2, 2022 3:28 PM
To: info@hdi.land
Cc: David Di Pietro; Steven Di Pietro; Connor MacIsaac; Andrew McGregor; Nick Palomba; Stephanie Bryenton; Leslie Muñoz
Subject: City of Guelph - Downtown Project Update
Attachments: HCCC-DowntownProjectUpdate_20221101.pdf

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Dear Sir / Madam,

Attached is a letter update on the City of Guelph's Downtown Renewal Project along with a link to a Stage 1 Archeological Assessment.

 [City of Guelph - Stage 1 Archeological Assessment](#)

Please do not hesitate to contact me if you wish to discuss the project further.

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
519-822-1260 extension 2765
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November 1, 2022

Attn: The Haudenosaunee Confederacy
c/o Haudenosaunee Development Institute
P.O. Box 714
Ohsweken, Ontario, N0A 1M0
info@hdi.land

Via E-mail Only

RE: The Haudenosaunee Confederacy Engagement for the Downtown Renewal Project Municipal Class Environmental Assessments

Dear Sir / Madam,

This letter is a follow up to our initial correspondence sent on August 30, 2021 and our subsequent correspondence May 27, 2022.

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Haudenosaunee, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to discuss with you our work to date and learn of any issues, concerns or recommendations that you may have with regard to this initiative. We welcome the opportunity for Haudenosaunee Confederacy Chiefs Council representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project.

We hope the information below will be useful in explaining the Downtown Guelph project to HCCC and will also help to grow the relationship between the City of Guelph and HCCC by facilitating a genuine and meaningful relationship. Your insight will ensure your voice is centered and input is considered in the project as it moves forward.

About the Project

Planning for Downtown Renewal began in spring of 2021. During the planning phase we are looking at the existing infrastructure and the future needs of downtown Guelph to determine what we need to update.

As part of the planning work, we will be completing two Municipal Class Environment Assessment (EA) studies that will involve collaborating with the community to support the planning and decision-making process:

- The Wyndham Street EA study, which will examine the function of the road for all users, including vehicle capacity requirements and active transportation amenities.
- The Macdonell Street Bridge and Allan Structures EA across the Speed River to address required structural and corridor improvements.

By the end of 2024, we will present the EA studies and Downtown Renewal approach and recommendations to City Council for approval. We expect to start

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work on the final design of the first project for the Downtown Renewal in late 2024 or early 2025. Construction will start no sooner than 2026. The overall design and construction process is expected to last from eight to 10 years depending on pace and capacity.

Work to Date and Preliminary Findings

Since initiating the project in 2021, a number of studies have been, or are in the process of being completed, including a Stage 1 Archaeological Assessment, Natural Environmental Assessment, and Cultural Heritage Evaluation & Preliminary Impact Assessment. The project team has also developed preliminary options for consideration, for each EA.

Please visit the project webpage <https://guelph.ca/living/construction-projects/downtown-infrastructure-revitalization/> or the website haveyoursay.guelph.ca/downtownproject, for more information about the project.

We would be pleased to discuss the project and our findings with MCFN representatives. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Input on Stage 1 Archaeological Assessment Report

The City would also like to invite HCCC to provide input on the Stage 1 Archaeological assessment Report. As such, a copy of the draft Stage 1 Archaeological Assessment Report has also been provided for HCCC representatives' review and input. We would appreciate any formal comments on this report by **November 30, 2022**.

Once the report has been updated to reflect HCCC representatives' comments, it will be finalized and submitted for review to the Ontario Ministry of Citizenship and Multiculturalism. Kindly advise if you would like to receive a copy of any of the other noted documents and we would be happy to provide them once available.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

Encl.: Stage 1 Archaeological Assessment

cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.

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Leslie Muñoz, City of Guelph
Stephanie Bryenton, City of Guelph
Andrew McGregor, R.V. Anderson Associated Ltd.

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April 25, 2024

Attn: The Haudenosaunee Confederacy
c/o Haudenosaunee Development Institute
P.O. Box 714
Ohsweken, Ontario, N0A 1M0
info@hdi.land

Via E-mail Only

RE: The Haudenosaunee Confederacy Engagement for the Guelph Downtown Infrastructure Renewal Program Municipal Class Environmental Assessments

Dear Sir / Madam,

This letter is a follow up to our initial correspondence sent on August 30, 2021, and our subsequent correspondence on May 27, 2022, and November 1, 2022.

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Haudenosaunee, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to provide Haudenosaunee Confederacy Chiefs Council representatives the opportunity to **review the revised Stage 1 Archaeological Assessment report, which has been updated to include an assessment of the extended study area of Downtown Guelph.** We welcome the opportunity for HCCC representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project.

We hope the information below will be useful in explaining the Downtown Guelph project to HCCC and will also help to grow the relationship between the City of Guelph and HCCC by facilitating a genuine and meaningful relationship. Your insight will ensure your voice is centered and input is considered in the project as it moves forward.

About the Project

Planning for the Downtown Infrastructure Renewal Program (DTIRP) began in spring of 2021. During the planning phase we are looking at the existing infrastructure and the future needs of Downtown Guelph to determine what needs to be replaced or upgraded.

As part of this program of work, the City is undertaking a Class Environmental Assessment (EA) for the Macdonell Street bridge and Allans Dams structures and has completed an EA process for Wyndham Street (initiated as an EA however now exempt). The City is now developing an overall Capital Implementation Plan for this and other works requiring infrastructure upgrades in the City's downtown core.

By early 2025 we anticipate presenting the Macdonell and Allans Structures EA study and Downtown Renewal recommendations to City Council for approval.

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Detailed design of the first phase of DTIRP (i.e. Wyndham Street corridor) has recently been initiated following approval by Council. Construction will start no sooner than 2026.

Input on the Updated Stage 1 Archaeological Assessment Report

The City would like to invite HCCC to provide input on the updated Stage 1 Archaeological Assessment Report. As such, a copy of the draft updated Stage 1 Archaeological Assessment Report has been provided for HCCC representatives' review and input. We would appreciate any formal comments on this report by **May 28, 2024**.

Once the report has been updated to reflect HCCC representatives' comments, it will be finalized and submitted for review to the Ontario Ministry of Citizenship and Multiculturalism. Kindly advise if you would like to receive a copy of any of the other noted documents and we would be happy to provide them once available.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

Encl.: Stage 1 Archaeological Assessment

cc: Andrew McGregor, R.V. Anderson Associates Ltd.
Andrew Miller, City of Guelph
Kimberly Krawczyk, City of Guelph

APPENDIX 12-8-2

Mississaugas of the Credit First Nation



September 16, 2021

Attn: Fawn Sault
Consultation Manager
Mississaugas of the Credit First Nation
4065 Hwy. 6, Hagersville, NOA 1H0
fawn.sault@mncfn.ca

Via E-mail only

**RE: Mississaugas of the Credit First Nation Engagement for the Downtown Guelph Project
Municipal Class Environmental Assessments**

Dear Fawn,

The City of Guelph acknowledges that the land which we call Guelph is situated on the traditional home to Mississaugas of the Credit and your ancestors and is part of the Between the Lakes Treaty No. 3 territory. We wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph Project to Mississaugas of the Credit First Nation and that this engagement will also help to grow our relationship by facilitating meaningful dialogue. Your participation will ensure your input is considered in the project as it moves forward.

About the Downtown Guelph Project

As you may know, Guelph's downtown has aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)
- The other EA will examine the Macdonell Street Bridge and Allan Dam structures across the Speed River to confirm required improvements and the preferred solution for each.

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By the end of 2022, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in early 2023. Construction will start no sooner than 2024.

The Downtown Guelph Project study area includes the Downtown Secondary Plan area north of the railway tracks as outlined in the image below.



About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

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Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and answer any questions you may have.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

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Next steps

We would be pleased to discuss the project in further detail together and welcome the opportunity for MCFN representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, two virtual public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca
guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
doca@mncfn.ca
Leslie Muñoz, City of Guelph

Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: September 17, 2021 9:05 AM
To: Fawn Sault
Cc: 'doca@mncfn.ca'; Nick Palomba; Andrew McGregor; Connor MacIsaac; Leslie Muñoz; David Di Pietro
Subject: City of Guelph - Downtown Infrastructure Revitalization Program - Notice of EA Commencement
Attachments: Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf; Notice of Commencement-WyndhamStreetClassEA_FINAL.pdf; MCFN-DowntownProjectNoticeLetter-signed.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Fawn,

Enclosed please find an introductory letter regarding the City of Guelph's Downtown Infrastructure Revitalization Program along with the Notices of Commencement for two Environmental Assessments being completed as part of this work.

Please do not hesitate to contact me if you have any questions.

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
519-822-1260 extension 2765
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May 30, 2022

Attn: Mark LaForme
Director, Department of Consultation and Accommodation
Mississaugas of the Credit First Nation
2789 Mississauga Road R.R. #6
Hagersville, Ontario, N0A 1H0
Mark.LaForme@mncfn.ca

Via E-mail & Hard Copy

**RE: Mississaugas of the Credit First Nation Engagement for the Downtown Guelph Project
Municipal Class Environmental Assessments**

Dear Mark,

This letter is a follow up to our initial correspondence sent on August 30, 2021.

The City of Guelph acknowledges that the land which we call Guelph is situated on the traditional home to Mississaugas of the Credit and your ancestors and is part of the Between the Lakes Treaty No. 3 territory. We wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph project to Mississaugas of the Credit First Nation and will also help to grow the relationship between the City of Guelph and MCFN by facilitating meaningful engagement and dialogue. Your participation will ensure your input is considered in the project as it moves forward.

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As you may know, Guelph's downtown is filled with aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)

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- The other EA will examine the Macdonell Street Bridge and Allan Dam structures across the Speed River to confirm required improvements and the preferred solution for each.

In 2023, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in 2023/2024. Construction will start no sooner than 2024.

About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and any questions you may have about the MCFN interest and involvement in this project.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

Next steps

We would be pleased to discuss the project in further detail together and welcome the opportunity for MCFN representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, three public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca
guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

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cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
Adam LaForme, Mississaugas of the Credit First Nation
Leslie Muñoz, City of Guelph

Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: May 27, 2022 3:23 PM
To: Mark.LaForme@mncfn.ca
Cc: Adam.LaForme@mncfn.ca; Nick Palomba; Andrew McGregor; Connor MacIsaac; Leslie Muñoz; David Di Pietro; Steven Di Pietro
Subject: Potentially Tampered Mississauga's of the Credit First Nation Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments
Attachments: MCFN-DowntownProjectNotice-Follow-up-vf.pdf; Notice of Commencement-Macdonell and Allan Structures-FINAL.pdf; Notice of Commencement-Wyndham Street Class EA_FINAL.pdf
Categories: Indigenous

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Dear Mark,

Please find attached a follow-up letter and Notice of Commencements for two Environmental Assessments in Downtown Guelph.

Sincerely,

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
519-822-1260 extension 2765
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Mila Khatri

From: Mark LaForme <Mark.LaForme@mncfn.ca>
Sent: May 30, 2022 8:38 AM
To: Reg Russwurm
Cc: Adam LaForme; Nick Palomba; Andrew McGregor; Connor MacIsaac; Leslie Muñoz; David Di Pietro; Steven Di Pietro
Subject: RE: Mississaugas of the Credit First Nation Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments

Categories: Indigenous

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Hello Reg,

I hope you are well. Thank you for the Notices of Commencement for the Environmental Assessments in Guelph. Upon review of the documents sent, the MCFN has no comments or questions at this time. However, I ask that you keep me informed as these projects progress.

Thank you.

Kind regards,

Mark LaForme (he/him)

Director

MCFN-DOCA

4065 Hwy. 6

Hagersville, ON N0A 1H0

Office: 905-768-4260

Mobile: 289-527-6577

<http://mncfn.ca/doca>

Google Maps: <https://www.google.ca/maps/place/MNCFN-DOCA/@42.9718566,-80.0429177,15z/data=!4m5!3m4!1s0x0:0xd52b4642633e9aa2!8m2!3d42.9718566!4d-80.0429177>

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From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: Friday, May 27, 2022 3:23 PM
To: Mark LaForme <Mark.LaForme@mncfn.ca>
Cc: Adam LaForme <Adam.LaForme@mncfn.ca>; Nick Palomba P.Eng. (npalomba@rvanderson.com) <npalomba@rvanderson.com>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Connor MacIsaac' <cmacIsaac@rvanderson.com>; Leslie Muñoz <Leslie.Munoz@guelph.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Steven Di Pietro <Steven.DiPietro@guelph.ca>
Subject: Mississaugas of the Credit First Nation Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments

Dear Mark,

Please find attached a follow-up letter and Notice of Commencements for two Environmental Assessments in Downtown Guelph.

Sincerely,

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

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Mila Khatri

From: Marie-Annick Prevost <Marie-Annick.Prevost@mncfn.ca>
Sent: November 11, 2022 9:57 AM
To: Reg Russwurm
Cc: Adam LaForme; David Di Pietro; Steven Di Pietro; Connor MacIsaac; Andrew Miller; Nick Palomba; Stephanie Bryenton; Leslie Muñoz
Subject: RE: City of Guelph - Downtown Project Update
Categories: Indigenous

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Aanii Reg,

On behalf of the Mississaugas of the Credit First Nation, Department of Consultation and Accommodation, I reviewed the Stage 1 Archaeological Assessment report prepared by ASI for downtown Guelph.

I do not have questions or comments about the content of the report.

We look forward to collaborating with you on this project.

Miigwech,

Marie-Annick Prevost, Ph.D. (she/her)
Field archaeologist




Mississaugas of the Credit First Nation (MCFN)
Department of Consultation and Accommodation (DOCA)
4065 Highway 6 North, Hagersville, ON N0A 1H0
Cell: 905-870-5844

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: Wednesday, November 2, 2022 3:20 PM
To: Mark LaForme <Mark.LaForme@mncfn.ca>
Cc: Adam LaForme <Adam.LaForme@mncfn.ca>; Abby LaForme <Abby.LaForme@mncfn.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Steven Di Pietro <Steven.DiPietro@guelph.ca>; Connor MacIsaac <cmacisaac@rvanderson.com>; Andrew Miller <Andrew.Miller@guelph.ca>; Nick Palomba P.Eng. <npalomba@rvanderson.com>; Stephanie Bryenton <Stephanie.Bryenton@guelph.ca>; Leslie Muñoz <Leslie.Munoz@guelph.ca>
Subject: City of Guelph - Downtown Project Update

Mr. LaForme,

Attached is a letter update on the City of Guelph's Downtown Renewal Project along with a link to a Stage 1 Archeological Assessment.

 [City of Guelph - Stage 1 Archeological Assessment](#)

Please do not hesitate to contact me if you wish to discuss the project further.

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
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May 30, 2022

Attn: Mark LaForme
Director, Department of Consultation and Accommodation
Mississaugas of the Credit First Nation
2789 Mississauga Road R.R. #6
Hagersville, Ontario, N0A 1H0
Mark.LaForme@mncfn.ca

Via E-mail & Hard Copy

**RE: Mississaugas of the Credit First Nation Engagement for the Downtown Guelph Project
Municipal Class Environmental Assessments**

Dear Mark,

This letter is a follow up to our initial correspondence sent on August 30, 2021.

The City of Guelph acknowledges that the land which we call Guelph is situated on the traditional home to Mississaugas of the Credit and your ancestors and is part of the Between the Lakes Treaty No. 3 territory. We wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph project to Mississaugas of the Credit First Nation and will also help to grow the relationship between the City of Guelph and MCFN by facilitating meaningful engagement and dialogue. Your participation will ensure your input is considered in the project as it moves forward.

About the Downtown Guelph Project

As you may know, Guelph's downtown is filled with aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)

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- The other EA will examine the Macdonell Street Bridge and Allan Dam structures across the Speed River to confirm required improvements and the preferred solution for each.

In 2023, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in 2023/2024. Construction will start no sooner than 2024.

About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and any questions you may have about the MCFN interest and involvement in this project.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

Next steps

We would be pleased to discuss the project in further detail together and welcome the opportunity for MCFN representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, three public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca
guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

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cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
Adam LaForme, Mississaugas of the Credit First Nation
Leslie Muñoz, City of Guelph

April 25, 2024

Attn: Mark LaForme
Director, Department of Consultation and Accommodation
Mississaugas of the Credit First Nation
2789 Mississauga Road R.R. #6
Hagersville, ON N0A 1H0
Mark.LaForme@mncfn.ca

Via E-mail Only

**RE: Mississaugas of the Credit First Nation Engagement for the Guelph
Downtown Infrastructure Renewal Program Municipal Class Environmental
Assessments**

Dear Sir / Madam,

This letter is a follow up to our initial correspondence sent on August 30, 2021, and our subsequent correspondence on May 27, 2022, and November 1, 2022.

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Mississaugas of the Credit First Nation, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to provide Mississaugas of the Credit First Nation the opportunity to **review the revised Stage 1 Archaeological Assessment report, which has been updated to include an assessment of the extended study area of Downtown Guelph**. We welcome the opportunity for MCFN representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project.

We hope the information below will be useful in explaining the Downtown Guelph project to MCFN and will also help to grow the relationship between the City of Guelph and MCFN by facilitating a genuine and meaningful relationship. Your insight will ensure your voice is centered and input is considered in the project as it moves forward.

About the Project

Planning for the Downtown Infrastructure Renewal Program (DTIRP) began in spring of 2021. During the planning phase we are looking at the existing infrastructure and the future needs of Downtown Guelph to determine what needs to be replaced or upgraded.

As part of this program of work, the City is undertaking a Class Environmental Assessment (EA) for the Macdonell Street bridge and Allans Dams structures and has completed an EA process for Wyndham Street (initiated as an EA however now exempt). The City is now developing an overall Capital Implementation Plan for this and other works requiring infrastructure upgrades in the City's downtown core.

By early 2025 we anticipate presenting the Macdonell and Allans Structures EA study and Downtown Renewal recommendations to City Council for approval.

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Detailed design of the first phase of DTIRP (i.e. Wyndham Street corridor) has recently been initiated following approval by Council. Construction will start no sooner than 2026.

Input on the Updated Stage 1 Archaeological Assessment Report

The City would like to invite MCFN to provide input on the updated Stage 1 Archaeological Assessment Report. As such, a copy of the draft updated Stage 1 Archaeological Assessment Report has been provided for MCFN representatives' review and input. We would appreciate any formal comments on this report by **May 28, 2024**.

Once the report has been updated to reflect MCFN representatives' comments, it will be finalized and submitted for review to the Ontario Ministry of Citizenship and Multiculturalism. Kindly advise if you would like to receive a copy of any of the other noted documents and we would be happy to provide them once available.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

Encl.: Stage 1 Archaeological Assessment

cc: Andrew McGregor, R.V. Anderson Associates Ltd.
Andrew Miller, City of Guelph
Kimberly Krawczyk, City of Guelph

January 14, 2025

Attn: Abby LaForme,
Consultation Coordinator
Mississaugas of the Credit First Nation
4065 Hwy. 6, Hagersville, NOA 1H0
Abby.LaForme@mncfn.ca

Via E-mail only

RE: Mississaugas of the Credit First Nation Update and Engagement for the Downtown Guelph Infrastructure Renewal Program

Dear Abby,

The City of Guelph acknowledges that the land which we call Guelph is situated on the traditional home to Mississaugas of the Credit and your ancestors and is part of the Between the Lakes Treaty No. 3 territory. We wish to ensure you continue to be informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to update you on the progress of this project (our initial correspondence around this project began in September 2021 under the name Wyndham Street Municipal Class EA). This update comes in three areas:

- 1) We are entering the second phase of work on the Macdonell and Allan Structures Municipal Class EA
- 2) Sharing the draft archaeological report for the Downtown Guelph Infrastructure Renewal Program Phase 1 Reconstruction – Wyndham Street North
- 3) Seeking your interest in engagement for the design process for St. Georges Square, a sub project under Phase 1 –the Phase 1 – Wyndham Street N Reconstruction

Through this update We also wish to provide you with the opportunity to engage with us, to convey any issues, guidance, concerns or recommendations that you may have with regard to this initiative. Through this engagement, we also hope to continue as we grow our relationship with you and the Mississaugas of the Credit First Nation by facilitating meaningful dialogue.

About the Downtown Guelph Infrastructure Renewal Program

The Downtown Infrastructure Renewal Program (DTIRP) aims to improve and upgrade roads, sewers, watermain, sidewalks, cycling facilities, and the streetscape in parts of the Downtown Secondary Plan area. The study area for this work is limited to the portion north of the Metrolinx railway tracks, plus Wyndham Street South to Wellington Street, and Wellington Street from Gordon Street to Neeve Street.

As we rebuild roads, we'll also be modernizing the streetscape and public spaces including St. George's square in the heart of Downtown. With intentional and responsible planning, the infrastructure we build today will create the foundation for a vibrant Downtown Guelph for half a century or longer.

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Construction planning for the project in its entirety will take place over a decade. In the immediate future, the plan outlines construction beginning with Wyndham Street South and Wellington Street in the summer of 2025, then along Wyndham Street North and St. George's Square between 2026 and 2028, and along Macdonell including the Bridge and Allan's Dam structures beginning as early as 2028.

Macdonell and Allan Structures Municipal Class EA

As part of the Downtown Renewal project, the City of Guelph is currently undertaking a Municipal Class Environmental Assessment (EA) Study in the downtown core for the Macdonell Bridge and Allan's Dam Structures As well as a potential new pedestrian/cyclist crossing known as the Ward to Downtown Bridge (which was originally an independent EA process initiated through consultation with MCFN under the name *the Pedestrian Bridge Over the Speed River linking Patrick's Ward to Downtown* with initial notice to MCFN on October 3, 2016).



Through the study, the following recommendations are proposed for each structure and have been brought forward through a public engagement process for further feedback and refinement between December 9, 2024 and January 12, 2025.

- Macdonell Bridge – Replace with a wider bridge that can also accommodate active transportation such as cycling and walking with a multi-use path.
- Allan's Dam Bridge - Remove the bridge entirely and find opportunities to honour the heritage of the bridge.
- Allan's Dam Sluiceway and Spillway - Repair and rehabilitate the Sluiceway and Spillway
- Ward to Downtown Bridge - Construct a new structure exclusively for active transportation on the south side of Guelph Junction Rail bridge with a simplified design based on the one previously proposed in a separate EA process.

More details about the recommendations can be found [on our engagement website](#), and we will be pleased to meet with you at your convenience to discuss these solutions and your interests further.

DTIRP Phase 1 - Wyndham Street N. Reconstruction

The Wyndham Street N. Reconstruction Project is the result of the [Municipal Class Environmental Assessment](#) completed in early 2024. As part of the preparatory work, a draft Archaeological Risk Management Plan was prepared and enclosed with this letter. Please review and offer feedback on the Archaeological Study. We're also interested in how you wish to be involved as the project advances.

St George's Square Design

A core feature of Downtown Guelph is the St. Georges Square. As part of DTIRP Phase 1 - Wyndham Street N. Reconstruction, the City is re-visioning the use, function and features of the Square. Part of that effort includes an opportunity for acknowledgement of historical uses of this area and recognition of the peoples of this land today and since time immemorial. We are interested in your guidance about appropriate ways to recognize Treaty 3, and Indigenous histories and culture in this space. At this same time, we are exploring this topic with other Nations with the intention of integrating different perspectives, context and ideas where appropriate.

If available, we would also appreciate examples of other communities who have successfully included efforts such as art, plaques and installations that recognize First Nations', histories and cultures in public spaces.



Figure 1 - St. Georges Square

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Your interests and input in the process

Your feedback is important. The City of Guelph would welcome the opportunity to discuss this project or any of these components with you and answer any questions you may have.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on the recommended alternatives for the Macdonell and Allan Structures EA;
- Any feedback or interest in involvement from your Nation in the Archaeological study, or the preliminary report,
- The level of involvement or specific interests your Nation has in the St. George's square design process and engagement,
- The level of interest in the project from the Nation for further engagement going forward; and
- The best methods to communicate with your Nation.

Next steps

We would be pleased to discuss the project in further detail together and welcome the opportunity for MCFN representatives to participate in the process. The City of Guelph is interested in learning about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project or any of the components. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca
guelph.ca

Encl.: DRAFT Archaeological Risk Management Plan, Downtown Guelph Infrastructure Renewal Project, Phase 1 Reconstruction – Wyndham Street North, Archaeological Research Associates Ltd. November 11, 2024

cc: Andrew Miller, City of Guelph
Kimberly Krawczyk, City of Guelph
Andrew McGregor, R.V. Anderson Associated Ltd.
Vince Pugliese, MTE

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April 25, 2025

Project Name: Macdonell and Allans Structures
DOCA Project Number: 2022-0790
Proponent: City of Guelph

Dear Reg Russwurm,

This letter is to confirm receipt of the project-related correspondence sent by the City of Guelph, on April 3, 2025, regarding the Natural Environmental Assessment report for the Macdonell and Allans Structures.

The Mississaugas of the Credit First Nation (MCFN) are the Treaty Holders of the land on which the project will take place – specifically, the Between the Lakes Treaty #3 (1792). The MCFN holds Indigenous and Treaty Rights specific to the project location and its environs, which may be adversely impacted by it. The Department of Consultation and Accommodation (DOCA) is designated by the MCFN to handle consultation matters on its behalf.

The DOCA consultation team has filed the project-related correspondence identified above. **We have no questions or comments for you at this time.** This does not indicate a position of support for the project, that the Duty to Consult and Accommodate the MCFN has been met, or that there are no adverse impacts to the MCFN's Indigenous and Treaty Rights.

DOCA expects to be notified of any and all future project updates and/or changes. Additionally, DOCA must be notified of, invited to participate in, and provided the opportunity to review any environmental and/or archaeological assessments. At its discretion, DOCA may request capacity funding from the proponent for its consultation and engagement activities relating to the project.

If you have any questions for the DOCA consultation team, please feel free to contact us.

Thank you,

Megan DeVries

Megan DeVries
Manager of Consultations



Mississaugas of the Credit First Nation
2789 Mississauga Road, Hagersville, Ontario N0A 1H0



Phone: (905) 768-1133
Fax: (905) 768-1225





Department of Consultation and Accommodation

Phone: 905-768-4260

Email: megan.devries@mncfn.ca

CC

Adam LaForme, Manager of Archaeology, adam.laforme@mncfn.ca

Lindsay Wong, Manager of Environment, lindsay.wong@mncfn.ca



Mississaugas of the Credit First Nation
2789 Mississauga Road, Hagersville, Ontario N0A 1H0



Phone: (905) 768-1133
Fax: (905) 768-1225



Mila Khatri

From: Andrew McGregor
Sent: April 25, 2025 11:04 AM
To: Mila Khatri
Subject: FW: BR0013 - Guelph - Macdonell and Allans Structures Natural Environment Assessment Review
Attachments: MCFN-DOCA Response Letter - MacDonell and Allans Structures (2025 April 25).pdf

Andrew McGregor, MCIP, RPP

Associate, Senior Planner Transportation



R.V. Anderson Associates Limited
43 Church Street, Suite 104
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From: Megan DeVries <Megan.DeVries@mncfn.ca>
Sent: April 25, 2025 10:16 AM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>; Adam LaForme <Adam.LaForme@mncfn.ca>; Lindsay Wong <Lindsay.Wong@mncfn.ca>; Darin Wybenga <Darin.Wybenga@mncfn.ca>; Abby LaForme <Abby.LaForme@mncfn.ca>
Cc: Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew Miller <Andrew.Miller@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>
Subject: Re: BR0013 - Guelph - Macdonell and Allans Structures Natural Environment Assessment Review

You don't often get email from megan.devries@mncfn.ca. [Learn why this is important](#)

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Good morning,

Please find attached the letter of response from the MCFN-DOCA. We have no questions or comments for you at this time.

Sincerely,
Megan.

Megan DeVries (she/her)

Manager of Consultations

Department of Consultation and Accommodation



Mississaugas of the Credit First Nation

Mailing: 2789 Mississauga Road, Hagersville ON, N0A 1H0

Physical: 4065 Highway 6 North, Hagersville ON, N0A 1H0

Web: www.mncfn.ca

Cell: 226-934-8640

Disclaimer

The Mississaugas of the Credit First Nation (MCFN) asserts the right to maintain stewardship on our ancestral homelands which includes the right to protect the lands and waters that sustain and support the life and health of all. Historically, and without our consent, MCFN homelands have been industrialized to the point where cumulative effects have already surpassed the point of a questionable future for coming generations. In this era of reconciliation, building a renewed relationship with First Nation peoples based on recognition of rights, respect and partnership, MCFN fully expect legal obligations with the body of the politic to adhere to National interests and engage MCFN in all future development taking place within our ancestral lands.

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From: Reg Russwurm <Reg.Russwurm@guelph.ca>

Sent: Thursday, April 3, 2025 9:11 AM

To: Megan DeVries <Megan.DeVries@mncfn.ca>; Adam LaForme <Adam.LaForme@mncfn.ca>; Lindsay Wong <Lindsay.Wong@mncfn.ca>; Darin Wybenga <Darin.Wybenga@mncfn.ca>; Abby LaForme <Abby.LaForme@mncfn.ca>

Cc: Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew Miller <Andrew.Miller@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>

Subject: BR0013 - Guelph - Macdonell and Allans Structures Natural Environment Assessment Review

Good Morning,

Further to our discussions on March 7, below is a link to the Natural Environment Assessment Report for the Macdonell and Allans Structures. Please review and comment at your earliest convenience.

 [CoG MS - MKoe - RB0013 MCFN](#)

We're implementing a new file sharing system and I apologize if you got this link previously without context.

For convenience, I'm also attaching Stage 1 Archaeological Assessment related to this project sent previously.

Please do not hesitate to contact me if you have any questions for require clarification.

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Environment
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[@cityofguelph](https://twitter.com/cityofguelph)

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APPENDIX 12-8-3

Six Nations of the Grand River



September 16, 2021

Attn: Robbin Vanstone
Six Nations of the Grand River
Lands Resources Office
1695 Chiefswood Rd. P.O. Box 5000
Ohsweken, ON N0A 1M0
rvanstone@sixnations.ca

Via E-mail only

RE: Six Nations of the Grand River Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments

Dear Robbin,

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Six Nations, since time immemorial. We wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph Project to Six Nations of the Grand River and that this engagement will also help to grow our relationship by facilitating meaningful dialogue. Your participation will ensure your input is considered in the project as it moves forward.

About the Downtown Guelph Project

As you may know, Guelph's downtown has aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)
- The other EA will examine the Macdonell Street Bridge and Allan Dam structures across the Speed River to confirm required improvements and the preferred solution for each.

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By the end of 2022, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in early 2023. Construction will start no sooner than 2024.

The Downtown Guelph Project study area includes the Downtown Secondary Plan area north of the railway tracks as outlined in the image below.



About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

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Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and answer any questions you may have.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

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Next steps

We would be pleased to discuss the project in further detail together, and welcome the opportunity for Six Nations of the Grand River representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, two virtual public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
Lonny Bomberry, Six Nations of the Grand River
Dawn Laforme, Six Nations of the Grand River
Leslie Muñoz, City of Guelph

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Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: September 17, 2021 9:02 AM
To: 'rvanstone@sixnations.ca'
Cc: 'Lonny Bomberry'; 'dlaforme@sixnations.ca'; Nick Palomba; Andrew McGregor; Connor MacIsaac; Leslie Muñoz; David Di Pietro
Subject: City of Guelph - Downtown Infrastructure Revitalization Program - Notice of EA Commencement
Attachments: Notice of Commencement-MacdonellandAllanStructures-FINAL.pdf; Notice of Commencement-WyndhamStreetClassEA_FINAL.pdf; SixNationsoftheGrandRiver-DowntownProjectNoticeLetter-signed.pdf

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Robbin,

Enclosed please find an introductory letter regarding the City of Guelph's Downtown Infrastructure Revitalization Program along with the Notices of Commencement for two Environmental Assessments being completed as part of this work.

Please do not hesitate to contact me if you have any questions.

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

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May 30, 2022

Attn: Robbin Vanstone
Six Nations of the Grand River
Lands Resources Office
1695 Chiefswood Rd. P.O. Box 5000
Ohsweken, ON N0A 1M0
rvanstone@sixnations.ca

Via E-mail & Hard Copy

RE: Six Nations of the Grand River Engagement for the Downtown Guelph Project Municipal Class Environmental Assessments

Dear Robbin,

This letter is a follow up to our initial correspondence sent on August 30, 2021.

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Six Nations of the Grand River, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to inform you of our work to date as well as our plans for this project. We also wish to provide you with the opportunity to engage with us, to convey any issues, concerns or recommendations that you may have with regard to this initiative.

We hope this information will be useful in explaining the Downtown Guelph project to Six Nations of the Grand River and will also help to grow the relationship between the City of Guelph and Six Nations of the Grand River by facilitating meaningful engagement and dialogue. Your participation will ensure your input is considered in the project as it moves forward.

About the Downtown Guelph Project

As you may know, Guelph's downtown is filled with aging water and sewer pipes, roads and sidewalks. We are working to improve this important infrastructure to provide quality service and support the vitality of our downtown.

Work like this takes time. It takes years to research, engage with the community, plan and replace the infrastructure. By improving this infrastructure, downtown businesses, residents, users and visitors will all benefit.

Planning for the Downtown Guelph Project began in Spring 2021. As part of the planning phase we are looking at the existing and future infrastructure requirements and future needs of downtown. As part of the planning work we will be completing two Municipal Class Environment Assessments (EAs):

- One EA will consider reducing the number of lanes on Wyndham Street North from four to two in line with the [City's streetscape manual](#)

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- The other EA will examine the Macdonell Street Bridge and Allan Dam structures across the Speed River to confirm required improvements and the preferred solution for each.

In 2023, we will present the EAs and the Downtown Guelph Project planning findings and approach to City Council for approval. We expect to start work on the final design for the downtown in 2023/2024. Construction will start no sooner than 2024.

About the Wyndham Street Municipal Class EA

Wyndham Street North is a vital corridor to the accessibility, local economy and placemaking of Downtown Guelph. The *Downtown Streetscape Manual*, completed in 2014, included recommendations to reduce Wyndham Street from four to two lanes and introduce a traffic circle at the Wyndham/Quebec/Douglas intersection, creating a public square in the St. George's Square area.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to confirm the required improvements to Wyndham Street North from Carden Street to Woolwich Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Wyndham Street Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'B' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

About the Macdonell and Allan Structures Class EA

The Macdonell Bridge (Structure No. 112), located on Macdonnell Street over the Speed River, and known to many as Allans Bridge, is a main route for vehicles, pedestrians and cyclists travelling to Downtown Guelph. Constructed in 1963 and rehabilitated in 1988, recent inspections of the Macdonell Bridge identified the need to repair or replace the structure. Rehabilitations, improvements and modifications to the Allans Dam Bridge (Structure 131) and Allans Dam (Structure No. 320), located at the Speed River immediately south of the Macdonell Bridge are also required.

Subsequently, the City of Guelph has retained R.V. Anderson Associated Ltd. to undertake a Municipal Class Environment Assessment (EA) Study to assess a range of improvements and modifications to the Macdonell and Allan structures alternatives to address the need to repair or replace the structures.

The study will consider options for the Macdonell Street Bridge area as a whole, including all three structures, as well as the intersections of Macdonell Street/Woolwich Street and Macdonell Street/Arthur Street North/Elizabeth Street.

Potential impacts of the project alternatives on social, cultural, economic and natural environments will be evaluated and assessed during the Study. The study area is shown in the attached *Macdonell and Allan Structures Class EA Notice of Study Commencement*.

This Class EA Study is being carried out in accordance with the planning and design process for Schedule 'C' projects as outlined in the Municipal Engineers Association *Municipal Class Environmental Assessment* (October 2000, as amended in 2015), which is approved under the Ontario *Environmental Assessment Act*.

Initial input

Your input is important. The City of Guelph would welcome the opportunity to discuss this project with you and any questions you may have about the Six Nations of the Grand River interest and involvement in this project.

At this point in the study, we respectfully request your response pertaining to the following information, for each Class EA:

- Any preliminary comments or concerns that your Nation has on either of the proposed projects;
- The level of interest in the project from the Nation for further engagement; and
- The best methods to communicate with your Nation.

Next steps

We would be pleased to discuss the project in further detail together, and welcome the opportunity for Six Nations of the Grand River representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Alternatively, three public open houses will be held during each of the Class EAs to share information and receive input from the public. Details, including the date, time, and how to participate in each virtual public open house, will be announced in a subsequent notice and posted on the project webpage at guelph.ca/downtownproject.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact directly the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

guelph.ca

Encl.: Wyndham Street Class EA Notice of Study Commencement
Macdonell and Allan Structures Class EA Notice of Study Commencement

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cc: Nick Palomba, R.V. Anderson Associated Ltd.
Andrew McGregor, R.V. Anderson Associated Ltd.
Connor MacIsaac, R.V. Anderson Associated Ltd.
Lonny Bomberry, Six Nations of the Grand River
Robbin Vanstone, Six Nations of the Grand River
Leslie Muñoz, City of Guelph

November 1, 2022

Attn: Peter Graham
Six Nations of the Grand River
Lands Resources Office
1695 Chiefswood Rd. P.O. Box 5000
Ohsweken, ON N0A 1M0
petergraham@sixnations.ca

Via E-mail Only

RE: Six Nations of the Grand River Engagement for the Downtown Renewal Project Municipal Class Environmental Assessments

Dear Peter,

This letter is a follow up to our initial correspondence sent on August 30, 2021 and our subsequent correspondence May 27, 2022.

The City of Guelph acknowledges that the lands in which Guelph is situated on lands that have been home to many Indigenous people, including the Six Nations of the Grand River, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to discuss with you our work to date and learn of any issues, concerns or recommendations that you may have with regard to this initiative. We welcome the opportunity for Six Nations of the Grand River representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project.

We hope the information below will be useful in explaining the Downtown Guelph project to Six Nations of the Grand River and will also help to grow the relationship between the City of Guelph and Six Nations of the Grand River by facilitating a genuine and meaningful relationship. Your insight will ensure your voice is centered and input is considered in the project as it moves forward.

About the Project

Planning for Downtown Renewal began in spring of 2021. During the planning phase we are looking at the existing infrastructure and the future needs of downtown Guelph to determine what we need to update.

As part of the planning work, we will be completing two Municipal Class Environment Assessment (EA) studies that will involve collaborating with the community to support the planning and decision-making process:

- The Wyndham Street EA study, which will examine the function of the road for all users, including vehicle capacity requirements and active transportation amenities.
- The Macdonell Street Bridge and Allan Structures EA across the Speed River to address required structural and corridor improvements.

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By the end of 2024, we will present the EA studies and Downtown Renewal approach and recommendations to City Council for approval. We expect to start work on the final design of the first project for the Downtown Renewal in late 2024 or early 2025. Construction will start no sooner than 2026. The overall design and construction process is expected to last from eight to 10 years depending on pace and capacity.

Work to Date and Preliminary Findings

Since initiating the project in 2021, a number of studies have been, or are in the process of being completed, including a Stage 1 Archaeological Assessment, Natural Environmental Assessment, and Cultural Heritage Evaluation & Preliminary Impact Assessment. The project team has also developed preliminary options for consideration, for each EA.

Please visit the project webpage <https://guelph.ca/living/construction-projects/downtown-infrastructure-revitalization/> or the website haveyoursay.guelph.ca/downtownproject, for more information about the project.

We would be pleased to discuss the project and our findings with Six Nations of the Grand River representatives. We are happy to arrange a meeting at a time and in a format that suits you in the coming weeks.

Input on Stage 1 Archaeological Assessment Report

The City would also like to invite Six Nations of the Grand River to provide input on the Stage 1 Archaeological assessment Report. As such, a copy of the draft Stage 1 Archaeological Assessment Report has also been provided for Six Nations of the Grand River representatives' review and input. We would appreciate any formal comments on this report by **November 30, 2022**.

Once the report has been updated to reflect Six Nations of the Grand River representatives' comments, it will be finalized and submitted for review to the Ontario Ministry of Citizenship and Multiculturalism. Kindly advise if you would like to receive a copy of any of the other noted documents and we would be happy to provide them once available.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

Encl.: Stage 1 Archaeological Assessment

cc: Tanya Hill-Montour, Six Nations of the Grand River

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Leslie Muñoz, City of Guelph
Stephanie Bryenton, City of Guelph
Andrew McGregor, R.V. Anderson Associated Ltd.

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Mila Khatri

From: Tanya Hill-Montour <tanyahill-montour@sixnations.ca>
Sent: November 3, 2022 4:33 AM
To: Reg Russwurm
Cc: Connor MacIsaac; Andrew McGregor
Subject: Re: City of Guelph - Downtown Project Update

Categories: Indigenous

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Thank you
Reg and Peter , acknowledging that I have received

Get [Outlook for iOS](#)

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: Wednesday, November 2, 2022 4:45:13 PM
To: Tanya Hill-Montour <tanyahill-montour@sixnations.ca>
Cc: Connor MacIsaac <cmaclsaac@rvanderson.com>; Andrew McGregor <amcgregor@rvanderson.com>
Subject: FW: City of Guelph - Downtown Project Update

Tanya,

Link being sent per Peter's email. If you can't access, please let me know and we'll find another means to get the report to you.

 [City of Guelph - Stage 1 Archeological Assessment](#)

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
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reg.russwurm@guelph.ca

guelph.ca
facebook.com/cityofguelph
[@cityofguelph](https://twitter.com/cityofguelph)

From: Peter Graham <petergraham@sixnations.ca>
Sent: November 2, 2022 3:50 PM

To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Tanya Hill-Montour <tanyahill-montour@sixnations.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Steven Di Pietro <Steven.DiPietro@guelph.ca>; 'Connor MacIsaac' <cmacisaac@rvanderson.com>; Andrew Miller <Andrew.Miller@guelph.ca>; Nick Palomba P.Eng. (npalomba@rvanderson.com) <npalomba@rvanderson.com>; Stephanie Bryenton <Stephanie.Bryenton@guelph.ca>; Leslie Muñoz <Leslie.Munoz@guelph.ca>
Subject: RE: City of Guelph - Downtown Project Update

[EXTERNAL EMAIL] Do not click links or attachments unless you recognize the sender and know the content is safe.

Thank you Reg,

Please email the archaeology report directly to Tanya-Hill Montour.

Best, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: November 2, 2022 3:24 PM
To: Peter Graham <petergraham@sixnations.ca>
Cc: Tanya Hill-Montour <tanyahill-montour@sixnations.ca>; David Di Pietro <David.DiPietro@guelph.ca>; Steven Di Pietro <Steven.DiPietro@guelph.ca>; 'Connor MacIsaac' <cmacisaac@rvanderson.com>; Andrew Miller <Andrew.Miller@guelph.ca>; Nick Palomba P.Eng. (npalomba@rvanderson.com) <npalomba@rvanderson.com>; Stephanie Bryenton <Stephanie.Bryenton@guelph.ca>; Leslie Muñoz <Leslie.Munoz@guelph.ca>
Subject: City of Guelph - Downtown Project Update

Mr. Graham,

Attached is a letter update on the City of Guelph's Downtown Renewal Project along with a link to a Stage 1 Archeological Assessment.

 [City of Guelph - Stage 1 Archeological Assessment](#)

Please do not hesitate to contact me if you wish to discuss the project further.

Reg Russwurm, MBA, P.Eng, Manager, Design and Construction
Engineering and Transportation Services, Infrastructure, Development and Enterprise
City of Guelph
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April 25, 2024

Attn: Peter Graham
Six Nations of the Grand River
Lands Resources Office
1695 Chiefswood Road, P.O. Box 5000
Ohsweken, ON N0A 1M0
petergraham@sixnations.ca

Via E-mail Only

RE: Six Nations of the Grand River Engagement for the Guelph Downtown Infrastructure Renewal Program Municipal Class Environmental Assessments

Dear Sir / Madam,

This letter is a follow up to our initial correspondence sent on August 30, 2021, and our subsequent correspondence on May 27, 2022, and November 1, 2022.

The City of Guelph acknowledges that the lands in which Guelph is situated on have been home to many Indigenous people, including the Six Nations of the Grand River, since time immemorial and wish to ensure you continue to feel adequately informed and engaged in our work which may have an impact on the land, the waterways and the people of this area for generations to come.

The purpose of this letter is to provide Six Nations of the Grand River the opportunity to **review the revised Stage 1 Archaeological Assessment report, which has been updated to include an assessment of the extended study area of Downtown Guelph.** We welcome the opportunity for Six Nations of the Grand River representatives to educate the City of Guelph and consulting team members about the important historical, political and geographical context, as well as the Nation's interests as it relates to this project.

We hope the information below will be useful in explaining the Downtown Guelph project to Six Nations of the Grand River and will also help to grow the relationship between the City of Guelph and Six Nations of the Grand River by facilitating a genuine and meaningful relationship. Your insight will ensure your voice is centered and input is considered in the project as it moves forward.

About the Project

Planning for the Downtown Infrastructure Renewal Program (DTIRP) began in spring of 2021. During the planning phase we are looking at the existing infrastructure and the future needs of Downtown Guelph to determine what needs to be replaced or upgraded.

As part of this program of work, the City is undertaking a Class Environmental Assessment (EA) for the Macdonell Street bridge and Allans Dams structures and has completed an EA process for Wyndham Street (initiated as an EA however now exempt). The City is now developing an overall Capital Implementation Plan for this and other works requiring infrastructure upgrades in the City's downtown core.

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By early 2025 we anticipate presenting the Macdonell and Allans Structures EA study and Downtown Renewal recommendations to City Council for approval. Detailed design of the first phase of DTIRP (i.e. Wyndham Street corridor) has recently been initiated following approval by Council. Construction will start no sooner than 2026.

Input on the Updated Stage 1 Archaeological Assessment Report

The City would like to invite Six Nations of the Grand River to provide input on the updated Stage 1 Archaeological Assessment Report. As such, a copy of the draft updated Stage 1 Archaeological Assessment Report has been provided for Six Nations of the Grand River representatives' review and input. We would appreciate any formal comments on this report by **May 28, 2024**.

Once the report has been updated to reflect Six Nations of the Grand River representatives' comments, it will be finalized and submitted for review to the Ontario Ministry of Citizenship and Multiculturalism. Kindly advise if you would like to receive a copy of any of the other noted documents and we would be happy to provide them once available.

Thank you in advance for your participation. Kindly respond to this email with your comments or contact the undersigned by phone or email should you have any questions or require additional information. We look forward to scheduling a meeting with you.

Sincerely,

Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services
City of Guelph
519-822-1260 extension 2765
reg.russwurm@guelph.ca

Encl.: Stage 1 Archaeological Assessment

cc: Andrew McGregor, R.V. Anderson Associates Ltd.
Andrew Miller, City of Guelph
Kimberly Krawczyk, City of Guelph

Mila Khatri

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: January 14, 2025 11:20 AM
To: 'LRCS@sixnations.ca'
Cc: Andrew Miller; Kimberly Krawczyk; Andrew McGregor; 'Vince Pugliese'
Subject: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program
Attachments: SNGR-DowntownProjectUpdateLetter- Final.pdf; ARMP - Downtown Guelph Infrastructure Renewal Project (Draft 12-11-24).pdf

Mr. Graham,

We wish to update you on the status of the City of Guelph's Downtown Infrastructure Renewal Program.

Please find attached a letter and DRAFT Archaeological Risk Management Plan. Do not hesitate to contact me to discuss the City's work.

Thanks,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**
Engineering and Transportation Services, Infrastructure, Development and Environment
City of Guelph
519-822-1260 extension 2765
TTY 519-826-9771
reg.russwurm@guelph.ca

guelph.ca
facebook.com/cityofguelph
[@cityofguelph](https://twitter.com/cityofguelph)

My work hours may not match yours, and I do not expect you to respond outside your working hours.

Mila Khatri

To: Andrew McGregor; Reg Russwurm
Cc: Andrew Miller
Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

From: Peter Graham <L RCS@sixnations.ca>
Sent: Friday, January 17, 2025 12:55 PM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>
Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

[EXTERNAL EMAIL] This email originates outside the City of Guelph. Do not click links or attachments unless you recognize the sender and know the content is safe.

Good afternoon Reg,

Thank you for the letter. It's certainly packed with content, so please let me know if I haven't picked up on something.

Macdonell and Allan Structures Municipal Class EA: This is the first I've heard of this EA, likely because there hasn't been much public-facing activity in recent years. Please ensure I'm on the contact list. Per MECP guidance, we charge for EA work: \$125/hr for document review and \$1250 for meetings. I'm largely interested in just two aspects considering the preferred alternatives. Macdonell Bridge: Will there be in-water work for the preferred alternative? Please send me the natural environmental study. Allan's Dam Sluiceway and Spillway: I'd like to better understand the implications of option 3.

DTIRP Phase 1 - Wyndham Street N. Reconstruction: I have not received any documents for this EA. Considering the lack of notice on the recent PIC for the above EA (not that we attend them) and that I didn't receive the notice of completion for this one last year, I'm concerned.

St George's Square Design: We're always interested in educational pieces involving written text. We need a better understanding of how the square is being revamped, but permeable pavers with images important to SNGR members, murals, and culturally-appropriate plantings are amongst the placekeeping elements often discussed for such things. There are particularly extensive examples of the above and more with the recent reimagining of Ontario Place. I've asked the landscape architectural firm permission to send you a file, with assurances it won't be further circulated or publicly released.

Please send any requests to comment on archaeological studies to Tanya Hill-Montour: tanyahill-montour@sixnations.ca We have a strong interest in consultation for the Downtown Guelph Infrastructure Renewal Program.

We should consider a meeting sometime in the spring or summer to talk about this and other Guelph projects.

Best, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>

Sent: January 14, 2025 11:20 AM

To: Peter Graham <L RCS@sixnations.ca>

Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>

Subject: [External] City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Mr. Graham,

We wish to update you on the status of the City of Guelph's Downtown Infrastructure Renewal Program.

Please find attached a letter and DRAFT Archaeological Risk Management Plan. Do not hesitate to contact me to discuss the City's work.

Thanks,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**

Engineering and Transportation Services, Infrastructure, Development and Environment

City of Guelph

519-822-1260 extension 2765

TTY 519-826-9771

reg.russwurm@guelph.ca

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March 12, 2025

Attn: Peter Graham,
Land Use Officer, Department of Lands and Resources
Six Nations of the Grand River
1695 Chiefswood Rd. P.O. Box 5000
Ohsweken, ON N0A 1M0
LRCS@sixnations.ca

Via E-mail only

RE: Guelph Downtown Infrastructure Renewal Program: Response to Email Dated January 17, 2025, RE: Project Update and Draft Archaeological Risk Management Plan

Dear Mr. Graham,

Thank you for your email dated January 17, 2025, in response to the update for the Downtown Infrastructure Renewal Program (DTIRP) and draft Archaeological Risk Management Plan sent to you on January 14, 2025. We have reviewed your comments and provide the following responses.

Macdonell and Allan's Structures Municipal Class EA

You have been added to the study's contact list and will receive all future notices and project updates. Previous notices for the Macdonell and Allan's Structures Class EA were sent to various contacts at Six Nations of the Grand River (SNGR) as noted below:

- Notice of Commencement – sent by the City to Lonny Bomberry (Lands & Resource Director), Robbin Vanstone (Land Use Office, Lands & Research), and Dawn LaForme on September 17, 2021
- Follow-Up Letter – sent by the City to Lonny Bomberry and Robbin Vanstone on May 27, 2022.
- A Stage 1 Archaeological Assessment Report for the overall DTIRP project, including the Macdonell & Allan's Structures Class EA Study Area, was sent to you and forwarded to Tanya Hill-Montour on November 2, 2022. Tanya acknowledged receipt of the report on November 3, 2022.
- The final Stage 1 Archaeological Assessment Report for the overall DTIRP project including the Macdonell & Allan's Structures Class EA Study Area, was sent to you on May 17, 2024. There is no record of a response.

Macdonell Bridge

For the Macdonell Bridge, the recommendation is to replace the entire structure and widen the bridge deck to accommodate active transportation facilities. This recommendation will require in-water works. Temporary impacts on aquatic species are anticipated from the replacement and removal of the

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old bridge materials. Detailed desktop studies and field investigations were completed to inventory the natural environment and inform the evaluation of alternative solutions. Findings are being summarized in a Natural Environment Report, currently in progress. The Project Team would be happy to share the draft report with you once it is ready and value any input you may provide. Anticipated impacts and mitigation measures will be further confirmed in an Environmental Impact Study (EIS) to be undertaken during detailed design.

Allan's Sluiceway and Spillway

The recommendation for the Allan's Sluiceway and Spillway is rehabilitation over Do Nothing (Alternative 1) and Removal of the Sluiceway and Spillway (Alternatives 3 & 4), and is based on a comparative evaluation against all key criteria.

The removal of the Allan's Sluiceway and Spillway under Alternative 3 will impact the hydraulic function of the river. Since the weir structure has a significant influence on the elevation of the Speed River, upstream and downstream, the removal of the structure will impact the water level of the Speed River and the established environmental balance. The removal of the structure would also impact the public recreational use of the river and have potentially negative impacts on properties backing onto the river. It should be noted that the Spillway and Sluiceway is a Known Built Heritage Resource.

Based on our evaluation of Alternative 3 (Removal), we identified that removing the structure would have an overall positive impact on the natural environment of the Speed River (also a Known Cultural Heritage Landscape). Removal of the structure would restore/naturalize significant valleylands, remove barriers for fish passage, restore the migration corridor connectivity, and potentially restore the natural floodplain.

The recommendation of the Rehabilitation Option (Alternative 2) over Do Nothing (Alternative 1) and Removal of the Sluiceway and Spillway (Alternatives 3 & 4) was based on a comparative evaluation against all key criteria.

Wyndham Street Municipal Class EA

As mentioned in our letter dated January 14, 2025, initial communications regarding the Wyndham Street Class EA were sent in September 2021. Please see below for a summary of communications sent to SNGR:

- Notice of Commencement – sent to Robbin Vanstone, Lonny Bomberry, and Dawn LaForme on September 17, 2021
- City to confirm if there was any follow up between September 17, 2021, and September 25, 2023, around the time of PIC #1.
- A Stage 1 Archaeological Assessment Report for the overall DTIRP project, including the Wyndham Street Class EA Study Area, was sent to you and forwarded to Tanya Hill-Montour on November 2,

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2022. Tanya acknowledged receipt of the report on November 3, 2022.

- Follow-up Letter – sent on September 25, 2023, to Robbin Vanstone, and included mention of when Public Information Centre #2 was being held.

The follow up letter sent on September 25, 2023, noted that the Wyndham Street Class EA was originally initiated as a Schedule 'B' Class EA and was being downgraded to a Schedule A+ in accordance with the Municipal Class EA (October 2000, amended in 2007, 2011, and 2015), as the range of options under consideration are pre-approved under the Class EA process. The recommendation for the Wyndham Street corridor is to reduce vehicle travel lanes from four lanes to two lanes, introduce dedicated bicycle lanes with buffers, and provide on-street parking and loading zones.

Compensation

The City of Guelph currently has a policy in place related to compensation for Field Liaison Representative (FLR) agreements as part of archaeology field work for major projects. In relation to the Macdonell and Allan's Structures Municipal Class EA, the field work and potential for a Stage 2 assessment will take place in mid to late 2026 as part of the final design work, and project staff will be in touch regarding interest to participate through an FLR. While the City does not currently have a policy in place to provide compensation outside of the FLR process, we acknowledge the opportunity to review our policies and practices as part of advancing our commitments in the City's [Indigenous Relations Framework](#). Commitment 1.4 '*Work with the MCFN, SNGR, GRMC, and urban Indigenous peoples on initiatives impacting them. Early progress will include establishing engagement and communication protocols*', provides the City with the guidance to undertake a review of our engagement policies related to Indigenous consultation and engagement. This review will take into consideration best practices across the sector, budget impacts, and consistency across all projects and outreach to First Nations and Metis governments. We expect this work will take place throughout 2025. While we cannot offer compensation currently, we are committed to conducting a review of our policies over the coming months. To help us as we undertake this work, we would be grateful if you could send the MECP guidance you referenced in your previous response.

Sincerely,



Reg Russwurm, Manager, Design and Construction
Engineering and Transportation Services

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1 Carden St
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reg.russwurm@guelph.ca
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cc: Andrew Miller, City of Guelph
Kimberly Krawczyk, City of Guelph
Andrew McGregor, R.V. Anderson Associated Ltd.
Vince Pugliese, MTE

Mila Khatri

From: Andrew McGregor
Sent: March 13, 2025 2:50 PM
To: Mila Khatri
Subject: FW: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Follow Up Flag: Follow up
Flag Status: Flagged

Fyi

Vacation alert: I will be away from the office on vacation Mar 10-14.

Andrew McGregor, MCIP, RPP
Associate, Senior Planner Transportation



R.V. Anderson Associates Limited
43 Church Street, Suite 104
St. Catharines ON L2R 7E1
t 905 685 5049 x4211 | m 905 964 4056

[LinkedIn](#) | [Facebook](#) | [Website](#)



From: Peter Graham <LRCS@sixnations.ca>
Sent: March 13, 2025 11:03 AM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>
Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

[CAUTION EXTERNAL EMAIL] Make Sure that it is legitimate **before** Replying or Clicking on any links

Hi Reg,

Thanks again for your earlier email, as it's helping us iron out some communication issues.

Macdonell EA: My email address changed in late November 2022 and I'm guessing the May 17 2024 email was sent to my old address as it's not showing up in my inbox (but per your below it's only of importance to Tanya as I don't do arch). Yes, please provide me the draft report when available. I'm very concerned about the EIS being left to detailed design.

Wyndham EA: Robbin hadn't worked here for a year when the Sep 25 2023 letter was sent to her, so we did not receive it.

Compensation: The MECP guidance language is "bearing the reasonable costs associated with these consultation opportunities". As it's provincial guidance, we expect all municipalities to conform to it. We do not review project documents or otherwise engage in EA consultation until capacity funding is agreed to.

Best, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>

Sent: March 12, 2025 3:44 PM

To: Peter Graham <L RCS@sixnations.ca>

Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>

Subject: [External] RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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I wish as well to confirm that we should send the review materials to Tanya Hill-Montour that we sent earlier at this time and/or arrange an overview meeting to introduce the material to your team.

We're looking forward to discussing the projects with you.

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**

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TTY 519-826-9771

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Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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Best, Peter

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Sent: January 20, 2025 8:47 AM

To: Peter Graham <LRCS@sixnations.ca>

Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>

Subject: [External] RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Thanks for your quick response Peter,

We'll be in touch regarding your requests.

- Reg

From: Peter Graham <LRCS@sixnations.ca>

Sent: Friday, January 17, 2025 12:55 PM

To: Reg Russwurm <Reg.Russwurm@guelph.ca>

Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>

Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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Good afternoon Reg,

Thank you for the letter. It's certainly packed with content, so please let me know if I haven't picked up on something.

Macdonell and Allan Structures Municipal Class EA: This is the first I've heard of this EA, likely because there hasn't been much public-facing activity in recent years. Please ensure I'm on the contact list. Per MECP guidance, we charge for EA work: \$125/hr for document review and \$1250 for meetings. I'm largely interested in just two aspects considering the preferred alternatives. Macdonell Bridge: Will there be in-water work for the preferred alternative? Please send me the natural environmental study. Allan's Dam Sluiceway and Spillway: I'd like to better understand the implications of option 3.

DTIRP Phase 1 - Wyndham Street N. Reconstruction: I have not received any documents for this EA. Considering the lack of notice on the recent PIC for the above EA (not that we attend them) and that I didn't receive the notice of completion for this one last year, I'm concerned.

St George's Square Design: We're always interested in educational pieces involving written text. We need a better understanding of how the square is being revamped, but permeable pavers with images important to SNGR members, murals, and culturally-appropriate plantings are amongst the placekeeping elements often discussed for such things. There are particularly extensive examples of the above and more with the recent reimagining of Ontario Place. I've asked the landscape architectural firm permission to send you a file, with assurances it won't be further circulated or publicly released.

Please send any requests to comment on archaeological studies to Tanya Hill-Montour: tanyahill-montour@sixnations.ca We have a strong interest in consultation for the Downtown Guelph Infrastructure Renewal Program.

We should consider a meeting sometime in the spring or summer to talk about this and other Guelph projects.

Best, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>

Sent: January 14, 2025 11:20 AM

To: Peter Graham <L RCS@sixnations.ca>

Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>

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Please find attached a letter and DRAFT Archaeological Risk Management Plan. Do not hesitate to contact me to discuss the City's work.

Thanks,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**
Engineering and Transportation Services, Infrastructure, Development and Environment
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[@cityofguelph](https://twitter.com/cityofguelph)

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Mila Khatri

From: Peter Graham <LRCS@sixnations.ca>
Sent: June 24, 2025 3:15 PM
To: Reg Russwurm
Cc: Andrew Miller; Kimberly Krawczyk; Andrew McGregor; Intergovernmental Relations
Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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Hi Reg,

I've reached out to MECP about this. Barring new information from the city or ministry, we will initiate Section 16 requests for every Guelph EA until this situation is resolved.

Best, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: June 24, 2025 1:54 PM
To: Peter Graham <LRCS@sixnations.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; Intergovernmental Relations <InterGovernmental.Relations@guelph.ca>
Subject: [External] RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Mr. Graham,

We acknowledge your feedback regarding capacity funding, however at this time the City does not have a policy to provide capacity funding for consultation activities.

We will continue to keep you informed about the progress and project updates and are sharing the information to ensure you stay updated. To that end, below is a link to the Macdonell and Allans Structures Natural Environment Assessment Review. Any comments by the end of July, 2025 are most welcome.

 [CoG MS - MKoe - RB0013](#)

Please follow the following steps:

1. Click "Open" to access the content.
2. Enter the email (LRCS@sixnations.ca) that received this link and click "Next".
3. A verification code will be sent to your email.
4. Enter the code and click "Verify". Verification is required every 24 hours.

Let me know if you have any difficulties opening the link or file.

Thanks,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**

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From: Peter Graham <L RCS@sixnations.ca>
Sent: Thursday, May 1, 2025 10:07 AM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Intergovernmental Relations <InterGovernmental.Relations@guelph.ca>
Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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Good morning Reg,

Why is provincial guidance an insufficient rationale? I don't understand why the city is refusing to follow provincial direction for both EAs and planning applications.

Thank you, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: April 30, 2025 4:56 PM
To: Peter Graham <L RCS@sixnations.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Intergovernmental Relations <InterGovernmental.Relations@guelph.ca>
Subject: [External] RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Good afternoon Peter,

Thank you for your email and for sharing your perspective on the requirement of capacity funding for Environmental Assessments (EAs). I appreciate the opportunity to discuss this matter further.

Regarding the request for capacity funding for all EAs, and the belief that it should be offered as a matter of course for all proposed EA consultations, we understand this position and are committed to reviewing how the City responds to such requests. As previously outlined, the City is dedicated to undertaking an evaluation of our policies towards the potential for a modified approach. However, until this internal policy work is complete, the information you provided does not give the City the necessary rationale for the rapid change in approach around

the provision of capacity funding for EA consultation. Your point about being severely understaffed is noted and provides some clarity on the need for capacity funding more broadly and long-term as it pertains to EAs. At this time, the City will continue to establish opportunities for consultation with SNGR on this project, but cannot offer capacity funding prior to completing the internal work needed to evaluate a change in approach.

In the coming months, we will undertake the steps required to evaluate a different approach to how the City addresses these requests. We look forward to collaborating effectively moving forwards as we navigate these challenges. Thank you for your understanding and cooperation.

Best regards,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**
Engineering and Transportation Services, Infrastructure, Development and Environment
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From: Peter Graham <LRCS@sixnations.ca>
Sent: Monday, March 24, 2025 12:55 PM
To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>
Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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Hi Reg,

We've been requiring capacity funding for all EAs over the past couple years. Prior to that, we weren't aware of MECP's guidance. As we believe capacity funding should be offered as a matter of course for all proposed developments, and we're severely understaffed here, it's incumbent on us to utilize such funding when available.

Thank you, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: March 24, 2025 11:16 AM
To: Peter Graham <LRCS@sixnations.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>
Subject: [External] RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Peter,

Thank you for your response regarding the Macdonell and Allan Structures Municipal Class Environmental Assessment (EA). We understand that capacity funding can be a tool in managing constraints to effectively fulfill consultation requests. We value our collaborative history with Six Nations of the Grand River on previous EAs where capacity funding was not required. Could you please provide further details on what has changed to necessitate capacity funding for this EA, or what specific aspects of this EA require the additional funding? We are interested in understanding your perspective and to explore how we can continue to work together effectively.

Looking forward to your response.

Thanks,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**
Engineering and Transportation Services, Infrastructure, Development and Environment
City of Guelph
519-822-1260 extension 2765
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To: Reg Russwurm <Reg.Russwurm@guelph.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>
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Subject: [External] RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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- Reg

From: Peter Graham <LRCS@sixnations.ca>

Sent: Friday, January 17, 2025 12:55 PM

To: Reg Russwurm <Reg.Russwurm@guelph.ca>

Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>

Subject: RE: City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

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Good afternoon Reg,

Thank you for the letter. It's certainly packed with content, so please let me know if I haven't picked up on something.

Macdonell and Allan Structures Municipal Class EA: This is the first I've heard of this EA, likely because there hasn't been much public-facing activity in recent years. Please ensure I'm on the contact list. Per MECP guidance, we charge for EA work: \$125/hr for document review and \$1250 for meetings. I'm largely interested in just two aspects considering the preferred alternatives. Macdonell Bridge: Will there be in-water work for the preferred alternative? Please send me the natural environmental study. Allan's Dam Sluiceway and Spillway: I'd like to better understand the implications of option 3.

DTIRP Phase 1 - Wyndham Street N. Reconstruction: I have not received any documents for this EA. Considering the lack of notice on the recent PIC for the above EA (not that we attend them) and that I didn't receive the notice of completion for this one last year, I'm concerned.

St George's Square Design: We're always interested in educational pieces involving written text. We need a better understanding of how the square is being revamped, but permeable pavers with images important to SNGR members, murals, and culturally-appropriate plantings are amongst the placekeeping elements often discussed for such things. There are particularly extensive examples of the above and more with the recent reimagining of Ontario Place. I've asked the landscape architectural firm permission to send you a file, with assurances it won't be further circulated or publicly released.

Please send any requests to comment on archaeological studies to Tanya Hill-Montour: tanyahill-montour@sixnations.ca We have a strong interest in consultation for the Downtown Guelph Infrastructure Renewal Program.

We should consider a meeting sometime in the spring or summer to talk about this and other Guelph projects.

Best, Peter

From: Reg Russwurm <Reg.Russwurm@guelph.ca>
Sent: January 14, 2025 11:20 AM
To: Peter Graham <L RCS@sixnations.ca>
Cc: Andrew Miller <Andrew.Miller@guelph.ca>; Kimberly Krawczyk <Kimberly.Krawczyk@guelph.ca>; Andrew McGregor <AMcGregor@rvanderson.com>; 'Vince Pugliese' <VPugliese@mte85.com>
Subject: [External] City of Guelph - Update and Engagement for the Downtown Infrastructure Renewal Program

Mr. Graham,

We wish to update you on the status of the City of Guelph's Downtown Infrastructure Renewal Program.

Please find attached a letter and DRAFT Archaeological Risk Management Plan. Do not hesitate to contact me to discuss the City's work.

Thanks,

Reg Russwurm, MBA, P.Eng, **Manager, Design and Construction**
Engineering and Transportation Services, Infrastructure, Development and Environment
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